

Kachemak Drive Path Committee



Tuesday
May 8, 2012
Regular Meeting 5:30 p.m.



City Hall Cowles Council Chambers
491 E. Pioneer Avenue
Homer, Alaska 99603

**MEETING NOTICE
REGULAR MEETING AGENDA**

- 1. CALL TO ORDER**
- 2. AGENDA APPROVAL**
- 3. APPROVAL OF THE MINUTES**
 - A. Minutes for the Regular Meeting on March 22, 2012 Page 5
- 4. PUBLIC COMMENTS UPON MATTERS ALREADY ON THE AGENDA**
- 5. RECONSIDERATION**
- 6. VISITORS**
- 7. STAFF & COUNCIL/COMMITTEE REPORTS/COMMISSION REPORTS**
- 8. PUBLIC HEARING(S)**
- 9. PENDING BUSINESS**
 - A. Memorandum dated May 2, 2012 Re: Resolution 11-090(S) and Council Action Page 7
- 10. NEW BUSINESS**
 - A. What is the Committee's Next Action? Discussion on the Next Steps for the Committee.
- 11. INFORMATIONAL MATERIALS**
 - A. Resolution 11-90(S), Supporting the Construction of a Non-motorized Pathway to Increase Safety for Motorized and Non-motorized Users Along Kachemak Drive Located Within Homer City Limits, from the Base of the Homer Spit to East End Road. Page 13
 - Preliminary Engineering Packet Page 15
 - Proposed Funding Plan for Kachemak Drive Path Page 29
 - Proposed Maintenance Plan for Kachemak Drive Path Page 31
 - Reference Materials Cited in Resolution, Engineering Packet, Funding and Maintenance Plans Page 33
 - Memorandum to City Council from Homer Advisory Planning Commission Page 77
 - Minutes from the Parks and Recreation Advisory Commission and Kachemak Drive Path Committee Page 79
- 12. COMMENTS OF THE AUDIENCE**
- 13. COMMENTS OF THE CITY STAFF** *(If present)*
- 14. COMMENTS OF THE COMMITTEE**
- 15. ADJOURNMENT/NEXT REGULAR MEETING IS SCHEDULED FOR MAY 24, 2012 AT 5:30 P.M.** All meetings scheduled to be held in the Homer City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

Minutes 22 March 2012, K-Drive Path Committee

Conference Room Upstairs at City Hall

Attending: Bumppo Bremicker (chair), Dave Brann, Dave Clemens, Beth Cummings, Lindianne Sarno (recording), Mike Stockburger (new member); Missing: Lynn Burt

Call to order, 5:40 p.m.

Dave Brann moves, Beth seconds approval of agenda, agenda approved.

Dave Brann moves, Beth seconds approval of minutes, with this addition: regarding Beth's fundraiser idea, Dave Brann has doubts (1) because of what happened at Karen Hornaday Park, (2) is concerned that privately raised petty cash would be used for items that should be paid from HART funds. Minutes approved.

No public comments, no reconsideration, no visitors, no committee reports, no hearings.

Pending Business: Continued Discussion and Planning. Dave Brann hands out and explains the draft path maintenance plan (see attached handout). Summer and winter maintenance differ. Pack snow or plow it? Dave Clemens advises winter bicyclists bike on studs. Path in winter will be multi-use: ski, snowshoe, bike, run. If plow in winter, melts quicker in spring. Regarding maintenance, we need to research: who owns the trail? The City? Don't make an assumption. Trail would be maintained at a high level, like a multiuse trail. City plows East End Road path, which is paved. K Drive Path will not be paved and plow vehicle could deform gravel structure.

Dave Brann is working on proposed funding plan. HART funds (Homer Accelerated Roads and Trails), \$304,554 in that account. \$100,000 added annually. Can be used for initial survey, materials, engineering, building, hiring contractor. Bond issue not needed, since HART funds are a reliable income stream, City can borrow against that and repay over time. STIP estimate was \$35-40 million, way too high. K Drive Path Committee is asking for city staff time, not funding. Other potential sources: Homer Foundation, donations, fundraisers, STIP, grants.

We describe path to Mike to get him up to speed. Mike owns Homer Boat Yard on Kachemak Drive and drives heavy rigs on K Drive frequently.

Packet that will go to Parks and Recreation and City Council: Make sure pages 57, 58, and 59 are removed from packet.

Private landowners on Kachemak Drive: will need each individual's permission. City Manager Walt Wrede will write the letter asking for right of way. . It's on his desk.

We agree to lobby City Council. Beth Cummings > Beth Wythe, Dave Brann > David Lewis, Dave Clemens > Mayor Hornaday, Lindianne Sarno > Brian Zak, Mike Stockburger > Barbara Howard, Bumppo Bremicker > Barbara Howard. We will wait til we have the whole packet to complete our lobbying assignments.

Walt Wrede joins us.

Beth Cummings wants to see this summer a retaining wall planned at the west end of the proposed trail, cut trees, get plans going.

Walt comments that Dave should bring a plan. Dave plans this summer to concentrate on making Mud Bay Trail a four foot wide walkable path.

Beth: Mud Bay Trail is inappropriate for bicyclists, OK for pedestrians. Find a way to build a terraced walkway or retaining wall on south side of drive. Also, have a bridge made beyond the west end of Aviation Leasing, over the culvert.

Bumppo: will need real engineering on that section of trail.

Dave Brann: This summer, signage. Small speed feedback signs. Need DOT permission? Letter from city not necessary. Dave will photograph existing signs and include with map to DOT. Share the Road program.

Digital signs are \$3,000 each, differing degrees of information can be harvested, depending on cost: count vehicles, record speeds.

Signs go up on Mud Bay Trail when snows melts. "No Camping." Sign on two 4 x 4 posts, "Mud Bay Trail." Lynn and Beth will arrange this.

We choose meeting dates: April 5, May 24 (no Dave Brann), June 14.

Comments: Thanks to Mike Stockburger for joining the committee. We are making good progress.

Adjourn: 8 p.m.

Office of the City Clerk

Jo Johnson, CMC, City Clerk

Melissa Jacobsen, CMC, Deputy City Clerk II
Renee Krause, CMC, Deputy City Clerk I



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MEMORANDUM

TO: KACHEMAK DRIVE PATH COMMITTEE
FROM: RENEE KRAUSE, CMC, DEPUTY CITY CLERK I
DATE: MAY 2, 2012
RE: RESOLUTION 11-090(S) AND CITY COUNCIL ACTIONS FROM APRIL 23, 2012
CITY COUNCIL MEETING

Background

Following is an excerpt from the City Council Action Agenda and the Minutes of the April 23, 2012 Council Meeting on the Substitute Resolution 11-090(S).

This resolution was postponed since the material cited was not included in the back up information. It was also brought to my attention that the incorrect amended resolution was presented to Council at the April 23rd meeting. This has been corrected.

I have included all the information that will be presented to City Council at the May 14, 2012 meeting. This will once more be under Pending Business on the agenda.

Recommendation

No Action required. Informational only.

public hearing at 5:00 p.m. There is a question and answer fact sheet available. A presentation to City Council is scheduled for May 14th.

F. Employee Committee Report

13. PENDING BUSINESS

A. **Memorandum 12-056**, from Mayor, Re: Appointments of Ken Castner, Bob Howard, Sharon Minsch, Lloyd Moore, Terry Yager, and Councilmember Mary E. (Beth) Wythe to the Water and Sewer Rate Task Force.

Memorandum 12-069 from City Clerk as backup.

APPROVED with discussion.

Councilmember Howard was also appointed to the task force.

B. **Resolution 11-090**, A Resolution of the City Council of Homer, Alaska, Supporting the Concept and Construction of Non-Motorized Pathways to Increase the Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

Resolution 11-090(S), A Resolution of the City Council of Homer, Alaska, Supporting ~~The Concept And~~ Construction of a Non-Motorized Pathways to Increase The Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the Homer City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

POSTPONED to May 14.

14. NEW BUSINESS

15. RESOLUTIONS

A. **Resolution 12-034**, A Resolution of the City Council of Homer, Alaska, Urging the North Pacific Fishery Management Council to Adopt Measures that Reduce the Halibut Prohibited Species Catch in the Gulf of Alaska Groundfish Fisheries. Lewis.

ADOPTED without discussion.

B. **Resolution 12-035**, A Resolution of the City Council of Homer, Alaska, Approving a New Five Year Lease at the Homer Airport Terminal for Hertz / Pioneer Car Rentals Inc. and Authorizing the City Manager to Execute the Appropriate Documents. City Manager.

Memorandum 12-066 from City Manager as backup.

There was no discussion.

VOTE: (amendment) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Six people applied to the task force within the time limit, two applied after the deadline and one was told he could not apply since the deadline had passed. Councilmember Wythe applied timely and Council has appointed Councilmember Howard to fill the other council seat.

VOTE: (main motion as amended) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

B. **Resolution 11-090**, A Resolution of the City Council of Homer, Alaska, Supporting the Concept and Construction of Non-Motorized Pathways to Increase the Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

Resolution 11-090(S), A Resolution of the City Council of Homer, Alaska, Supporting ~~The Concept And~~ Construction of a Non-Motorized Pathways to Increase ~~The~~ Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the **Homer** City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

Motion on the floor from September 12, 2011 – Motion for the adoption of Resolution 11-090 by reading of title only. Council referred Resolution 11-090 to Planning and Zoning due to issues of land use and utility right-of-ways. Some easements in place for sewer and water include limitations. The proposed trail is a land use issue rather than recreation.

Mayor Hornaday called for a motion to substitute Resolution 11-090(S) for Resolution 11-090.

WYTHE/LEWIS - SO MOVED.

WYTHE/ROBERTS - MOVED TO POSTPONE TO THE NEXT MEETING.

Reference information pertaining to the support of the trail was requested for the next packet. It should include page numbers from the following:

- Homer Non Motorized Transportation and Trails Plan
- Homer Area Transportation Plan
- Climate Action Plan
- Homer Accelerated Roads and Trails Policy Manual
- Capital Improvement Plan
- Planning Commission's recommendation

VOTE: (postponement) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

NEW BUSINESS

RESOLUTIONS

- A. **Resolution 12-034**, A Resolution of the City Council of Homer, Alaska, Urging the North Pacific Fishery Management Council to Adopt Measures that Reduce the Halibut Prohibited Species Catch in the Gulf of Alaska Groundfish Fisheries. Lewis.

Mayor Hornaday called for a motion for the adoption of Resolution 12-034 by reading of title only.

LEWIS/BURGESS – SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- B. **Resolution 12-035**, A Resolution of the City Council of Homer, Alaska, Approving a New Five Year Lease at the Homer Airport Terminal for Hertz / Pioneer Car Rentals Inc. and Authorizing the City Manager to Execute the Appropriate Documents. City Manager.

Memorandum 12-066 from City Manager as backup.

Mayor Hornaday called for a motion for the adoption of Resolution 12-035 by reading of title only.

WYTHE/LEWIS - SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- C. **Resolution 12-036**, A Resolution of the City Council of Homer, Alaska, Approving a New Short Term Lease (Six Months) for Peninsula Scrap and Salvage on a Portion of Lot 12, Homer Spit Subdivision No. 5 and Authorizing the City Manager to Execute the Appropriate Documents. City Manager.

Memorandum 12-067 from City Manager as backup.

1 CITY OF HOMER

2 Zak/Lewis/Parks and
3 Recreation Advisory Commission

4
5 RESOLUTION 11-090(S)
6

7 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
8 ALASKA, SUPPORTING ~~THE CONCEPT~~ AND
9 CONSTRUCTION OF A NON-MOTORIZED PATHWAYS TO
10 INCREASE THE SAFETY FOR MOTORIZED AND NON-
11 MOTORIZED USERS ALONG KACHEMAK DRIVE LOCATED
12 WITHIN THE HOMER CITY LIMITS, FROM THE BASE OF
13 THE HOMER SPIT TO EAST END ROAD.

14
15 WHEREAS, The Parks and Recreation Advisory Commission established a the Kachemak
16 Drive Path Ceommittee to specifically address possible solutions to the hazards presented to
17 non-motorized and motorized users of Kachemak Drive; and

18 ~~WHEREAS, Public input was sought through a variety of channels for solutions to address these~~
19 ~~safety concerns; and recommendations to Lower the Speed Limit, Alter the Travel Lane Width~~
20 ~~and Shoulder, Increase the Use of Signage, Construct Separated, Non-motorized Paths~~
21 ~~paralleling Kachemak Drive using the existing Utility Easements will be contingent on available~~
22 ~~funding in the future~~

23 WHEREAS, The Kachemak Drive Path Committee received substantial public input on
24 safety concerns; and

25 WHEREAS, The Homer City Council has shown support for this non-motorized pathway by
26 ~~in approval of the Homer Non-Motorized Transportation and Trail Plan, Homer Area~~
27 ~~Transportation Plan, Climate Action Plan, HART Policy Manual and inclusion of the Kachemak~~
28 ~~Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the Homer~~
29 Non-Motorized Transportation and Trail Plan; the Homer Area Transportation Plan; the
30 Climate Action Plan; and the Homer Accelerated Roads and Trails (HART) Policy
31 Manual; and

32 WHEREAS, Increasing active transportation, motorized and non-motorized transportation;
33 ~~offers the potential for improved~~ improves public health and safety, encourages tourism,
34 ~~economic development, a cleaner~~ cleans the environment, reduces transportation costs, and
35 ~~enhanced~~ community connections, social equity, and more livable communities; and

36 WHEREAS, The City of Homer has available HART trail funds that can, including other
37 sources, form a basis for funding this project; and

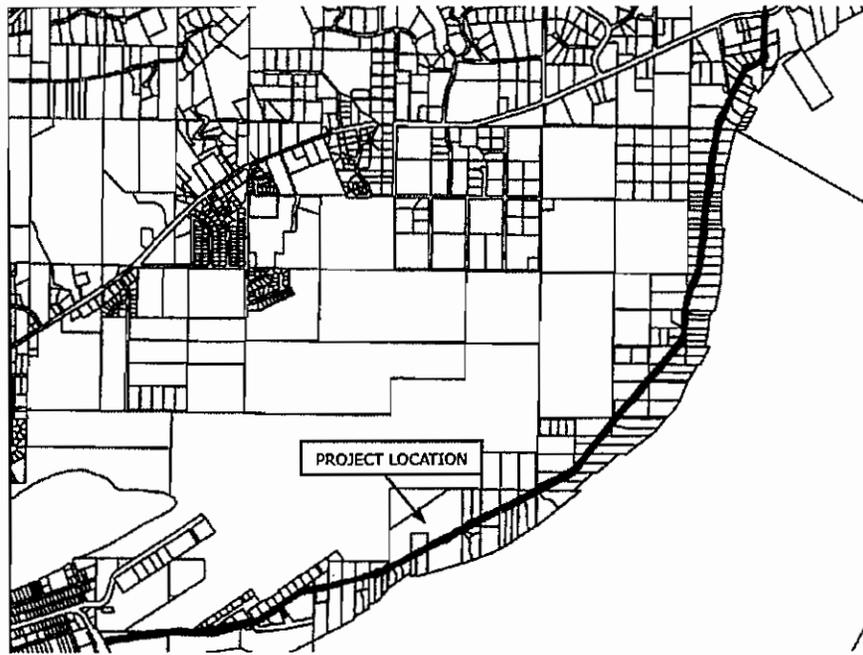
38 WHEREAS, Utility easements can be employed with the permission of property owners.

Kachemak Drive Path

Preliminary Engineering Packet

March 8, 2012

Mission Statement: To build a safe, separate non motorized trail along Kachemak Drive connecting East End Road to the Spit Road via the airport.



Contents / Index

Pg	Description
1	Cover Sheet, Vicinity Map, Index
2	General Notes
3	Typical Path Sections
4-7	Aerial Views of Path and Road

References:

Homer Non Motorized Transportation & Trail Plan
City of Homer Capital Improvement Plan (CIP) 2012-2017
City of Homer Trail Manual Design Criteria

References are available from the City Clerk, as hard copy or online at:

<http://www.cityofhomer-ak.gov/documentsandforms>

Kachemak Drive Path - Preliminary Engineering Packet

March 8, 2012

General Notes

PROJECT DESCRIPTION & BENEFIT: Kachemak Drive provides an alternate route for east-of-Homer traffic to the airport, Spit and harbor, and Ocean Drive commercial district (approximate daily traffic 1,500 vehicles). The road accesses the largest industrial marine storage repair and boat launch complex on the southern peninsula, passes residences, light commercial/industrial businesses, and moose wetlands. Rehabilitation needs have been identified for raising the embankment, surfacing, widening, and drainage improvements as a State project for the road.

Automobile and large truck traffic on Kachemak Drive has increased in recent years, with drivers showing a greater tendency to speed. These conditions make the road treacherous, at best, for bicycle and pedestrian traffic. Construction of a separated pathway along East End Road, as proposed, will increase recreational and commuter bicycle and pedestrian traffic on Kachemak Drive and will improve driver, bicycle, and pedestrian safety. Because of the significant right-of-way acquisition involved, the project to build a separated pathway along Kachemak Drive will take several years to complete. (from CIP, Pg 43)

The purpose of this document is to propose a general route and guidelines for construction details of the path so easements and detailed data can be collected.

All aerial photos in this package are from Google Earth and the Kenai Borough websites. They are not to scale and not current. The ongoing sewer and water improvements are not shown.

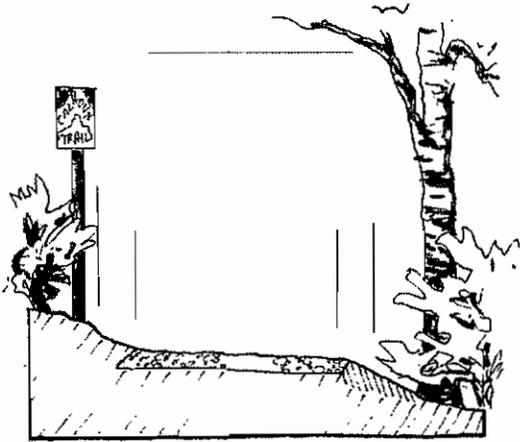
No detailed survey data has been taken for this project. Very rough stationing estimates have been created from available public information, Google Earth and Kenai Peninsula. Detailed survey information needs to be acquired.

The Kachemak Drive Path will attempt to follow existing water, sewer, and electric easements. The public access easements have not been acquired along these utility easements. A letter requesting public access easements from the City to landowners is needed.

Recommendation that the construction of the proposed path be done in a manner that can be upgraded to a higher level in the future.

TYPICAL SECTIONS

LVL3-Dry



LVL5-Ultimate

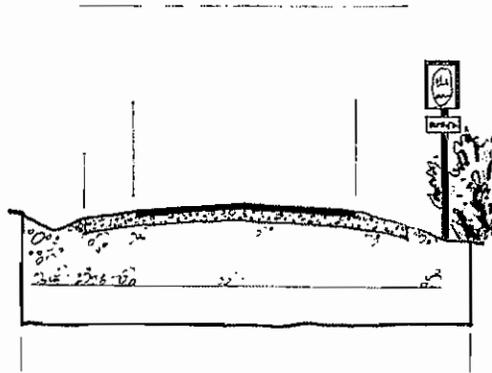


Figure D-15 Puncheon over Wetland

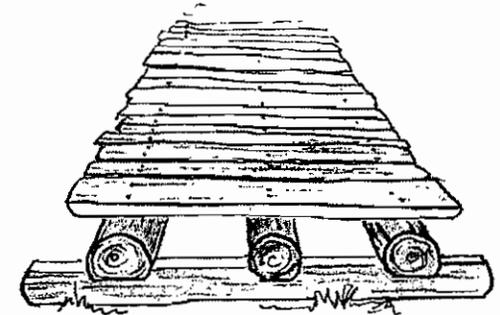
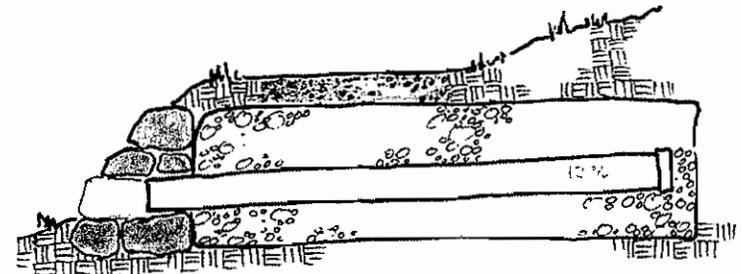


FIGURE D-10 STONE DIP WITH TURNPIKE LOGS

FIGURE D-11 UNDERDRAIN, OR FRENCH DRAIN



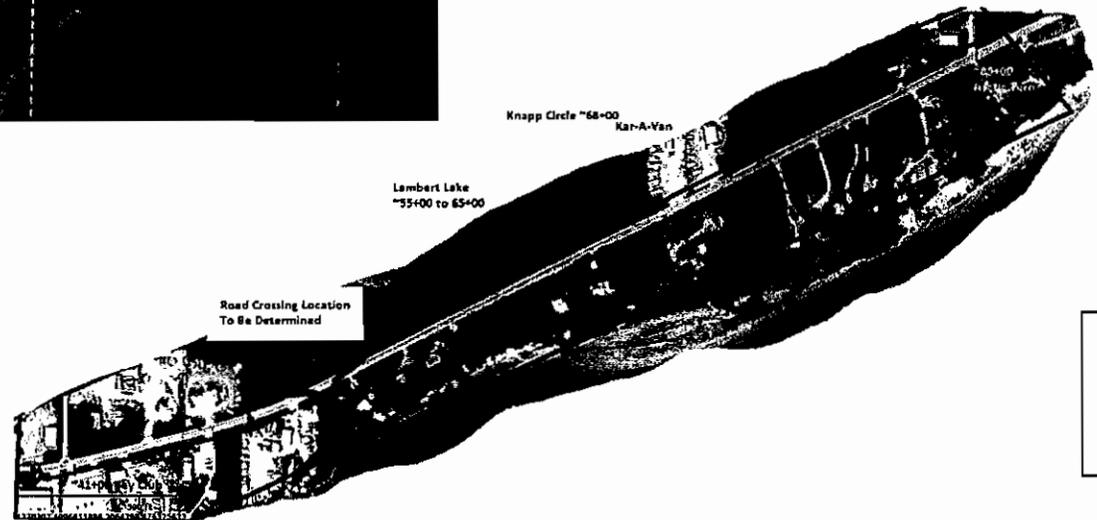
SOURCE OF (some) DRAWINGS: Wetland Trail Design and Construction, USDA Forest Service, 2007.
and CITY OF HOMER PUBLIC ACCESS EASEMENTS; AND TRAILS DESIGN CRITERIA MANUAL

There are several other suggested path types, including bridge sections over gullies, recycled City owned plastic sections near Islands and Oceans, other sections shown in the City of Homer Non-Motorized Transportation & Trail Plan (pgs 3, 17, 32, 33); the Trail Manual (pgs 27, 29, 31, 38, 39, 40, 44, 45, 47, and 49); and other public sources.

Stations Description Sheet 1

- 0+00 to ~5+00 Flat, continue existing Spit Path with separate trail from road
- ~5+00 to ~11+00 Path is on or near toe of embankment, in and out of trees
- ~11+00 to ~28+00 Trail follows beach to existing road that climbs to top of hill, on existing one lane road to beach from parking. Alternate trail would be a new route up the slope (Yellow line)
- ~17+00 to ~28+00 Path is in back of airport long term parking. Remove junk cars, need airport leasing approval.
- ~28+00 to 41+00 Adjacent to, but separate from road to Bay Club

Sheet 2



- ~41+00 to ~49+00 Bay Club to AP Mgr or boatyard road crossing. Exact crossing location to be determined, check sight distances on road, utility obstacles on north side, driveways, and topography. Follow electric or sewer / water easement.
- ~49+00 to ~85+00 Road crossing to Arctic Tern. Follow electric easement. Damp ground by Lambert Lake.

Sheet 3

~85+00 to ~111+00 Arctic Tern to Morris Ave (platted road only). Follow power line? May have to jog to road shoulder to get around private property at ~92+00.



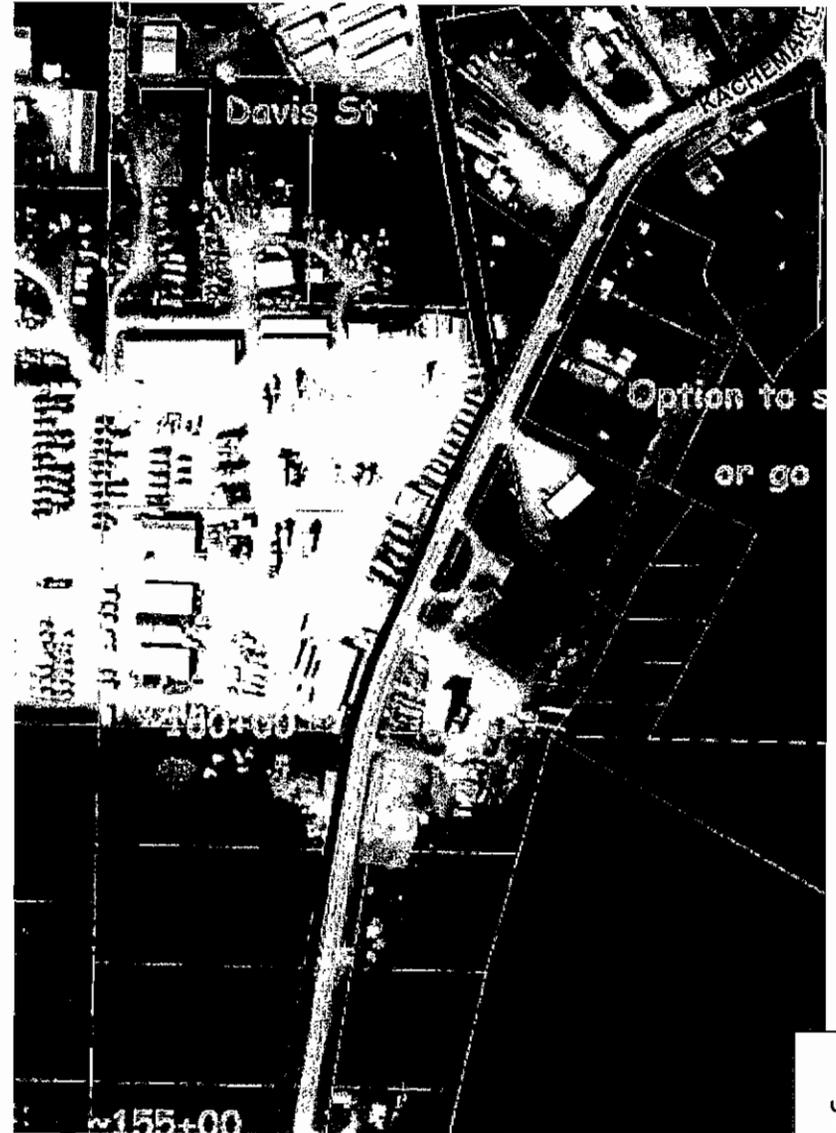
Sheet 4 ~111+00 to ~131+00 Morris Ave thru curves, ----->
follow new sewer line easement?



Sheet 5 ~135+00 to ~152+00 Follow new sewer line easement?



Sheet 6 ~152+00 to ~172+00
Follow new sewer line past the Northern Enterprises boatyard



Sheet 7 ~172+00 to E-End Rd The Davis St option would require about 900' of clearing and possible large culvert installation, then another ~300' to get to East End Road through a congested area between the Gear Shed, a coffee shop, and a bike shop, all good terminations for the trail. The Kachemak Drive option would involve building the trail across several driveways, without substantial drainage issues.



Proposed Funding Plan for Kachemak Drive Path

Utilizing Section VII Trail Prioritizing Criteria and Planning Guidelines of the H.A.R.T Policy manual, the trail would be identified as a high priority trail during the annual review by TAC, (Transportation Advisory Committee) and Parks and Recreation Advisory Commission.

As of 2012 there is an ending balance of \$304,554 in the HART Trail Reserves.
Approximately \$100,000 added annually plus investment interest of \$500 to \$1000 annually.

These funds could be utilized in amounts to be determined for initial surveying, engineering and design work, consultation fees, and basic materials to get started, i.e. trucking, geoblock trail hardening material, culverts, tytar road fabric, treated wood 4x4's and decking rental of equipment.

By utilizing volunteers for basic construction and labor, donated materials, and rented equipment, costs would be much less than if done by a contractor.

Possible sources of funding:

Sale of City owned Lot on Kachemak Dr. , Parcel #17910001, assessed at \$38,100

Homer Foundation

Rasmussen Foundation

ATI, Alaska Trails Initiative Grant

Private donations

Fundraisers

STIP

Corporate Grants - such as R.E.I.

Other future sources of funding as discovered or available.

Maintenance Plan for the Proposed Kachemak Drive Path

Proposed trail-

An 8 foot wide, compacted gravel path from the end of the existing spit trail along Kachemak Drive to East End Road.

1. Annual spring workday by volunteers, litter pick up, minor tread repairs, drainage
2. Encourage individual / groups to utilize Adopt-a-Trail program for a portion or all of the trail.
3. Bi-Annual inspection by City Parks and Recreation Maintenance staff.
4. Use of city ATV and ATV grader to grade the trail twice a year or as needed
- city staff or designated volunteer.

Winter maintenance:

Plow for pedestrians and bikes using city atv with plow, city staff or volunteer

OR

Don't plow, pack and drag for multi-use, ski, snowshoe, bike, pedestrian.

Use snowmachine, roller, drag.

Sign maintenance:

Volunteer / user reporting system for maintenance needs

Adopt-a Trail volunteers

Bike Club

Ski Club

Individual volunteers

Funded by private donations

Grants

HART Funds

City Maintenance as per other city trails

References Cited – Proposed Kachemak Drive Pedestrian Bike Pathway

Preliminary Engineering and Path Design

City of Homer Capital Improvement Plan 2011-2016, Pages i, v, vi, 41, 43, A-11

- Resolution 11-096(A) Page i Page 10
- Introduction: The Capital Improvement Program, Page v, vi Page 11
- State Projects, Page 41 Page 13
- Kachemak Drive Rehabilitation/Pathway, Page 43 Page 14
- City of Homer Financing Assumptions Capital Improvement Program, Page A-11 Page 15

City of Homer Non-Motorized Transportation & Trail Plan, Pages 3, 17, 18, 32, 33

- Introduction, Page 3 Page 16
 - Functional Aspects, Sidewalks, Walkways and Safe Crossings, Page 17 Page 17
 - Figure A: Sidewalks, Walkways and Safe Crossings, Page 18 Page 18
 - Functional Aspects, Connectivity and Safety: Bicycle Transportation, Page 32 Page 19
- Figure 12: Bicycle and Pedestrian Infrastructure
Functional Aspects, Connectivity and Safety: Bicycle Transportation, Page 33
Bicycle Lane Page 20

City of Homer Trail Manual Design Criteria - Non-Motorized Trails and Access Easements

Pages 27, 29, 31, 38, 39, 40, 44, 45, 47, and 49

- Level Three Semi-Improved Trail, Page 27 Page 21
- Level Four - Fully Improved Trail, Page 29 Page 22
- Level Five - High Use Trail, Page 31 Page 23
- D. Trail Design Criteria, Running Grade Criteria by Trail Level, Grade Reversals, Cross Slope & Cut/Fill, Page 38-39 Page 24
- D. Trail Design Criteria, Widths, Page 40 Page 26
- D. Trail Design Criteria, Structures, Page 44-45 Page 27
- D. Trail Design Criteria, Planks with Piles, Cribbing or Bents; Puncheons, Boardwalks Other Techniques; Materials, Page 47 Page 29
- D. Trail Design Criteria, Trail Heads & Parking; Amenities, Benches, Trash & Recycling Receptacles, Lighting, Information, and Bicycle Racks, Page 49 Page 30

References Cited - Proposed Kachemak Drive Pedestrian Bike Pathway

Proposed Funding for the Kachemak Drive Path

City of Homer Accelerated Roads and Trails Program Policy Manual
- Section VII, Trails Prioritizing Criteria and Planning Guidelines, Page 8 Page 31

Resolution 11-090(S)

Climate Action Plan, Page 33 & 34 Page 32

2005 Homer Area Transportation Plan, Pages I-3, I-16, I-17, I-18,
I-24 through I-33 Page 34

Homer Accelerated Roads and Trails Program and Policy (H.A.R.T.) Pages 2, 4, 5, 8 Page 48

CITY OF HOMER
HOMER, ALASKA

Mayor/City Council

RESOLUTION 11-096(A)

A RESOLUTION OF THE HOMER CITY COUNCIL ADOPTING THE 2012-2017 CAPITAL IMPROVEMENT PLAN AND ESTABLISHING CAPITAL PROJECT LEGISLATIVE PRIORITIES FOR FISCAL YEAR 2013.

WHEREAS, A duly published hearing was held on September 26, 2011 in order to obtain public comments on capital improvement projects and legislative priorities; and

WHEREAS, It is the intent of the City Council to provide the Governor, the State Legislature, State agencies, the Alaska Congressional Delegation, and other potential funding sources with adequate information regarding the City's capital project funding needs.

NOW, THEREFORE BE IT RESOLVED by the City Council of Homer, Alaska, that the "City of Homer Capital Improvement Plan 2012-2017" is hereby adopted as the official 6-year capital improvement plan for the City of Homer.

BE IT FURTHER RESOLVED that the following capital improvement projects are identified as priorities for the FY 2013 State Legislative Request:

1. Sewer Treatment Plant Bio-solids Treatment Improvements
2. Homer Area Natural Gas Pipeline, Phase 2
3. Harbor Improvement Revenue Bond Projects (Bundled Projects)
4. Skyline Fire Station
5. Fishing Lagoon Improvements
6. Karen Hornaday Park Improvements, Phase I
7. Tanker 2 Refurbishment and Fire Engine 4 Refurbishment HVFD
8. Homer High School Track Renovation
9. Alternative Water Source
10. Deep Water/Cruise Ship Dock Expansion, Phase I
11. Homer Intersection Improvements
12. Ocean Drive Reconstruction with Turn Lane
13. Mariner Park Restroom
14. Kachemak Drive Rehabilitation/Pathway
15. Truck Loading Facility Upgrades at Fish Dock

BE IT FURTHER RESOLVED that projects for the FY 2013 Federal Legislative Request will be selected from this list.

BE IT FINALLY RESOLVED that the City Manager is hereby instructed to advise appropriate State and Federal representatives and personnel of the City's FY 2013 capital project priorities and take appropriate steps to provide necessary background information.

PASSED AND ADOPTED by a duly constituted quorum of the City Council for the City of Homer on this 10th day of October, 2011.



[Signature]
 JO JOHNSON, CMC, CITY CLERK

CITY OF HOMER
[Signature]
 JAMES C. HORNADAY, MAYOR

Introduction: The Capital Improvement Program

A capital improvement plan (CIP) is a long-term guide for capital project expenditures. The CIP includes a list of capital projects a community envisions for the future, and a plan that integrates timing of expenditures with the City's annual budget. The program identifies ways projects will benefit the community. The CIP also indicates the priorities assigned to different projects and presents a target construction schedule.

A carefully prepared capital improvement plan has many uses. It can assist a community to:

- Anticipate community needs in advance, before needs become critical;
- Rank capital improvements needs so the most important projects are given consideration for funding before projects not as urgently needed;
- Plan for maintenance and operations costs so expenses are budgeted in advance and projects communities cannot afford to operate are avoided;
- Provide a written description and justification for projects submitted for state funding so the legislature, governor, and appropriate agencies have the information necessary to make decisions about funding capital projects; and
- Provide the basis for capital projects as part of the annual budget.

A capital improvement project is one that warrants special attention in the municipal budget. Normally, public funds are not expended if the project is not listed in the CIP. A capital expenditure should be a major, nonrecurring budget item that results in a fixed asset with an anticipated life of at least two years. Projects eligible for inclusion in the City of Homer CIP have a lower cost limit of \$50,000 for City projects and \$25,000 for those proposed by non-profit organizations. Projects proposed by non-profit organizations and other non-City groups may be included in the CIP with City Council approval but such inclusion does not indicate that the City intends to provide funding for the project.

The municipality's capital improvement plan is prepared in accordance with a planning schedule, usually adopted by City Council at the onset of the CIP process. A copy of the City of Homer CIP schedule appears in the appendix of this document.

The number of years over which capital projects are scheduled is called the capital programming period. The City of Homer's capital programming period coincides with the State's, which is a six year period. The CIP is updated annually, since only some of the projects are funded and completed each year.

A capital improvement plan is not complete without public input. The public should be involved throughout the CIP process, including nomination and adoption stages of the process. The City of Homer solicits input from City advisory bodies, advertises for public input during the CIP public hearing, and invites the public to participate throughout the entire process.

The City's capital improvement program integrates the City's annual budget with planning for larger projects that meet community goals. The CIP program involves a process where the City Council, with technical support from the administration and ideas and suggestions from the public, compiles a viable way to implement goals for the community.

Determining project priorities. City of Homer CIP projects are assigned a priority level of 1, 2, or 3, with 1 being the highest priority. To determine priority, the Council considers such questions as:

- Will the project correct a problem that poses a clear danger to human health and safety?
- Will the project significantly enhance City revenues or prevent significant financial loss?
- Is the project widely supported within the community?
- Has the project already been partially funded?
- Is it likely that the project will be funded only if it is identified as being of highest priority?
- Has the project been in the CIP for a long time?
- Is the project specifically recommended in other City of Homer long-range plans?
- Is the project strongly supported by one or more City advisory bodies?

Once the overall CIP list is finalized, the City Council names a subset of projects that will be the focus of efforts to obtain state and/or federal funding in the coming year. The overall CIP and the legislative priority list are approved by resolution.

Integration of the CIP with Comprehensive Plan Goals

Each project listed in the CIP document has been evaluated for consistency with the City's goals as outlined in the Comprehensive Plan. The following goals were taken into account in project evaluation:

- Land Use: Guide the amount and location of Homer's growth to increase the supply and diversity of housing, protect important environmental resources and community character, reduce sprawl by encouraging infill, make efficient use of infrastructure, support a healthy local economy, and help reduce global impacts including limiting greenhouse gas emissions.
- Transportation: Address future transportation needs while considering land use, economics, and aesthetics, and increasing community connectivity for vehicles, pedestrians, and cyclists.
- Public Service & Facilities: Provide public services and facilities that meet current needs while planning for the future. Develop strategies to work with community partners that provide beneficial community services outside of the scope of City government.
- Parks, Recreation & Culture: Encourage a wide range of health-promoting recreation services and facilities, provide ready access to open space, parks, and recreation, and take pride in supporting the arts.
- Economic Vitality: Promote strength and continued growth of Homer's economic industries including marine trades, commercial fishing, tourism, education, arts, and culture. Preserve quality of life while supporting the creation of more year-round living wage jobs.
- Energy: Promote energy conservation, wise use of environmental resources, and development of renewable energy through the actions of local government as well as the private sector.
- Homer Spit: Manage the land and other resources of the Spit to accommodate its natural processes, while allowing fishing, tourism, other marine-related development, and open space/recreational uses.
- Town Center: Create a community focal point to provide for business development, instill a greater sense of pride in the downtown area, enhance mobility for all forms of transportation, and contribute to a higher quality of life.

State Projects

The City of Homer supports the following state projects which, if completed, will bring significant benefits to Homer residents:

Transportation projects within city limits:

Homer Intersection Improvements
Kachemak Drive Rehabilitation/Pathway
Main Street Reconstruction/Intersection
Ocean Drive Reconstruction with Turn Lane
Pioneer Avenue Upgrade

Transportation projects outside city limits:

Sterling Highway Reconstruction, Anchor Point to Baycrest Hill
Sterling Highway Realignment, MP 150-157

Non-transportation projects:

Alaska Maritime Academy

See following pages for project descriptions.

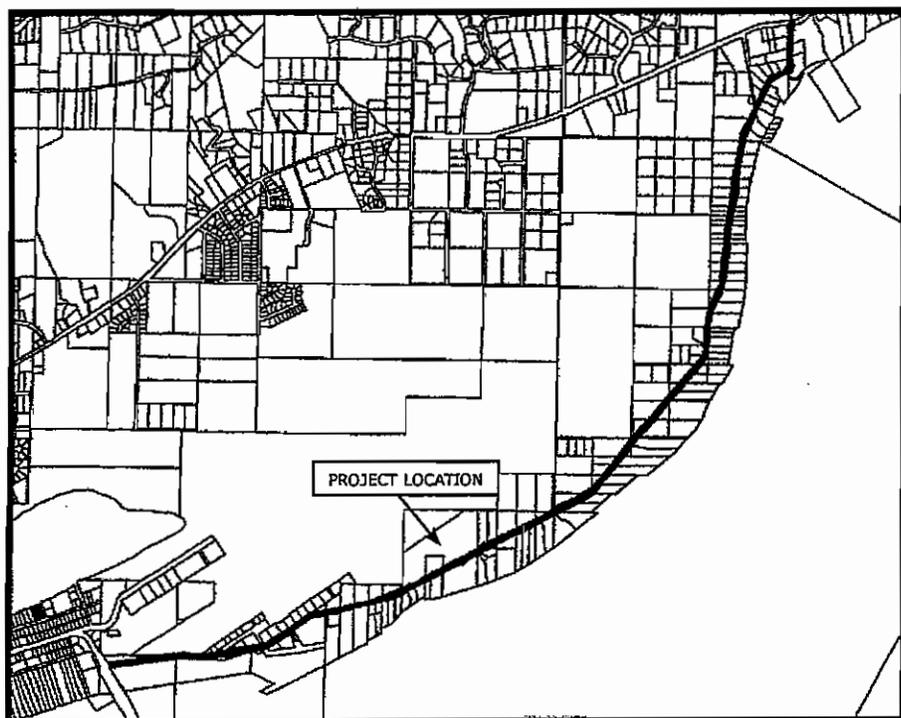


City of Homer Capital Improvement Plan • 2011 - 2016

Kachemak Drive Rehabilitation/Pathway

PROJECT DESCRIPTION & BENEFIT: Kachemak Drive provides an alternate route for east-of-Homer traffic to the airport, Spit and harbor, and Ocean Drive commercial district (approximate daily traffic 1,500 vehicles). The road accesses the largest industrial marine storage repair and boat launch complex on the southern peninsula, passes residences, light commercial/industrial businesses, and moose wetlands. Rehabilitation needs have been identified for raising the embankment, surfacing, widening, and drainage improvements.

Automobile and large truck traffic on Kachemak Drive has increased in recent years, with drivers showing a greater tendency to speed. These conditions make the road treacherous, at best, for bicycle and pedestrian traffic. Construction of a separated pathway along East End Road, as proposed, will increase recreational and commuter bicycle and pedestrian traffic on Kachemak Drive and will improve driver, bicycle, and pedestrian safety. Because of the significant right-of-way acquisition involved, the project to build a separated pathway along Kachemak Drive will take several years to complete.



**CITY OF HOMER FINANCING ASSUMPTIONS
CAPITAL IMPROVEMENT PROGRAM**

Implementation of the City of Homer Capital Improvement Plan requires utilization of various financing mechanisms. Financing mechanisms available to the City of Homer include:

- Federal grants or loans
- State grants or loans
- General obligation bonds
- Limited obligation bonds
- Revenue bonds
- Special assessment bonds
- Bank loans
- Pay-as-you-go
- Private sector development agreements
- Property owner contributions
- Lease or lease-purchase agreements

The use of any of the financing mechanisms listed above must be based upon the financial capability of the City as well as the specific capital improvement project. In this regard, financing the CIP should take into consideration the following assumptions:

1. The six-mill property tax limitation precludes utilizing General Fund operating revenue to fund major capital improvements. Available revenue should be utilized to fund operation and maintenance activities.
2. The operating revenue of enterprise funds (Port & Harbor, Water & Sewer) will be limited and as such, currently only fund operation and maintenance activities.
3. The utilization of Federal and State grants will continue to be significant funding mechanisms. Grants will be pursued whenever possible.
4. The 1½ percent sales tax approved by voters of Homer for debt service and CIP projects is dedicated at ¾ percent to sewer treatment plant debt retirement with the remaining balance to be used in water and sewer system improvement projects, and ¾ percent to the Homer Accelerated Roads and Trails (HART) Program.
5. The HART Program will require property owner contribution of \$30 per front foot for road reconstruction, with an additional \$17 per front foot for paving.
6. The Accelerated Water and Sewer Program will require substantial property owner contributions through improvement districts/assessment funding, set currently at 75 percent.
7. The private sector will be encouraged to finance, construct, and operate certain non-essential capital improvements (e.g., overslope development).
8. The utilization of bonds will be determined on a project-by-project basis.
9. The lease and/or lease-purchase of capital improvements will be determined on a project-by-project basis.

Introduction

Homer is a community whose charm, outdoor culture, and beautiful natural surroundings draw hundreds of thousands of visitors each year. It is known for its world-class halibut fishing, thriving arts community, adventurous outdoor activities, numerous festivals, and the Homer Spit, a 4.5-mile narrow slip of land that curls outward into Kachemak Bay. This small coastal hamlet has captured the hearts of almost everyone who lives or visits here and the imaginations of many who hear of this place "at the end of the road." Despite this success, Homer faces economic development challenges, especially in its central business district. Most of the community's visitors spend their time on the Homer Spit where one can book fishing tours and kayak adventures, shop along the boardwalk, stroll the beaches, visit the harbor, and dine in a number of restaurants, all without needing a car.

In an effort to promote economic development throughout the community, the City of Homer applied for and received a grant from the Alaska Department of Economic and Community Development to create a comprehensive non-motorized transportation and trail plan. Developing and implementing such a plan is essential for enhancing the community's image as a wonderful place to live, visit, and engage in business. By establishing a truly superb trails network that enables visitors and residents alike to travel safely and comfortably through Homer without the need for an automobile, the community will capitalize on its outdoor culture and unmatched natural setting.

...a development manual to be consulted, referred-to, dog-eared, rained-on, coffee-stained, finger-print smeared, and otherwise continuously used...

Bekiga Slough Boardwalk

1.0

Introduction

To complete and connect the community's pedestrian system, walkways (referred to as separated pathways in the bicycle transportation section of this report) should be added to the following locations:

- Beluga Lake Trail System
(connecting Ben Walters Park to Paul Banks Elementary and beyond to East End Road)
- Chamber of Commerce Trail
(connecting the Homer Chamber of Commerce with the Islands and Ocean Visitor Center)
- Homer Coastal Trail
- Homer Public Library Trail
(between library and Poopdeck Trail)

- * Kachemak Drive Pathway
- Soundview Avenue Trail
(between West Homer elementary and Soundview Avenue)
- W.R. Bell Trail
(connecting Fairview Avenue to Reber Road)
- Smoky Bay Way
- Waddell Way
- Wright Street

A map of these routes appears in Figure A.

"Walking revitalizes me. After one day on the trail I become different from the way I am at home. I am in touch with the seasons, the weather, the varied hours of each day. I see more keenly. I am aware of the details."

-Marlyn Doan, 'Hiking Light', 1982

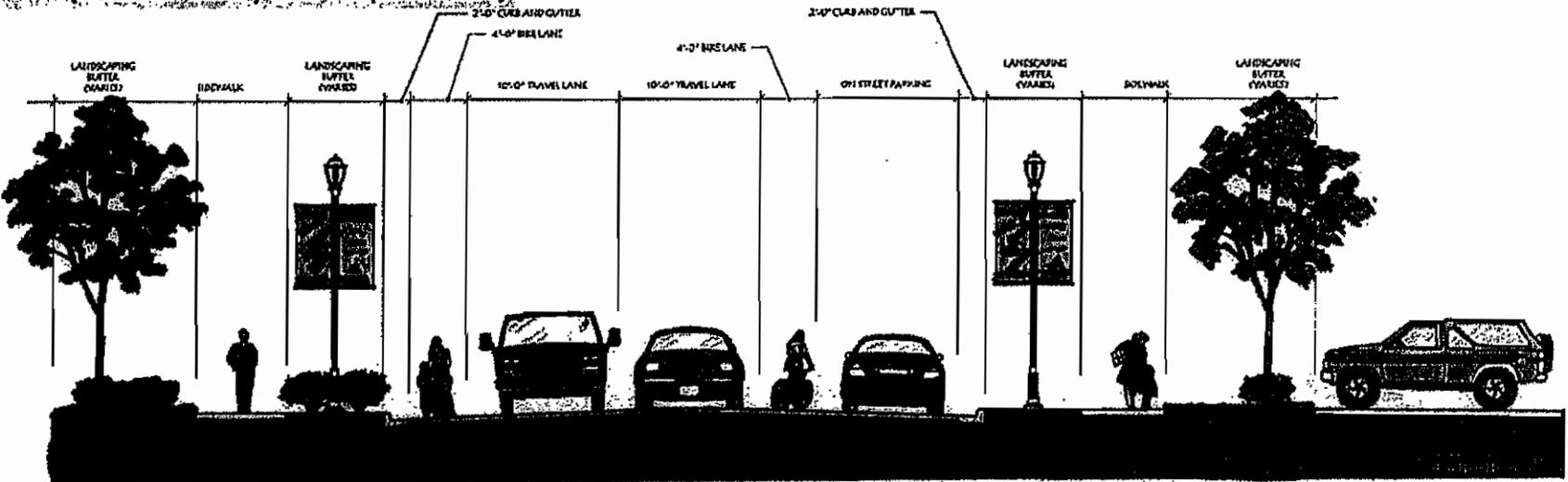
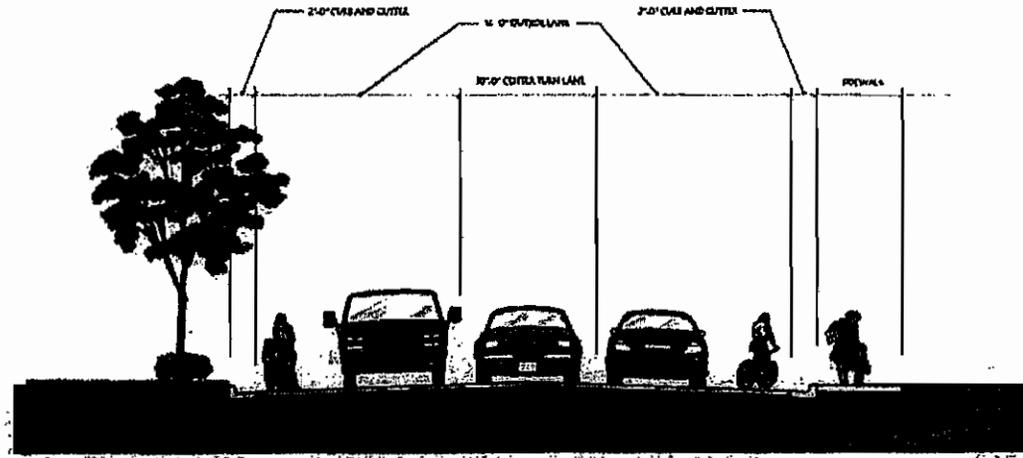


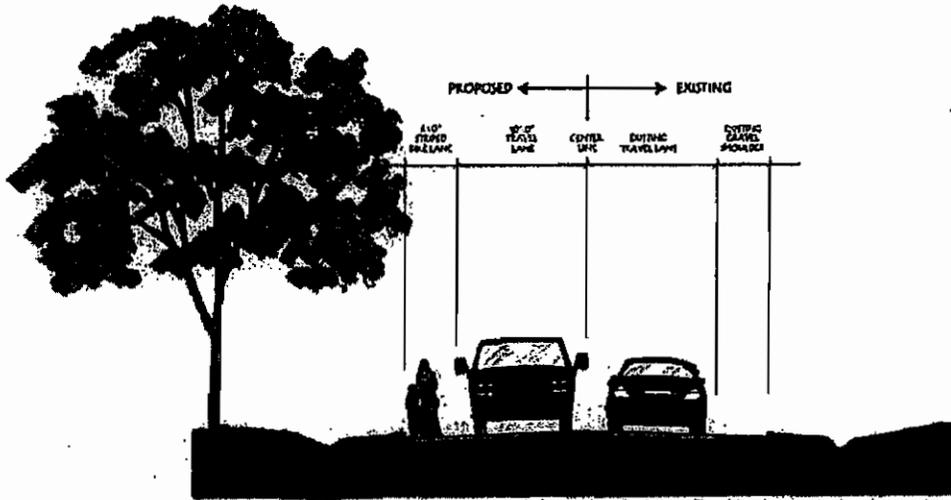
Figure 7: Pioneer Avenue



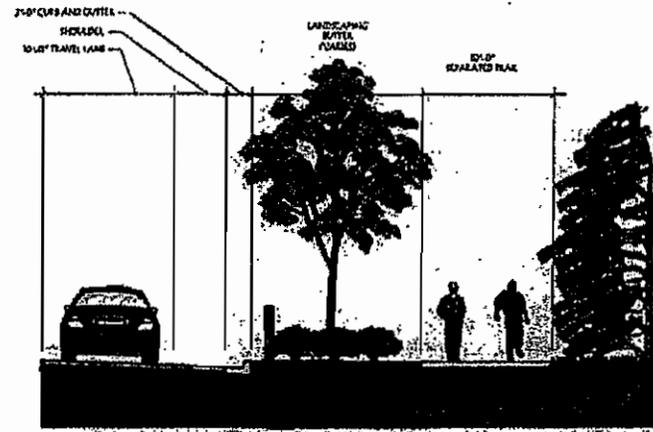
Figure A: Sidewalks, Walkways and Safe Crossings



WIDE OUTSIDE LANE SECTION
NOT TO SCALE



PAVED SHOULDER SECTION
NOT TO SCALE



SEPARATED PATHWAY SECTION
NOT TO SCALE

Figure 12: Bicycle and Pedestrian Infrastructure

Bicycle Lane

Bicycle lanes are designated portions of paved roadways intended for bicycle traffic. In Homer this type of facility is appropriate for arterial and collector roads and in locations where bicycle traffic is significant. Bicycle lanes are one-way only and travel with the flow of traffic. Streets that include bicycle lanes should have smooth pavement, a schedule of regular maintenance to repair potholes and remove debris, and drainage inlets that are flush with the pavement surface and have bicycle-safe grates designed to prevent trapped bicycle tires. Bicycle lanes are created by striping and stenciling pavement with lines and symbols. They are inexpensive to install on existing roads. Possible locations for bicycle lanes include the following routes:

Pioneer Avenue
Main Street
Sterling Highway
(between Pioneer Avenue and Lake Street)
East End Road
(between Lake Street and East Hill Road)
Kachemak Drive
(until a separated pathway is constructed)
Bunnell Avenue
(between Main Street and Beluga Place)
Beluga Place

A map of Homer's existing and proposed bicycle lanes appears in Figure B.

Wide Outside Lane

A wide outside lane is not differentiated from the vehicle travel way with striping. It is a paved travel lane that is wide enough to comfortably accommodate both bicycle and vehicular traffic. A wide outside lane measures 14 feet unless there is on-street parking or a steep grade in which case a width of 15 feet is more appropriate. A lane less than 14 feet does not provide comfortable maneuvering room for both motorized and non-motorized vehicles while a wider lane may lead motorists to believe that there is more than one lane available for travel. Wide outside lanes are appropriate on arterials, roads with high traffic volumes and speeds, roads without shoulders, and in restrictive urban environments such as busy commercial districts. In Homer, possible locations for wide outside lanes include the following routes:

Sterling Highway
(between Pioneer Avenue and West Hill Road)
Soundview Avenue
Fairview Avenue
Bayview Avenue
Lake Street
Heath Street
Bartlett Street

A map of these routes appears in Figure B.

"Toleration is the greatest gift of the mind; it requires the same effort of the brain that it takes to balance oneself on a bicycle."

-Helen Keller, deaf and blind US lecturer, 1880-1968



City of Homer, Alaska

City of Homer
Non-Motorized Trail Planning & Design Criteria Summary
Level 3 Semi-Improved Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

TRAIL DESIGN CRITERIA

Trail Width & Shoulders 3 - 5 foot wide improved trail.

- 3 - 4 foot wide trail - for routes with lower volumes of traffic, and one-way or no bicycle use.
- 5 foot wide trail - for routes with moderate to high pedestrian volumes and/or two-way bicycle or equestrian uses.
- Trails should widen in areas of switchbacks, turns, steep side slopes, and as needed near structures or amenities.

Surface 4 inches NFS gravel over geotextile fabric, which may be placed over native vegetation. Alternate surfacing: porous pavement panels filled with native or imported material. Medium duty boardwalk or bridges where needed. Generally clear, with protrusions <4 inches and steps to 10 inches.

Clearance

- Vertical clearance - 8 feet minimum. Optimum 12 feet for winter and equestrian users.
- Horizontal clearance - 12 in. beyond trail edge. 24 in. from signs, trees or structures.

Grade

- Target grade < 8%, with grade reversals as needed to control erosion.
- 15% maximum for up to 50 feet.

Cross Slope of Trail

- Target cross slope - 3%, flowing to downside of tread, or to uphill side, if a drainage ditch is provided.
- Maximum - 10%

Signage

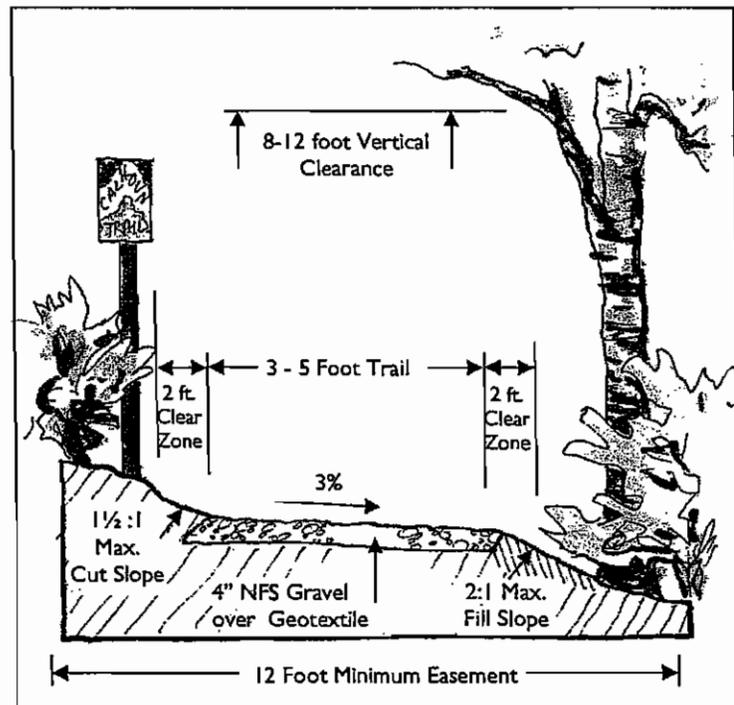
- Trail markers (as needed) to navigate winter use trails.
- Trail Information signage posted at each end of the trail: Trail system map (if appropriate), trail name, length, use restrictions or accessibility warnings, and resource protection information.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Few amenities, as approved by City of Homer, such as bear proof trash receptacles, trail heads, benches for rest or viewing, interpretive signs, such as at interesting historic or natural features.

Structures

- Medium duty structures, as needed.
- Elevated plank crossing of wetlands, creeks.
- Few railings or boardwalks.
- Log, timber or rock retaining structures for cut / fill edges, as needed.



CROSS SECTION - LEVEL 3 SEMI-IMPROVED TRAIL

City of Homer
Non-Motorized Trail Planning & Design Criteria Summary
Level 4 - Fully Improved Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

DESIGN CRITERIA

Trail Width & Shoulders 5 - 8 foot wide paved or gravel trail.

- 5 - 6 foot wide trail - for routes with lower volumes of traffic, and fewer recreational users.
- 7 - 8 foot wide trail - for routes with bicycles and/or moderate to high user volumes.
- PAVED TRAILS - where a Level 5 trail is recommended, but topography or other physical conditions prevent construction to Level 5 standards, a paved Level 4 trail is acceptable. Any Level 4 trail can be paved. Provide a minimum 12 in. gravel shoulders on all paved trails.

Surface Firm and stable. Smooth, few or no obstacles. Protrusions <3 in. Steps to 8 in. Remove surface vegetation and organic soils. For gravel trails: 2 in. leveling course over 8 in. NFS gravel over geotextile. For paved trails: 2 in. AC pavement over 2 in. leveling course over 24 in. NFS gravel over geotextile. Alternate surfacing: PPP filled with native or imported material.

Clearance

- Vertical clearance - 9 feet above trail and shoulders, 12 feet for equestrian use.
- Horizontal clearance - Minimum 12 in. beyond trail edge. 24 in. from signs and trees.

Grade & Accessibility

- Accessible trails: Target grade $\leq 5\%$, 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.
- Maximum: 10% for up to 50 feet.
- Stairs used where absolutely necessary and pedestrians are the primary user group.

Cross Slope of Trail

- Gravel trails - 3%
- Paved trails - 2%
- Shoulders - 10% Max.

Signage

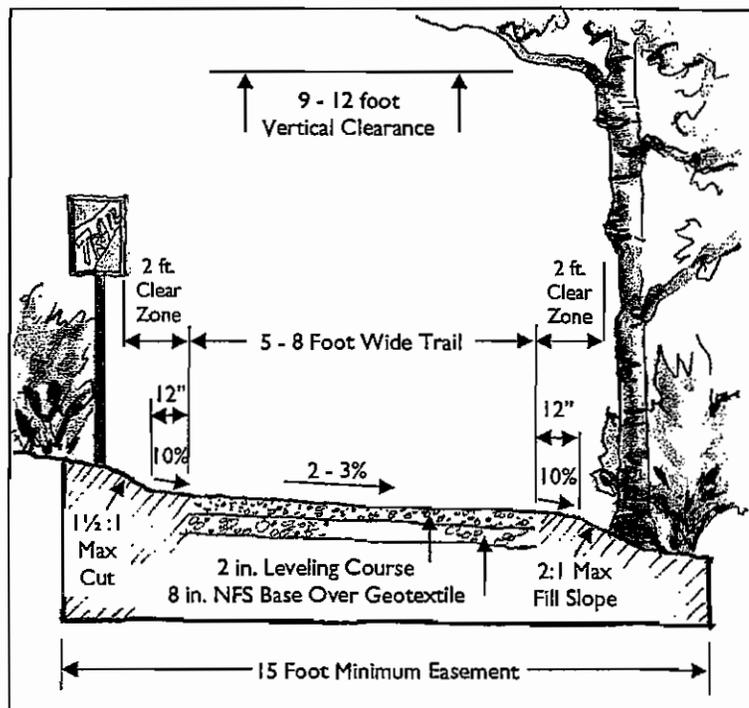
- Trail information signage posted at ends and intersections, as necessary, such as a trail system map, trail name, use restrictions, accessibility warnings, and resource protection information.
- Directional signs for nearby destinations, traffic control and warnings for intersections or other trail conditions.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Amenities common. Lighting, bear proof trash & recycling receptacles, maps, benches for rests or viewing, and interpretive signs, as approved.

Structures

- Heavy duty structures, as needed: bridges, boardwalks, retaining structures, railings.



CROSS SECTION - LEVEL 4 FULLY IMPROVED TRAIL

City of Homer Non-Motorized Trail Planning & Design Criteria Summary Level 5 - High Use Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

TRAIL DESIGN CRITERIA

Trail Width & Shoulders. 8- 12 foot wide paved trail with 2 foot wide gravel shoulders.

- 8 foot Trail - for routes with lower volumes of traffic, few recreational users, or space limitations.
- 10 foot wide trail sections are the standard.
- 12 foot wide trails are recommended where traffic volumes are high, bicycles and in-line skates are common, near intersections with other trails or streets, as the trail approaches a bridge, where grades exceed 5% and handrails are provided, or near points of interest along the trail.
- **ALTERNATE TRAIL DESIGN** - Where trail is highly recreational, with bicycles, equestrians, joggers, an alternative design of 6 foot wide paved trail with 4 foot shoulders on each side or a trail with one 2 foot and one 6 foot wide shoulder is allowable. Or, provide a separated dual trail, one paved, one gravel, with a vegetated median in-between.

Surface. Uniform, firm and stable. Pavement or boardwalk. Smooth, no obstacles. Protrusions <2 inches. Construct using 2 in. AC pavement over 2 in. leveling course over 24 in. NFS gravel over geotextile fabric.

Clearance.

- Vertical clearance - 9 feet above trail and shoulders, 12 feet for equestrian use.
- Horizontal clearance - Minimum 24 inches beyond trail edge. 36 inches for posts and structures.

Grade

- Accessible Trails: Target grade ≤ 5%, 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.

Cross Slope of Trail

- Target cross slope - 2% Shoulders - 10% Max.
- Maximum, where needed for driveway crossings or other intersections - 3%

Signage

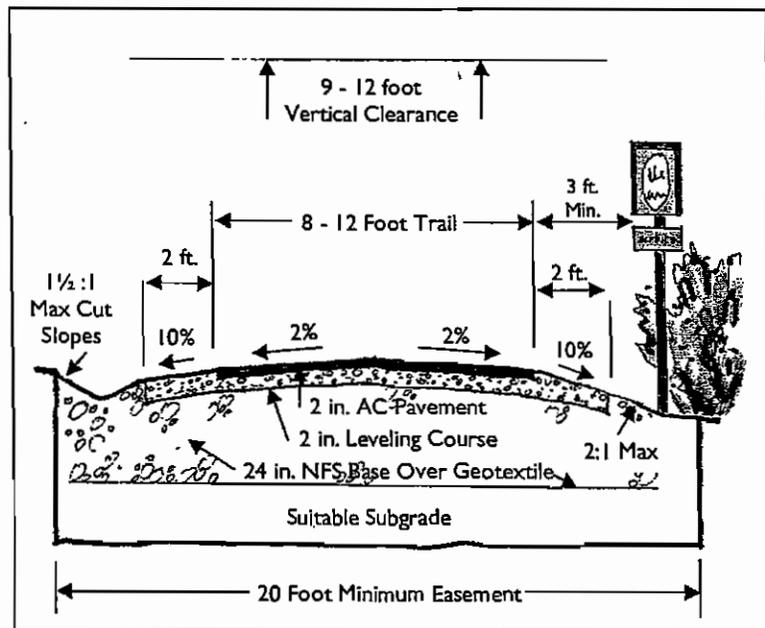
- Trail information signage posted at ends and intersections, as necessary: Trail system map (if appropriate), trail name, use restrictions or accessibility warnings, and resource protection information.
- Directional signs for nearby destinations, traffic control and warnings for intersections or other trail conditions.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Amenities common. Lighting, bear proof trash & recycling receptacles, maps, benches for rests or viewing, and interpretive signs, such as at historic or natural features.

Structures

- Heavy duty structures, as needed: bridges, boardwalks, retaining structures, railings.



CROSS SECTION - LEVEL 5 HIGH USE TRAIL

D. TRAIL DESIGN CRITERIA

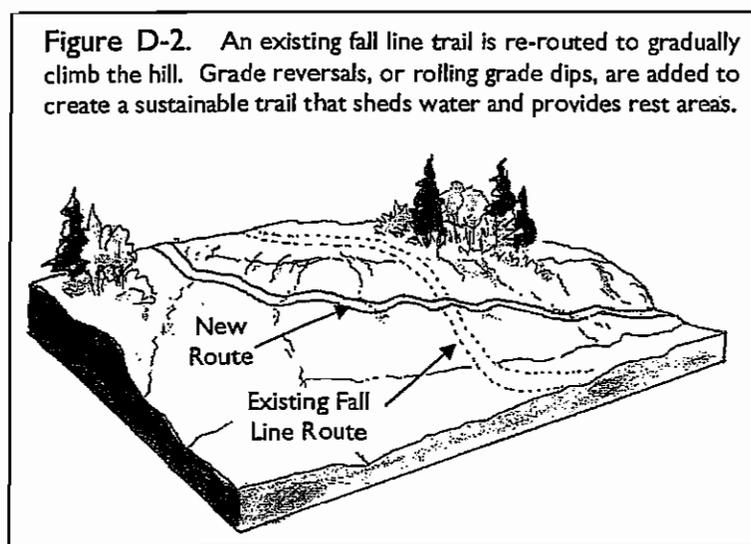
- Compliance would substantially alter the nature of the setting or the purpose of the facility, or portion of the facility.
- Compliance would require construction methods or materials that are prohibited by Federal, State, or Local Regulations or Statutes.
- Compliance would not be feasible due to terrain or the prevailing construction practices.

v) Running Grade Criteria by Trail Level.

- LEVEL 1:** Maximum grade is based primarily on the ability of the trail to resist erosion caused by trail use, surface water, or wet soils. Target grade <12%. Maximum 20% for trails where underlying soils are sand, silt, or clay. 20%- 30% for gravel or rock base. For grades over 30%, natural trail base and surface must be composed of angular rock, large rock or solid rock. Provide grade reversals every 20-50 feet. Construct steps to minimize erosion.
- LEVEL 2:** Target grade: <10%. Maximum: 20% for distances up to 50 feet. Use on-site cut and fill to soften dips or peaks in trail corridor.
- LEVEL 3:** Target grade: < 8%. Maximum: 15% for up to 50 feet.
- LEVEL 4/5:** Target grade: \leq 5%. Maximum: 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.

b. Grade Reversals

A grade reversal is a change in the direction of running grade, from an upslope grade to a down slope grade. They are used on unpaved trails to prevent erosion that is caused by water running *along* the surface of a trail versus *across* the trail. They should be provided every 20-50 feet along the trail corridor.



c. Cross-Slope & Cut / Fill

All trails require enough cross-slope to shed water off the trail surface, but not so much that it impacts the comfort or safety for the trail user. Managing surface water drainage along a trail corridor is critical to maintaining a safe and long lasting trail. Poorly managed drainage can erode soils and destroy vegetation. Keeping water moving across the surface of a trail will prevent ponding, erosion, and icing.

Steep side slopes (> 30%) are a common obstacle to the construction of trails on Homer's hillside terrain, and often trigger the need for extensive cut and fill to "fit" a trail into a hillside. Careful planning can minimize expense and environmental damage.

D. TRAIL DESIGN CRITERIA

i) General Cross-Slope and Cut / Fill Criteria:

- All construction-related disturbance, including areas of cut or fill, shall occur within the limits of the easement;
- Limits of cut and fill should be in proportion to the construction level of the trail. For example: low level trails justify very little cut / fill, high level trails may utilize the entire easement for most of the length of the trail;
- Maximum 1 ½ :1 (75%) cut slopes, maximum 2:1 (50%) fill slopes. Where soils are unstable, sandy, or saturated, 3:1 (33%) max slopes are recommended.
- For trails along side slopes of 30% or greater, construct the trail on the cut bench portion only. Avoid locating the trail on fill portions of the side slope;
- Provide retaining structures, as needed to minimize disturbance and to improve accessibility on Level 3, 4 or 5 trails;
- Construct trails to ensure water flows across or under the trail surface, not along the trail. Where it is necessary to run the water along the trail, it should be contained in a ditch with provisions made to protect against erosion. Ditch length should be minimized by diverting runoff across the trail at the nearest point feasible.
- To accommodate vision-impaired or wheelchair users on Level 4 or 5 trails with an adjacent fill slope, provide a vertical barrier along the cut slope edge of the shoulder, such as vegetation, or a minimum 3 in. curb or barrier.

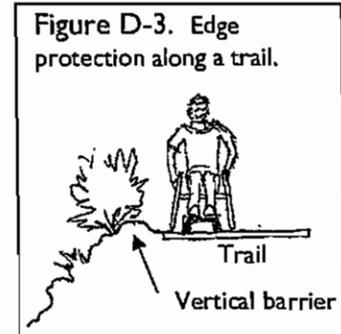


Figure D-3. Edge protection along a trail.

ii) Criteria by Trail Level

LEVEL 1: Target cross slope is 3-10%. Maximum is up to the natural side slope. If the trail is designed for mountain bikes, cross slope maximum is 10%. Very minimal cut and fill. Little or no use of (rustic) retaining methods.

LEVEL 2: Target cross slope: 5%. Maximum: 10%. For ski trails, if bicycles are not allowed, steeper side slopes may be allowed. Minimal cut and fill as necessary to meet criteria and soften dips, ruts, bumps or peaks.

LEVEL 3: Target cross slope is 3%. Maximum is 10%. Cut and fill as needed to meet design criteria. Rock or timber used for most retaining needs.

LEVEL 4: Gravel trails: Target cross slope: 3%, Max.: 4%. Paved trails: target cross slope: 2%, Max.: 3%. Cut and fill may be significant, as needed to meet design criteria. May likely extend to edges of easement for much of the trail length. Imported materials for retaining structures common.

LEVEL 5: Target cross-slope is 2%. Where necessary, such as when crossing driveways, a cross-slope of 3% is allowable. Paved surfaces must be uniform enough to prevent ponding and icing. Shoulders should slope away from the paved sections of the trail with a target slope of 3%, and a maximum of 10%. Cut and fill may extend to the outer edges of the easement. Retaining structures common.

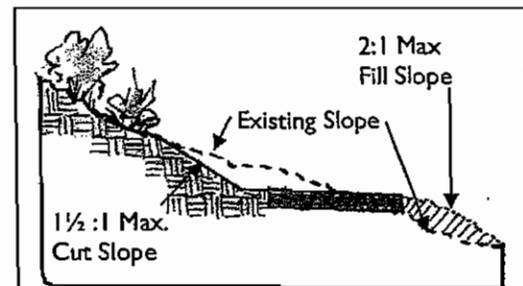


Figure D-4. A Full Bench Trail, placed on cut portion of the slope only, provides a more stable base than a trail placed on fill material.

iii) Re-vegetation. All cut / fill slopes should be vegetated with native species. Attempts should be made to salvage and stockpile existing vegetation for re-use on cut / fill slopes. Avoid reseeding with non-native species.

D. TRAIL DESIGN CRITERIA

4. WIDTHS

The complete trail cross-section is composed of the easement, the trail surface, the shoulders, and the clearance zone. The desired width is primarily related to the volume and mix of users. Secondary considerations include topography, curves, intersections, structures, and amenities.

Table D-5

REQUIRED EASEMENT WIDTHS	
LEVEL 1:	8 Feet
LEVEL 2:	20 Feet
LEVEL 3:	12 Feet
LEVEL 4:	15 Feet
LEVEL 5:	20 Feet

a. Easement Width

The following criteria apply to easement widths:

- A narrower portion of easement may be allowed when available space is limited by existing structures or property boundaries, for a short duration of the trail, and the narrow segment of the trail does not create a safety hazard or an uncomfortable trail segment of trail;
- Vary the easement width as needed to accommodate switchbacks or turns;
- Wider easement sections are allowed where existing side slopes require additional cut and fill, and retaining structures are not feasible, and the widened area is not extensive.

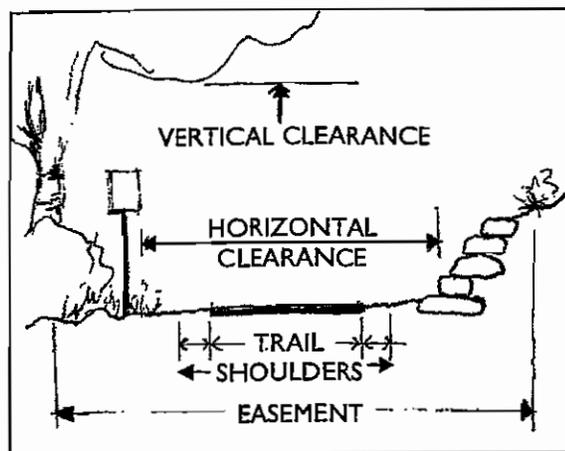
b. Trail Width

The width of the trail surface, or tread, is determined by the volume and type of users, as well as the nature of the terrain and the trail surface. Always provide for the user with the most demanding needs.

LEVEL 1: Trail tread width may range from 6 - 24 inches. Consistent width along the length is preferred, but not required on this level of trail. Natural obstacles and topography may both affect variability of the tread width. Provide 24 in. width when the trail is expected to attract mountain biking, equestrians, snow-shoeing, or skiing.

LEVEL 2: There is typically not a constructed trail tread for recreation corridors. They are a specified width of area that is cleared of woody vegetation and obstacles, mowed (optional), and identified with trail markers for use as a recreation corridor. Minimum width for an un-programmed low use corridor is 6 feet. Groomed ski trail routes require up to a 16 foot wide mowed corridor.

FIGURE D-5 Trail Profile



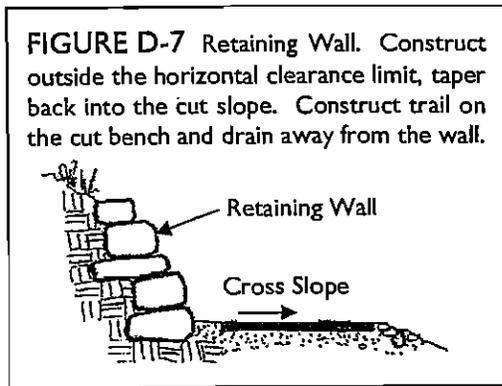
LEVEL 3: Widths may range from 3-5 feet. Safety may be a concern on narrow trails with a mix of pedestrians, bicycles and equestrians, even if the volumes are low. It cannot be expected that bicycles will use these routes as "one-way" trails, or stay off them altogether, so it is imperative that they be designed to mitigate potential hazards. For trails that will expect regular use by bicycles, overall use volumes are moderate, or hills are frequent, the width should be 5 feet. Narrower trails are allowed for lower use trails, but horizontal clearance and sight stopping distance should both be increased, curves widened, and passing areas provided at a minimum of every 1000 feet.

D. TRAIL DESIGN CRITERIA

LEVEL 5: Base: Remove vegetation and organic soils. 24 in. NFS gravel over geotextile over suitable soils. Surface: 2 in. AC pavement over 2 in. leveling course. For bridges and wet crossings: wood, synthetic, recycled plastic, treated wood, or metal.

6. STRUCTURES

Where trails cross creeks or traverse areas where existing grades or side slopes are too steep to construct the trail without excessive disruption to adjacent areas, structures may be necessary.



a. Retaining Walls

Construct all retaining walls outside the horizontal clearance limit of the trail. Retaining walls higher than 24 in. on the down slope side of a trail are discouraged. Where necessary, they should include a railing, for safety. Retaining wall materials vary depending on the level of the trail, with rock, concrete block, or timbers used on higher level trails and on-site materials, such as logs or rocks used on lower level trails. Where seeps occur behind retaining walls, provide method to ensure drainage through and under the wall.

b. Steps or Stairs

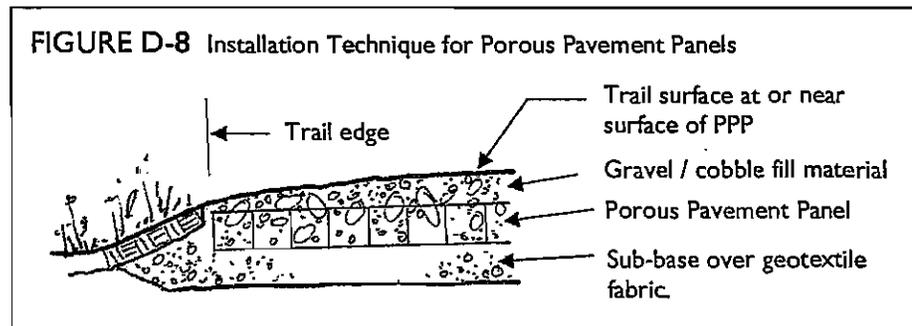
Steps and stairs are obstacles to many trail users, and are to be avoided, where possible. As needed, construct steps on Level 1 trails using on site materials, such as rocks. Only when all other options, including ramps, have been ruled out, are stairs allowed on Level 3, 4 or 5 trails. When stairs are necessary, consider providing long perron style steps, as strollers and wheelchairs can maneuver them easier.

c. Ramps

Along required ADA accessible pedestrian routes, sections of trail greater than 5% may be considered ramps, and are allowed for limited lengths (see section 3. GRADE & CROSS SLOPE).

d. PPP (Porous Pavement Panels)

These are three dimensional structural grids designed to provide a durable wear surface and load distribution system in wetland and other degradable soils



D. TRAIL DESIGN CRITERIA

e. Bridges

Bridges should be designed for pedestrian live loads and for maintenance or emergency vehicles if they may be expected to cross the bridge. Bridge decking should be designed with bicycle safe expansion joints or planks laid perpendicular to the trail direction unless bicycles are not allowed or not expected. Bridge widths should be the same as that of the approach trail plus 2 feet clear area on each side. Bridge decking should be flush with the approaching trail surface.

f. Railings

Railings are provided for safety on elevated trail segments, such as bridges. All railings should be engineered to withstand all loads that may be expected to occur on the bridge. The type of railing that is required is determined by the accessibility level of the trail, and fall into three basic types:

i) **Urban Setting.** Railings in highly pedestrian urban settings must meet International Building Code (IBC) requirements. Railings must be at least 42 inches high with vertical rails to prevent climbing, and be spaced to not allow a 4-inch sphere to pass through. Railings are required on ADA accessible ramps.

ii) **Rural Bridges.** Handrails on bridges or crossings, that are elevated at 30 inches or more, on accessible trails, such as Level 4 & 5 trails, need to meet AASHTO standards for pedestrian highway bridges. These standards require a 6-inch sphere must not pass through the railing in the bottom 27 inches, and an 8-inch sphere must not pass through the area higher than 27 inches. It also requires that the top railing is at least 42 inches for bicycles use, and 54 inches high for equestrian traffic. Rails should also be horizontal to prevent wheels and other objects from catching. All accessible trail bridges that do not have a rail system must have a minimum 3 inch high curb.

iii) **Remote Bridges.** For bridges in remote areas with a drop of 30 in. or more, railing requirements must meet OSHA standards. For typical crossings along Level 1, 2 & 3 trails, handrails are required to be at least 42 inches high for pedestrian traffic and 54 inches high for bicycle and equestrian traffic. They must include an intermediate rail so that vertical distances between rails do not exceed 15 inches between 2x4 wood rails or 19 inches between steel rails.

iv) **Railing Exceptions.** Not all trail bridges require railings. An analysis should be completed to identify and evaluate the bridge's potential users and the hazards of not having a rail system, including situations where a railing is provided on only one side. As a general rule, a remote trail or bridge with a drop of 8 feet or more, should have a pedestrian railing system.

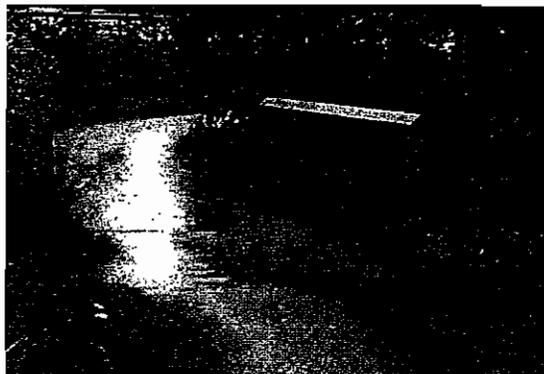
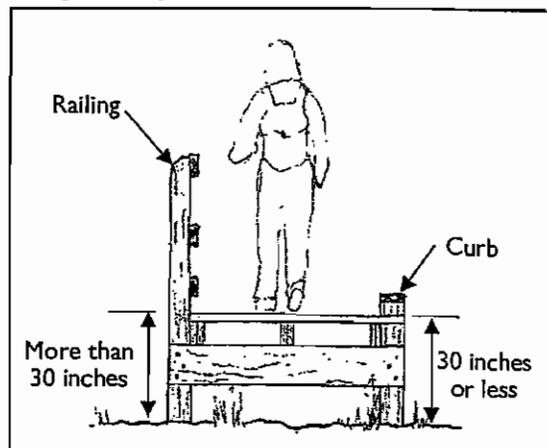


FIGURE D-9 Bridge, railing and typical warning sign on a Level 5 Trail (Urban setting).

FIGURE D-10



D. TRAIL DESIGN CRITERIA

iv) **Planks with Piles, Cribbing or Bents.** An elevated trail technique where one or more tread planks are laid parallel to the trail corridor, attached to piles, cribbing, or bents. Choice of support method depends on type of wetland, range of water depth, user volumes, size of trail. Piles are not recommended on low level trails, due to the depth needed to prevent frost heaving.

v) **Puncheons.** A crossing technique for low water areas that utilizes sleepers. Some have linear planks, others also have stringers to support perpendicular decking, which is necessary for bicycle travel.

vi) **Boardwalks.** These are the most substantially constructed form of elevated crossings. They use piles, diagonal bracing, stringers, and planking laid perpendicular to the direction of travel. They often include curbed edges or railings, and can be constructed to suit many user groups, including bicycles and wheelchairs.

vii) **Other Techniques.** Avoid using ditches, culverts or other channelization techniques to divert water, as they may create issues with landslides and super-saturation of soils. Corduroy, turnpikes and causeways are all variations of at-grade wetland crossings, each with their pros and cons. Use of these may be appropriate in some situations, but they are typically not the most environmentally friendly.

c. Materials

Choose materials that are long-lasting and environmentally safe. More investment is expected on higher level trails.

FIGURE D-12 Log Cribbing with Two Sleepers

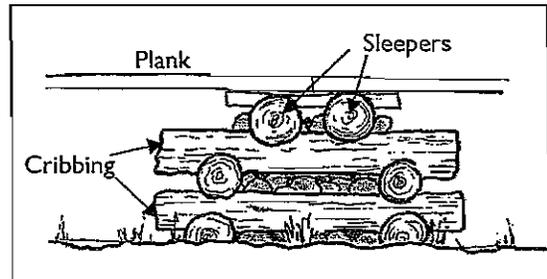


FIGURE D-13 Bog Bridge with Sleepers, or Single Plank Boardwalk

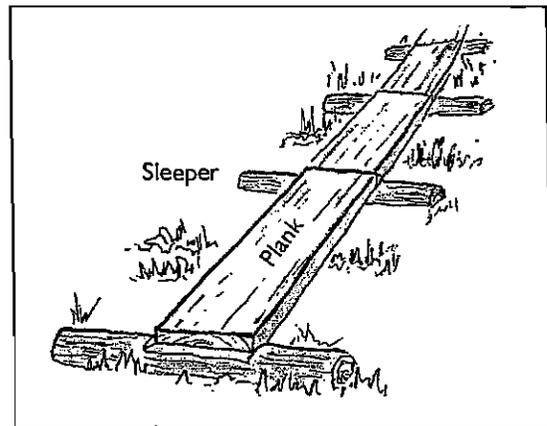


FIGURE D-15 Puncheon

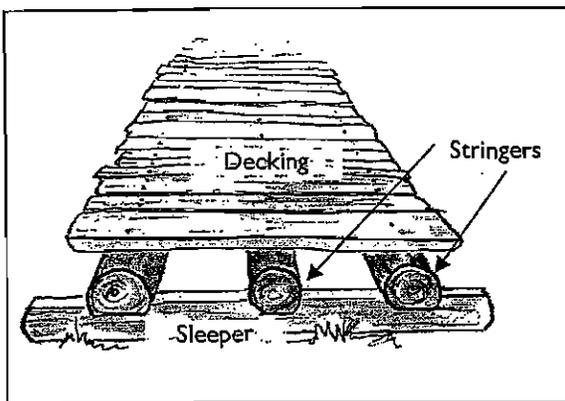
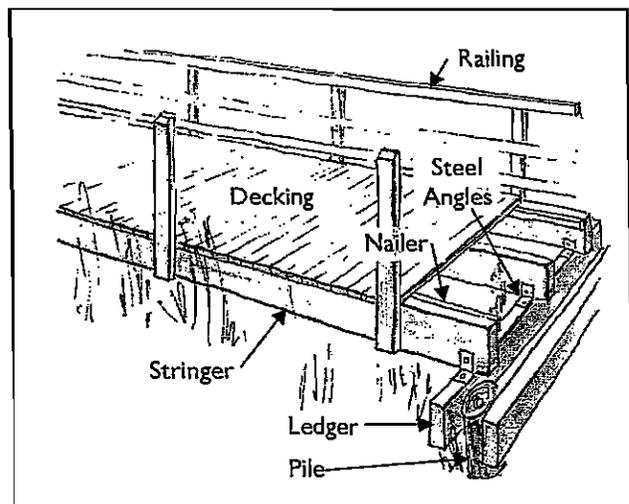


FIGURE D-14 Boardwalk



SOURCE OF DRAWINGS: Wetland Trail Design and Construction, USDA Forest Service, 2007.

D. TRAIL DESIGN CRITERIA

d. Trail Heads & Parking

Provide adequate parking, signage and staging areas as needed to accommodate various recreational activities on trails. Amenities such as maps, educational information, trash receptacles, seating, and other trail information are all possible features found at trail heads. Place trail heads and parking areas at the most logical locations along the trail, typically at ends.

9. AMENITIES

Trails are expected to serve many purposes including transportation, recreation, education and social interaction. Amenities, such as benches, trash receptacles, lighting, interpretive panels, and structures are appropriate and necessary for a trail network that meets these objectives. Generally, the higher level trails require more amenities. All amenities should be located outside the trail's clear zone. All amenities provided on accessible trails must also be accessible.

a. Benches

Benches are integral to recreation facilities, and can be used to provide seating for resting, socializing, or viewing. They should be provided at crests of hills, at midpoints of long inclines, in conjunction with other trail amenities, near recreation areas such as playgrounds, and at overlooks or viewpoints along a trail. All benches should meet ATBCB Guidelines for Recreation Facilities.

b. Trash & Recycling Receptacles

Provide bear proof facilities for trash and recycling along higher level trails in locations such as trail heads, rest areas, & interpretive facilities. Locate these facilities for easy maintenance.

c. Lighting

Lighting provides safety and comfort on trails used for transportation, which is primarily Level 4 and Level 5 trails. Where ambient lighting from nearby areas is not adequate to light the trail, additional pedestrian scale lighting may be advisable on these trails, especially at intersections.

d. Information

Trail maps, interpretive information is useful and appropriate in many circumstances along trails, such as to provide information on nearby historic, cultural or natural features. Such amenities enhance the user experience and also protect those community assets. Provide a minimum 4 feet clearance between informational amenities, such as interpretive signs and kiosks, and the edge of the trail.

e. Bicycle Racks

Provide bicycle racks at trail heads, parking areas, and other destinations along the trail corridor. Provide a minimum 4 feet clearance between bicycle racks and the trail.



FIGURE D-18 Trail widens to accommodate interpretive signage.

developer shall construct the streets on the alignment adopted in the Master Roads and Streets Plan, and conforming to the respective classification. The developer shall be required to construct the street to a twenty-eight-foot width in accordance with the minimum requirements of a local residential street; provided, however, that the City may, upon direction of the City Council, elect to require construction to the full standards and pay to the developer the cost difference between the required street and the proposed street. (Resolution 07-82)

VII. TRAIL PRIORITIZING CRITERIA AND PLANNING GUIDELINES

A. Trail Prioritizing. The TAC and Parks and Recreation Advisory Commission will review the trail priority list during the annual review of the HART. The list will be presented in a memorandum from staff, and will contain a mix of large and small projects. Generally it will include up to five trail projects that staff has reviewed and found ready for preliminary work. Trails on this list are planned for construction in the near term (one to three year timeframe). Staff will actively work to prepare those projects for construction. (Resolution 07-82)

B. Trail Planning Guidelines

Trail design shall take into account at minimum the following:

1. Use context sensitive design when locating and planning trails to take advantage of scenic resources.
2. Respect the character of trails based on function, setting, and expectation of accessibility.
3. Evaluate the soils, drainage, wetlands, Tsunami zone, flood plain, stream setbacks, historical resources, visual resources, topography, existing and potential land use, zoning and land ownership.
4. Where estimated costs, operating costs and outside funding availability are considerations and important criteria, care should be used to ensure that important trails are not eliminated solely using cost as a determinant.
5. Multi-use trails are encouraged. Design of the trail should include consideration of compatible uses such as pedestrians and bicycles.
6. All trails should be designed to recognize the requirements of ADA standards and guidelines. (Resolution 07-82)

LAND USE/PLANNING & ZONING

Through land use planning, including transportation planning, local government has enormous influence over community energy use and greenhouse gas emissions. Development that reduces the need to drive and encourages non-motorized and public transportation will significantly reduce the use of fossil fuels.

Many of these same strategies (often referred to as "smart growth") help reduce costs in providing services. Denser "cluster development," for example, reduces taxpayer expenditures for water and sewer lines, road construction and road maintenance, and street lights. These reductions are tied directly to reductions in energy use and greenhouse gas emissions. Another benefit of more compact development is that less soil is disturbed, which helps prevent the release of CO₂ stored in the soil.

In September 2007, the Urban Land Institute and the National Center for Smart Growth Research and Education at the University of Maryland published *Growing Cooler: The Evidence on Urban Development and Climate Change*. After reviewing dozens of empirical studies, the authors predict that if sprawling development continues to fuel growth in driving, the increase in total miles driven will overwhelm expected gains from vehicle efficiency and low-carbon fuels. Lead author Reid Ewing stated, "The research shows that one of the best ways to reduce vehicle travel is to build places where people can accomplish more with less driving."

Depending on several factors, from mix of land uses to pedestrian-friendly design, compact development reduces driving from 20 to 40 percent, and more in some instances. Typically, Americans living in compact urban neighborhoods where cars are not the only transportation option drive a third fewer miles than those in automobile-oriented suburbs, the researchers found.

At the same time, the book documents market research showing a shift in future housing demand to smaller homes and lots, townhouses, and condominiums in neighborhoods where jobs and activities are close at hand. Homer planners and developers would be wise to recognize the benefits of such development and encourage these trends.

The City of Homer will develop and implement land use plans that explicitly recognize the urgent need to reduce greenhouse gas emissions.

1. Support current (or stronger) language in the draft Homer Comprehensive Plan Update that calls for denser, more compact development and increased emphasis on developing infrastructure for non-motorized transportation.
2. Update City planning and zoning regulations to promote land use strategies that include compact, mixed-use development, higher density development, and infill.

"Planners in local government...are planning things that will be here in 50 and 100 years. Every single thing should be demonstrating sustainability. Every single one should have significant reductions in greenhouse gases, particularly in transportation, built in. If not, planners will be seen as absolute pariahs by their children and grandchildren. They'll say 'How could you have done that when everyone knew?'"

—Urban planner and author Peter Newman

“Compact development provides an insurance policy against the worst effects of climate change and oil price spikes. In the worst case, current or future residents of compact development will have a variety of viable transportation options, while the residents of sprawl will not.”

*—from Growing Cooler:
The Evidence on Urban
Development and Climate
Change*



Compact mixed-use development that includes sidewalks and trails creates a pleasant environment and encourages residents and visitors to walk instead of drive. (Photo from “Growing Cooler: The Evidence on Urban Development and Climate Change”)

3. Implement the City of Homer Non-Motorized Transportation and Trails Plan, including construction of specific trails, sidewalks, and safe crossings recommended in the plan, and revisions to Homer City Code, Title 21, to require non-motorized circulation systems.
4. Institute traffic calming measures and “complete street” designs to make bicycling and walking safer and more pleasant. (“Complete streets” are those which are designed to encourage and safely accommodate pedestrians, bicyclists, and transit users as well as automobiles. A number of cities and states have passed complete-street legislation.)
5. Develop Homer’s “Town Center” in line with the guiding principles in the Homer Town Center Development Plan and use this as a model for future development as Homer grows.

The City of Homer will make use of the permitting process to encourage development that helps reduce greenhouse gas emissions.

1. Provide assistance to developers and builders in evaluating plans to increase energy efficiency and promote non-motorized transportation.
2. Adopt building codes and incentives to increase energy efficiency in all new residential and commercial development.
3. Keep abreast of new LEED (Leadership in Energy and Environmental Design) standards for neighborhood development and building remodeling and consider adopting these standards in the permitting process.

1 INTRODUCTION

The Homer Area 2001 Transportation Plan was produced to be a comprehensive transportation-planning tool for the City of Homer. Additionally, the Non-Motorized Trails and Transportation Plan should be considered a companion document to the Transportation Plan, as it is the City's policy document for comprehensive long-range non-motorized transportation and trails system. This study will provide a general guide for planning and funding requests for road and trail transportation capital improvement projects for the next 20 years. This plan is not intended to identify site-specific locations of improvement.

The City of Homer (COH) obtained funding for this study from the Alaska Department of Transportation and Public Facilities (DOT/PF), and has retained Mike Tauriainen, P.E., Consulting Engineers, Inc. as the prime consultant to perform this study. Other members of the consultant planning team include Land Design North (trails element), Kinney Engineering (transportation engineering), Bechtol Planning & Development (planning), and Brooks & Associates (transit engineering).

The 2001 Draft Transportation Plan was completed but not adopted. In Fall 2003 the City of Homer Road Standards Committee began reviewing the goals, objectives, and recommendations of the 2001 Draft Transportation Plan. The Homer Advisory Planning Commission took up discussion of the Draft Transportation Plan following the Roads Standards Committee. This plan reflects the plan as recommended by the Road Standards Committee.

The Road Standard Committee and the Homer Advisory Planning Commission further recommend that where ever this plan recommends signalization that alternatives, such as roundabouts, be seriously considered.

3 TRANSPORTATION FUNDING

3.1 Local Funding

The City of Homer has created the Homer Accelerated Roads Program (HARP); the intent of which is to reconstruct local substandard city streets and/or upgrade existing city streets and rights-of-way. Improvements funded under this program reduce maintenance costs, improve access, increase property values and improve the quality of life. The program utilizes dedicated City sales tax not to exceed three quarters of one percent supplemented by assessments against benefited properties. The program provides for the sale of revenue bonds to cover the cost of the projects. Abutting property owners generally share the cost of upgrading a street to residential standards by paying \$30/front foot for gravel improvements and \$17/front foot for paving. There has been some discussion about revising the program to more closely meet the current needs of the community.

3.2 State of Alaska

The State of Alaska Department of Transportation and Public Facilities publishes a State Transportation Improvement Program (STIP) on a three-year cycle. For roads and highways, the STIP funds projects under 4 categories:

- National Highway System, NHS (Includes Sterling Highway);
- State Highway System, SHS (established for this current cycle, no SHS roads in Homer);
- Community Transportation and Economic Development Program, CTP (any street or road nominated by local and borough governments); and,
- Trails and Recreational Access for Alaska, TRAAK (Trails, enhancements).

The 1998 to 2000 STIP and the 2001 to 2003 STIP has identified projects for the Homer area. Figure I-4 shows approximate total Statewide STIP projects, and those within Homer as listed in the 1998-2000 and 2001 to 2002 STIPs.

Figure I-4: Statewide STIP and Funding Dedicated to Homer Area

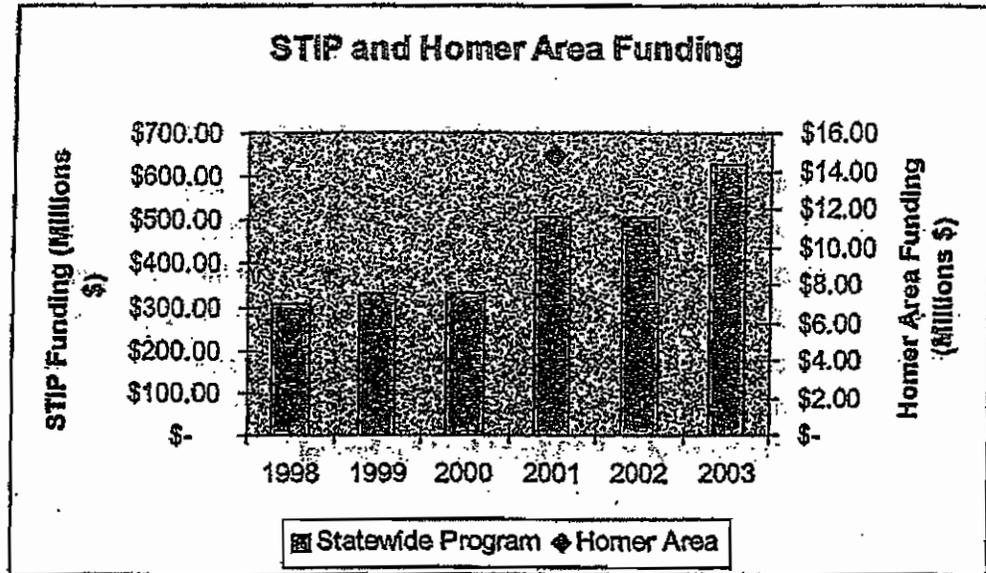


Table I-7 presents the street and roads needs for Homer, and is from the DOT/PF website (http://www.dot.state.ak.us/cgi-bin/profneeds.d/need_list).

Table I-7: State of Alaska Road and Trail STIP Needs for Homer

Name	Description	Estimate	STIP Category
Bartlett/Hohe Street	Rehab Bartlett St. from Pioneer St. to Fairview Ave. (2000). Rehab Hohe St. from Fairview Ave. to the South Peninsula Hospital (1000). Work on both streets includes lowering grade, minor realignments within existing ROW, widening driving lanes to current standards, water/sewer stubouts, storm drains, curb & gutter, sidewalks, and lighting at street intersections.	1,000,000	CTP
Baycrest/Homestead Ski/Trailhead Rehabilitation	Construct a year round trailhead that provides off-street parking on Rogers Loop Road.		TRAAK
Bunnell St/Olsen Lane Rehabilitation	Reconstruct and pave 1 mile of urban streets within the Homer City limits. Project scope and cost estimates are modified to include lighting and sidewalks.	1,100,000	CTP
East End Road Improvements	Rehabilitate and resurface Homer East End Road.		
East End Road Safety Trail MP 9.5 to MP 11.9	Construct a separated pedestrian/bicycle facility along East End Road from MP 9.5 to MP 11.9		TRAAK

Name	Description	Estimate	STIP Category
East End Road: MP 0.0-3.6	Rehab from Lake Street (MP 0.0) to the Kachemak Drive (MP 3.6). Reconstruct the road base, drainage, vertical and horizontal curves, pedestrian facilities and resurface. Sidewalks on both sides will be constructed to the elementary school from that point a separated trail will be constructed on the uphill side with four shoulders on both sides of the road to Kachemak Drive.	9,600,000	CTP
East End Road: MP 3.6 to 12.5	Rehabilitate from the intersection with Kachemak Drive (MP 3.6) to the McNeil Canyon School (MP 12.5). Rehabilitate the roadway; provide shoulders; improve drainage; provide pedestrian facilities; and address safety concerns as warranted. Pedestrian facilities will be considered between MP 9.5 and MP 11.8. Also includes separated path pathway from Kachemak Drive to Chelsea Street and from Hunter Road to McNeil Canyon School.	9,850,500	CTP
End of the Road Wayside Park Construction	Construct a highway wayside at the beginning of the Sterling Hwy and adjacent to the AMHS terminal and dock which consists of a rest and viewing area, toilet facilities, parking, pathway and interpretive signs.		TRAAK
Freight Dock Road	Upgrade and pave Freight Dock Road from Sterling Highway to Homer Deep Water Dock (4,000').	200,000	CTP
Homer Mooring Improvements	Replace the existing Homer ferry terminal marine structures.	3,500,000	NHS
Homer Scenic Overlook	Construct a scenic overlook adjacent AMHS deck/parking area on Homer Spit.	600,000	TRAAK
Homer: East End Road: MP 14.2 to 22.0 Rehabilitation - Phase II	Rehabilitate roadway from the McNeil Canyon School (12.5) to the vehicle parking turnaround at Vosnesenka (MP 22). The project will include widening, realignment, drainage improvements and resurfacing.	12,765,000	CTP
Kachemak Drive	Rehabilitate Kachemak Drive from the Sterling Hwy to East End Road, 3.5 miles in length. Work includes improving and raising the embankment, surfacing, widening and drainage improvements. Pedestrian facilities will be evaluated.		CTP
Kachemak Drive Pathways	Construct a pathway along Kachemak Drive from East End Road to the Sterling Highway (approximately 3.5 miles).	1,000,000	TRAAK
Kenai Peninsula Road and Trail Improvements	Rehabilitate, resurface, and pave approximately 3 miles of Skyline Drive from the end of pavement on West Hill Road to the intersection with East Hill Road in Homer. Construct a trail connection and landscaping at the intersection of Lake Street and the Sterling Highway.		
Mission Avenue	Improve drainage, replace soft areas in the road base and hard surface approximately one mile.	750,000	CTP

5 EXISTING STREET CLASSIFICATION

Streets provide dual functions of access and mobility. The degree to which these functions are served determines the street classification. The primary reference for street classification is the American Association of State Highway and Transportation Officials "A Policy on Geometric Design of Highways and Streets". In this reference, AASHTO uses the broad classifications of arterials, collectors, and local streets as a hierarchical system.

Arterial Streets emphasize mobility. Although, arterial streets can provide access, the street is usually designed to carry higher volumes at higher speeds, attributes that usually conflict with safe access.

Local Streets are those streets, which emphasize access and penalize mobility. These will have frequently spaced driveways, will be designed for low speeds and low volumes, and may have a high degree of pedestrian or bicycle use, parking, on-street delivery, and other landside functions that spill into the street and street right-of-way.

Collector Streets provide an intermediate link in the hierarchy between local streets and arterials. Typically, many local streets will connect with a collector, and many collectors to an arterial. Collectors often provide a mixed function of both access and mobility.

The study area roads and streets are presented on Plate 1. Arterial and collector streets are identified. All other streets are local streets.

AASHTO further defines the demographic environment for streets and roadways as urban (population areas > 50,000 people), small urban areas (between 5,000 and 50,000 people), and rural areas (those outside of the urban areas). Street classifications are modified with the adjective of either "rural" or "urban". Based on a population of about 4,000, the streets in Homer will be rural. However, as a practical matter, the roadways serve an area population that extends far beyond the City of Homer boundaries, and this area-wide population is well above 5,000. It should be noted that the DOT/PF has categorized all of their roads in the study area as rural.

AASHTO also subdivides arterials into "principal arterials" and "minor arterials", and subdivides collectors into "major collectors" and "minor collectors". The reader is directed to AASHTO for these definitions. DOT/PF adheres to AASHTO sub-classes for their streets.

The State of Alaska DOT/PF Central Region publishes the Annual Traffic Volume Report that lists functional classifications and Annual Average Daily Traffic (AADT) for selected streets and roads. Table I-8 shows the data from the 1999 report.

Table I-8: Homer Area Roads

CDS Route	Name	Class	1999 AADT	AADT Location
110000	Sterling Highway	Rural Principal Arterial	8,300	Lake St.
110000	Homer Spit Rd. (Sterling Hwy)	Rural Principal Arterial	4009	Between End of the Spit and Kachemak Bay Drive.
110100	Pioneer Avenue	Rural Major Collector	7,300	Lake St. and East End Rd.
110150	Lake Street	Rural Major Collector	5,550	Sterling Hwy.
110200	Kachemak Bay Drive	Rural Major/Minor Collector	1,720	Sterling Hwy.
110300	East End Road	Rural Major Collector	7,700	Lake St. / Pioneer Ave.
110305	East Hill Road	Rural Minor Collector	1,800	East End Rd.
110500	Bartlett Street	Rural Minor Collector	1,270	Pioneer Rd.
110610	Bunnell Avenue	Rural Minor Collector	1,930	Olson Lane
110615	Olson Lane	Rural Minor Collector	470	Sterling Hwy.
110625	Main Street	Rural Major Collector	2,770	Sterling Hwy.
110716	Mission Road	Rural Minor Collector	125	East Hill Rd.
110800	West Hill Road	Rural Minor Collector	1,400	Sterling Hwy.
110900	Skyline Drive	Rural Minor Collector	460	East Hill Rd.
111300	Diamond Ridge Road	Rural Minor Collector	250	Sterling Hwy.

6 TRAFFIC MODELING AND FORECASTS

6.1 Methodology

This study uses the Quick Response System II (QRS II) for transportation models. The Quick Response System (QRS) was formulated in the 1970s as a set of manual techniques that could be used on planning problems too small for the computer technology of the time. These methods are documented in the National Cooperative Highway Research Program Report #187. It has evolved into the Windows-based QRS II, which is packaged with the General Network Editor (GNE), which serves as a data input module to QRS II. QRS II software employs all four steps of the modeling process and uses the inputs of development and land use, economic information, and transportation system attributes.

The existing network system of collectors and arterials was entered into a 1999 baseline model. This network system describes speeds, lanes, approach configurations, type of intersection control, and inherent delays. The year 1999 was selected because there is good roadway volume information that can be used for calibration. Only the arterials and collectors were entered into the network because the low volumes of local streets are not modeled well. In addition, it has been found that local streets are usually unaffected by area growth, and local traffic will be determined and limited by the site-specific development. Development is modeled by creating centroids that contain residential, business, and institutional data indicating location and levels of development.

QRS II's primary centroid inputs for traffic generation are dwelling units and employees. Appendix B contains information about housing, lots, schools, employment, and businesses, which were used for input into a baseline model. The baseline employment information was further adjusted using some of the data in Table I-5, above.

In addition, other land uses, such as industrial and institutional uses, require special Production/Attraction tags to describe unique trip generators. This model was built using the detailed schema. With this schema trip ends for special generators were modeled, such as post offices, schools, docks, and other facilities that require Production/Attraction tags. The ITE Trip Generation Manual as the basis of trips at special generators.

External stations are nodes that define the boundary conditions, and require information about travel patterns at the ends of the model. Types of trips were estimated (e.g. home to work, home to other, tourism) to and from the external stations.

In order to have confidence in the model, the base model was calibrated to 1999 (AADT) volumes published by DOT/PF for the Homer Area Roads shown in Table I-8. Calibration was a highly iterative process, where centroids, network attributes (e.g. delay at intersections) and external station trip types are adjusted until the model link volumes agree reasonably well with the published volumes. At the end of the calibration phase the 1999 calibrated base model was within 5% to 20% of the 1999 AADT, for most of the roadways. There were roads, most notably West Hill Road, Main Street and Bartlett Street, which did not calibrate very well with the 1999 AADT. However, Main and Bartlett are parallel streets, and together the model shows that they carry about the combined volume of the 1999 AADT. As such, the model as calibrated to the 1999 volumes was accepted (see Table I-9 in Section 6.3 and compare volumes in "1999 DOT/PF AADT" column to "1999 Base Model ADT" column).

Once the model was calibrated, it became the basis of future development and system activities. Residential dwelling units were assumed to grow at the same forecasted rate as the population. As discussed, under Subsection 2.4 **Population Forecasts**, the future annual population growth rate was estimated to be about 2%, and therefore the dwelling units will increase at about 2% per year. Using Equation I-1, it was estimated that the total number of dwelling units in the study area will increase about 49 to 50 percent over the next 20 years. Economic growth was assumed grow at 2.4% annually as developed under Subsection 2.5 **Local Economy**. Total economic growth, primarily stated as employees, is expected to increase about 60 percent in the next 20 years.

To model the locations of the future dwelling unit growth, future generators were placed in the model consistent with current development plans and patterns. An algorithm was developed to equally assign new residences to subdivisions. It was assumed that existing unsubdivided areas would be developed to some extent in the future, with lot sizes similar to surrounding subdivisions, and that growth was included in the model. Once a subdivision was full (dwelling units equal lots), no further units were added. Future commercial development was treated in a similar manner.

The modeled 2021 transportation network included those improvements listed in Table I-7. The one improvement that seemingly would have the most effect on this system is paving Kachemak Drive. Presently, the gravel surface limits speeds. The model does not have surfacing parameters, but the travel speed was raised by 10 mph as a means of estimating the travel benefit. Once paved, the road will provide an attractive alternative to traveling through the CBD and using East End Road for the entire trip. It was found that the volumes did increase on Kachemak Drive by 10 to 15% once the road is paved. However, other street volumes, especially within the CBD, were not reduced by an appreciable amount by the paving of

Kachemak Drive. However, the volumes on Sterling Highway were increased. (It should be noted that Kachemak Drive was paved in 2002, after the model was run, but before the adoption of this plan.) In addition to Kachemak Drive pavement, the new dock facility on the spit was included in separate model runs to ascertain the impact of the dock on roadway operations.

Homer has distinct seasonal variations in traffic flow. DOT/PF maintains a permanent traffic recorder on the Homer Spit Road south of Kachemak Drive. Average summer daily traffic is about 7,800 vehicles, which is about 95% higher than the AADT. It was decided to develop models based on the summer months as well because of these huge summer increases over the average model. Most traffic engineering analysis and decision processes use the peak hour of traffic flow as the design hour, therefore summer peak hour models were developed to review measures of effectiveness.

6.2 Recent Studies

The Boutet Company (TBC) prepared a Traffic Study (Draft) for Homer (January 2, 1999). Pertinent to this plan, TBC's study recommended a street classification system, and prepared capacity analysis of key intersections during summer peak hours. Their capacity analysis showed that all intersections, except the Sterling Highway and Lake Street operated at level of service "C" or better. Sterling and Lake Street operated at level of service "D".

The State of Alaska Central Region Traffic and Safety Section performed a traffic signal warrant analysis for the Sterling Highway (By-Pass) and Lake Street intersection. They found that signalization is warranted, even in the lower volume winter season.

TBC also prepared the Kachemak Bay Multi-Purpose Dock Traffic Impact Analysis, March 7, 1999. Trip generation data from this report for this model. They found that signalization, or other means of traffic control is warranted, even in the lower volume winter season.

6.3 Results

Several model runs were prepared for this transportation plan to describe the present and future operations of the existing system (with near-term known improvements). The following models are included under Appendix C.

- 1999 Base Model- This model was the basis of further work and was calibrated to the published 1999 AADT in the Central Region Traffic Volume Report.
- 2021 Base Model- This run includes future population and economic data, and known future street improvements. It shows AADT in 2021, without the Dock
- 2021 Base Model with Dock (both daily traffic and peak hour traffic)- These models superimposed the Dock volumes on the 2021 Base Model. This represents the future system with known improvements.
- 1999 Summer Model (both daily traffic and peak hour traffic)- These models show the 1999 summer volumes that are estimated to occur now.
- 2021 Summer Model (both daily traffic and peak hour traffic)- These models show the summer volumes in 2021, without the dock.
- 2021 Summer Model with Dock (both daily traffic and peak hour traffic)- These models include dock traffic, and will show the level of future dock impacts on the system when compared to the summer models without the dock. These models represent the design condition in the future.

Each model diagram has volumes shown on links. These volumes are directional, and the total segment volume is the sum of the two volumes. Table I-9 presents 1999 actual DOT/PF AADTs for key street segments, and the model outputs for those segments under various time and development scenarios. Table I-10 presents design hour volumes (DHV) for those segments.

Table I-9: Selected Average Daily Traffic (ADT)

State System Highway / Street	Segment Location	1999 DOT/PF AADT	1999 Base Model ADT	2021 Base Model w/Dock	2021 Summer Model w/Dock
Sterling Highway	Lake Street	8,300	7,277	11,919	14,441
Homer Spit Rd. (Sterling Hwy)	Between end of the Spit and Kachemak Bay Drive	4,009	4,161	7,468	13,719
Pioneer Avenue	Lake St. and East End Rd.	7,300	6,820	10,046	13,428
Lake Street	Sterling Hwy.	5,550	4,450	6,871	8,043
State System Highway / Street	Segment Location	1999 DOT/PF AADT	1999 Base Model ADT	2021 Base Model w/Dock	2021 Summer Model w/Dock
Kachemak Bay Drive	Sterling Hwy.	1,720	1,936	3,666	4,665
East End Road	Lake St/Pioneer Ave.	7,700	7,704	9,856	10,505
East Hill Road	East End Rd.	1,800	2,134	3,403	3,701
Bartlett Street	Pioneer Ave.	1,270	2,234	3,205	3,683
Bunnell Avenue	Olson Lane	1,930	1,218	1,902	2,073
Olson Lane	Sterling Hwy.	470	717	1,097	1,221

Main Street	Sterling Hwy. to Pioneer Ave.	2,770	1,480	2,883	3,919
Mission Road	East Hill Rd.	125	173	300	323
West Hill Road	Sterling Hwy	1,400	2,378	3,446	3,696
Skyline Drive	East Hill Rd.	460	385	625	671
Diamond Ridge Road	Sterling Hwy.	250	826	1,220	1,422

Table I-10: Selected Design Hour Volumes (DHV)

State System Highway / Street	Segment Location	1999 Base Model DHV	2021 Base Model w/Dock	2021 Summer Model w/Dock
Sterling Highway	Lake Street	635	990	1,308
Homer Spit Rd. (Sterling Hwy)	Between end of the Spit and Kachemak Bay Drive	356	641	1,158
Pioneer Avenue	Lake St. and East End Rd.	590	902	1,147
Lake Street	Sterling Hwy.	391	633	690
Kachemak Bay Drive	Sterling Hwy.	167	332	402
East End Road	Lake St./Pioneer Ave.	573	841	857
East Hill Road	East End Rd.	196	299	326
Bartlett Street	Pioneer Ave.	195	273	300
Bunnell Avenue	Olson Lane	105	155	178
Olson Lane	Sterling Hwy.	61	85	103
Main Street	Sterling Hwy.	137	250	503
Mission Road	East Hill Rd.	15	26	28
West Hill Road	Sterling Hwy	210	305	328
Skyline Drive	East Hill Rd.	33	54	62
Diamond Ridge Road	Sterling Hwy.	71	107	121

In evaluating the performance of the existing system, QRS II reports for delay were used to estimate intersection levels of service. Level of Service (LOS) is a quantitative description of roadway facility operations. In this project, LOS analysis is considered for unsignalized intersections. Levels of Service and capacity analysis methods are developed under the Highway Capacity Manual, Transportation Research Board Special Report 209 (HCM). Unsignalized intersection LOS is defined by control delay. Control delay includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. However, the methodology only presents LOS for the minor movements of the intersection, which include the minor street approaches under sign control, or major street movements that must yield to oncoming traffic such as left-turning traffic. There is no overall unsignalized intersection LOS. Unsignalized LOS is defined in Chapter 10 of the HCM as follows:

- LOS A: ≤ 10 seconds per vehicle
- LOS B: > 10 and ≤ 15 seconds per vehicle
- LOS C: > 15 and ≤ 25 seconds per vehicle

LOS D: >25 and ≤35 seconds per vehicle

LOS E: >35 and ≤50 seconds per vehicle

LOS F: >50 seconds per vehicle

QRS II calculates approach delay for intersections. Table I-11 presents the approach delay for the summer evening peak hours in 1999 and 2021 at the model's intersections. We have assigned LOS to these approaches based on the delay. This table presents delay with and without the multi-use dock. This enables the reader to determine the system impacts of the new facility. Those approaches that have LOS "D" or worse are shown in bold.

Table I-11: Delay and LOS for Model Intersections

Intersection	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
Diamond Ridge Road and Sterling Hwy	Southbound	1.6	A	0.6	A	0.6	A
	Westbound	13.5	B	18.3	C	18.5	C
	Northbound	0	A	0	A	0	A
Skyline Drive and East Hill Road	Westbound	5.2	A	8.6	A	5.6	A
	Eastbound	0	A	0	A	0	A
	Northbound	11.8	B	11.5	B	11.5	B
East End Road and Kachemak Drive	Westbound	9.5	A	4.8	A	4.6	A
	Eastbound	0	A	0	A	0	A
	Northbound	14.2	B	19.7	C	20.0	C
Fairview Street and Bartlett Street	Eastbound	11.0	B	11.5	B	10.9	B
	Southbound	4.5	A	4.3	A	2.7	A
	Eastbound	11.2	B	11.5	B	11.3	B
Fairview Street and Main Street	Northbound	2.2	A	3.1	A	3.1	A
	Eastbound	10.5	B	9.9	A	10.7	B
	Southbound	0	A	0	A	0	A
	Westbound	11.4	B	11.1	B	10.9	B
Svedlund Street and Fairview Street	Northbound	4.2	A	1.3	A	1.5	A
	Southbound	0	A	0	A	0	A
	Eastbound	9.4	A	10.3	B	9.6	A
Pioneer Avenue and Heath Street	Northbound	3.1	A	6.1	A	5.7	A
	Northbound	12.4	B	56.6	F	94.8	F
	Westbound	1.2	A	2.7	A	3.1	A
Kachemak Way and Pioneer Avenue	Eastbound	0	A	0	A	0	A
	Southbound	11.1	B	15.9	C	16.8	C
	Westbound	0.1	A	0	A	0	A
	Eastbound	0.5	A	1.3	A	1.0	A
Svedlund Street and Pioneer	Northbound	11.1	B	13.7	B	14.3	B
	Southbound	11.7	B	19.4	C	23.3	C

Intersection Avenue	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
	Westbound	0	A	0	A	0	A
	Eastbound	0.6	A	0.2	A	0.2	A
	Southbound	12.1	B	18.0	C	18.0	C
East Hill Road and East End Road	Westbound	0	A	0	A	0	A
	Eastbound	5.7	A	3.7	A	3.7	A
	Southbound	9.7	A	14.4	B	14.4	B
Fairview Drive and East End Road	Westbound	0	A	0	A	0	A
	Eastbound	8.4	A	1.3	A	1.4	A
	Eastbound	0.4	A	0.4	A	0.4	A
Sterling Hwy and West Hill Road	Southbound	18.0	C	35.1	E	35.8	E
	Westbound	0	A	0	A	0	A
	Southbound	12.8	B	55.2	F	18.1	C
Main Street and Pioneer Avenue	Westbound	4.2	A	4.5	A	3.6	A
	Eastbound	1.2	A	0.8	A	1.0	A
	Northbound	12.7	B	25.9	D	17.8	C
Bartlett Street and Pioneer Avenue	Southbound	10.8	B	18.0	C	18.6	C
	Westbound	0	A	0	A	0	A
	Eastbound	3.9	A	4.2	A	4.7	A
Heath Street and Sterling Hwy	Southbound	13.9	B	99.0	F	983.9	F
	Westbound	0	A	0	A	0	A
	Eastbound	3.9	A	4.2	A	4.7	A
Pioneer Avenue and Sterling Hwy	Eastbound	5.6	A	4.2	A	4.0	A
	Southbound	14.3	B	63.1	F	48.7	E
	Westbound	0	A	0	A	0	A
Ohlson Lane and Sterling Hwy.	Westbound	0	A	0	A	0	A
	Northbound	13.9	B	28.3	D	25.1	D
	Eastbound	0	A	0	A	0	A
Main Street and Sterling Hwy	Southbound	13.9	B	53.3	F	37.4	E
	Westbound	1.4	A	1.4	A	1.5	A
	Eastbound	0	A	0	A	0	A
	Northbound	11.9	B	19.8	C	17.2	C
Lake Street and Smokey Bay Way	Southbound	3.5	A	3.0	A	3.5	A
	Westbound	10.6	B	13.4	B	13.1	B
	Northbound	0	A	0	A	0	A
Trimmerman Court and Lake Street	Westbound	13.7	B	15.2	C	15.2	C
	Southbound	1.3	A	1.7	A	3	A
	Northbound	0	A	0	A	0	A
Sterling Hwy and Lake Street	Eastbound	6.1	A	7.6	A	10.7	B
	Westbound	0	A	0	A	0	A
	Southbound	14.2	B	1462.3	F	522.7	F
Sterling Hwy and Kachemak Drive	Southbound	7.8	A	3.1	A	3.1	A
	Northbound	0	A	0	A	0	A
	Westbound	15.3	C	52.8	F	71.3	F
Diamond Ridge Road / West Hill Road / Westwood Drive / Skyline Drive	Eastbound	11.7	B	11.4	B	11.4	B
	Southbound	11.7	B	11.5	B	11.5	B
	Westbound	11.6	B	11.3	B	11.3	B

Intersection	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
East End Road / Pioneer Avenue / Lake Street	Northbound	11.7	B	11.4	B	11.4	B
	Westbound	10.4	B	15.7	C	17.3	C
	Eastbound	10.9	B	17.9	C	15.9	C
	Northbound	11.0	B	15.9	C	16.9	C
Ben Walters Street / Smokey Bay Way / Timmerman Court	Southbound	10.3	B	10.2	B	10.1	B
	Eastbound	10.6	B	11.1	B	11.1	B
	Northbound	10.2	B	10.9	B	10.9	B

Table I-11 shows that the following intersections will have poor LOS and undesirable delays by the planning year.

- Pioneer Avenue and Heath Street
- Sterling Hwy and West Hill Road
- Main Street and Pioneer Avenue
- Heath Street and Sterling Hwy
- Pioneer Avenue and Sterling Hwy
- Main Street and Sterling Hwy
- Sterling Hwy and Lake Street
- Sterling Hwy and Kachemak Drive

In addition, TBC's analysis shows that the Sterling Highway/Lake Street intersection already operates at an undesirable LOS, and the DOT/PF shows that a signal is warranted at this intersection. It should be noted that the 1999 summer model shows a LOS "B" for the minor movement. The RSC recommends that traffic control alternatives to signalization be considered.

The models also show that the summer peak hour volumes for the Spit Road and East End Road will increase to levels where a two-lane facility may be over capacity, especially considering the volumes of left-turning vehicles that will use the road. Through past work, the consultant has found that two lane roadways with moderate speeds (40 to 45 mph) can accommodate through volumes of around 10,000 vehicles per day, or 1,000 to 1,300 per hour, as long as there is not a substantial number of left-turn vehicles. Lower speed roads (25 to 30 mph) can accommodate higher flows, because desirable headways (time between vehicles) can be maintained at a reduced distance.

I. PURPOSE and INTENT

1. The H.A.R.T. is a combined local funding source of dedicated sales tax and assessments to upgrade city streets, new city streets and new city non-motorized trails.(Ordinance 06-42(S); Resolution 88-47 #1)
2. The intent of the program is to reconstruct local substandard city roads and/or upgrade existing city roads, construct new city streets and non-motorized trails, thereby reducing maintenance cost, improving access, increasing property values and improving the quality of life. (Ordinance 06-42(S); Resolution 88-47 #2)
3. Reconstruction and new construction shall be to City Standards. (Ordinance 06-42(S) Resolution 88-47 #19)
4. The City will not accept a street for full time maintenance until it meets city standards and is shown on the official maintenance map.¹ (Ordinance 85-14 07/01/85; Resolution 88-47 #8)
5. When practical, the intent of the program is to preclude the destruction of existing property improvements in built up areas. (Resolution 88-77(A), be it further Resolved clause.)
6. State maintained roads are not part of this program. (Resolution 88-47 #7)
7. The criteria for the H.A.R.T. shall be reviewed annually by the Transportation Advisory Committee, with recommendations reported to the Homer City Council. (Resolution 88-47 #22)
8. Annexed roads are included as newly eligible roads, as listed on the Official Road Maintenance Map. (Resolution 03-116, 08/25/03)
9. New roads shall be listed on the Official Road Maintenance Map. (Resolution 07-82)
10. New trails shall be listed on a map in the City Clerk's Office. (Resolution 07-82)

II. DEFINITIONS

- A. Sidewalk- the term "sidewalk" means a pedestrian facility associated with a road and generally within a street right of way. (Resolution 07-82)
- B. Trail – a pedestrian facility detached from a road, or not within a street right of way. (Resolution 07-82)

¹1.Clerk's Note: Done by Ordinance

- a. Connectivity to existing road(s), for example completes a traffic pattern.
- b. Arterials or thoroughfares;
- c. Existing utilities;
- d. Contributing funds such as property owner assessments, loans, grants, etc;
- e. Level of need. (Resolution 07-82)

B. Trails

New local non-motorized trails shall be prioritized according to the following:

- a. Project is listed in the HNMTTP or furthers a stated goal of that plan;
- b. Solves a safety concern;
- c. Creates connectivity to existing trail(s), completes pattern or provides access to a point of interest;
- d. Protects an established trail;
- e. Creates or improves a trailhead;
- f. Has significant scenic or aesthetic value;
- g. Existence or potential for contributing funds;
- h. Property owner participation. (Resolution 07-82)

IV. FINANCING and ASSESSMENTS

1. The program will utilize an additional dedicated City sales tax not to exceed three quarters of one percent (¾%) supplemental with assessments against adjacent benefited properties. (Ordinance 06-42, Resolution 87-61(S), Resolution 88-47 #3).

2. A three quarters of one percent (¾) dedicated sales tax and will be collected for up to twenty years expiring December 31, 2007 and reauthorizing up to an additional twenty years expiring December 31, 2027 to participate in funding the accelerated roads and trails program (Ordinance 06-42, Resolution 87-61(S), Resolution 88-47 #4). Reauthorized twenty additional years at the October 3, 2006 election (Resolution 06-145(S)) to expire December 31, 2027. Ten percent of the annual revenue shall be used for trail projects.

3. The road improvements will be financed on a combined pay as you go basis as well as sale of revenue bonds in a fifty-fifty ratio. There may be future bond sales as revenues increase. (Resolution 87-47 #6)

4. The City will attempt to obtain long term financing for up to ten years for the private share of funding. (Resolution 88-74 #12, bond change Ordinance 89-17, regarding ten years financing.)

5. Interest, if any, generated from the program will remain with the program funds. (Resolution 88-47 #18)

6. Abutting property owners will share the cost of upgrading a street to residential standards by paying \$30 per front foot for gravel and \$17 per front foot for paving.² (Resolution 87-61(S), Resolution 88-47, Resolution 94-50, Resolution 95-97)

7. The City will pay all costs for any additional improvements required when deemed necessary by the City. Other improvements requested by the benefited property owners will be paid by those same property owners. (Resolution 88-47 #11)

8. The \$30/\$17 split in front foot assessment stands unless there is 100% agreement on a revised formula for a specific project or the amount is adjusted by the City Council. (Resolution 87-61(S), Resolution 88-47; #21; Resolution 95-97)

9. Road Reconstruction assessment payment date, penalty and interest shall be set as soon as the reconstruction project has been accepted by the Public Works Department regardless if the LID/Assessment district wherein reconstruction has been completed is also scheduled for paving as part of the same LID/Assessment District. Paving assessment payment date, penalty and interest will be set as soon as the paving project has been accepted by the Public Works Department. HCC 17.04.070--110. (Resolution 96-73)

10. New Local Roads may be constructed by 100% program funds when the construction thereof benefits the entire City or when the City owns the property wherein the road is to be constructed. The Road to be constructed must meet the qualifying criteria and be recommended by the Transportation Advisory Committee to the City Council. This expenditure must be approved via Ordinance with justification noted within the body of the Ordinance. Whenever possible, New Local Roads will be constructed using the LID process HCC 17.04 and the assessment methodology as noted in item 6. and 8. (Resolution 07-82)

11. HART funds may be used to leverage outside funds for New Local Roads and Trails. (Resolution 07-82)

12. New Local Trails may be constructed using 100% program funds and follow the procedures listed in item 10. (Resolution 07-82)

13. Sidewalks shall be paid for out of road funds, and trails shall be paid for out of the 10% allocated to trails. (Resolution 07-82)

² Danview/Svedlund and Sabrina/Mark White are grandfathered in at the \$20/\$11 split per Council action. (Resolution 94-52)

developer shall construct the streets on the alignment adopted in the Master Roads and Streets Plan, and conforming to the respective classification. The developer shall be required to construct the street to a twenty-eight-foot width in accordance with the minimum requirements of a local residential street; provided, however, that the City may, upon direction of the City Council, elect to require construction to the full standards and pay to the developer the cost difference between the required street and the proposed street. (Resolution 07-82)

VII. TRAIL PRIORITIZING CRITERIA AND PLANNING GUIDELINES

A. Trail Prioritizing. The TAC and Parks and Recreation Advisory Commission will review the trail priority list during the annual review of the HART. The list will be presented in a memorandum from staff, and will contain a mix of large and small projects. Generally it will include up to five trail projects that staff has reviewed and found ready for preliminary work. Trails on this list are planned for construction in the near term (one to three year timeframe). Staff will actively work to prepare those projects for construction. (Resolution 07-82)

B. Trail Planning Guidelines

Trail design shall take into account at minimum the following:

1. Use context sensitive design when locating and planning trails to take advantage of scenic resources.
2. Respect the character of trails based on function, setting, and expectation of accessibility.
3. Evaluate the soils, drainage, wetlands, Tsunami zone, flood plain, stream setbacks, historical resources, visual resources, topography, existing and potential land use, zoning and land ownership.
4. Where estimated costs, operating costs and outside funding availability are considerations and important criteria, care should be used to ensure that important trails are not eliminated solely using cost as a determinant.
5. Multi-use trails are encouraged. Design of the trail should include consideration of compatible uses such as pedestrians and bicycles.
6. All trails should be designed to recognize the requirements of ADA standards and guidelines. (Resolution 07-82)



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MEMORANDUM

TO: Mayor Hornaday and Homer City Council
THRU: Rick Abboud, City Planner
FROM: Julie Engebretsen, Planning Technician
DATE: November 2, 2011
SUBJ: Homer Advisory Planning Commission comments on the Kachemak Drive Pathway

At the September 12, Homer City Council meeting, the Council considered Resolution 11-90, brought forward by the Parks and Recreation Advisory Commission. The resolution supported the concept and construction of a pathway or other non-motorized improvement along Kachemak Drive. The Council referred the matter to the Planning Commission.

At the September 21st HAPC meeting, the Commission made and approved the following motions:

- THE ADVISORY PLANNING COMMISSION SUPPORTS THE CONCEPT OF A NON MOTORIZED ACCESS ALONG KACHEMAK DRIVE.
- A LARGE PART OF THIS PROJECT IS A PRIVATE PROPERTY RIGHTS ISSUE THAT SHOULD BE ADDRESSED CAREFULLY FROM THE ONSET. THE UTILITY EASEMENTS ARE PRIVATE PROPERTY.
- THE ADVISORY PLANNING COMMISSION RECOMMENDS THE CITY ADD THE KACHEMAK DRIVE PATH IMPROVEMENTS TO THE STIP NEEDS LIST AS AN AVENUE FOR STATE FUNDING.
- THE COMMISSION APPRECIATES THE EFFORTS OF THE PARKS AND RECREATION ADVISORY COMMISSION AND ENCOURAGES THEM TO CONTINUE WITH THIS GRASS ROOTS EFFORT.

Kachemak Drive Path Committee

Thursday, February 23, 2012, 5:30 p.m.

Bumppo Bremicker, Chairman, Beth Cumming, Lynn Burt, Lindianne Sarno, recording, Dave Brann, David Clemens, Kevin Walker, visitor.

Motion to Approve Agenda: Beth moves to put New Business B. before New Business A. No objections, we approve this change. Beth feels it's important to get our resolution done and ready to go to Parks and Rec.

Dave Brann moves to approve, Beth Cumming seconds, no objections, approved.

Motion to approve minutes: Beth moves. Dave seconds, no objections, minutes approved.

Public comments: Kevin Walker, when shall we discuss my packet of info? Beth thinks this packet should accompany the resolution to City Council, and it will be discussed then, including Kevin in the discussion.

Pending Business: Continuing Discussion and Planning for Path Design, page 7. Kevin Walker at the microphone. Regarding providing public with access to and from the airport for pedestrians who get off planes. Kevin's letter to Jennifer Bailey cleared up; we are not proposing to trespass on airport property, but rather to provide safe non-motorized transportation corridor for people coming to and from the airport. Dave Brann reports that Jocelyn Biloon and Scott Thomas, say DOT is working on this. They recognize there is a committee, and Dave is their contact. Walt Wrede, City Manager will get in touch with Ms. Biloon regarding a traffic count. Jennifer Bailey is our contact at DOT Public Transportation Facilities, Aviation Leasing. She is in Anchorage. Kevin Jones is the airport manager.

Bumppo points out that if we are in the DOT right of way we have to follow their requirements, regardless of whether DOT is paying for it. (1) Survey (2) Engineered drawings (3) ADA design specs (4) erosion control (5) contract with DOT to provide maintenance (6) liability and insurance coverage. City of Homer sponsorship is crucial, especially for ongoing maintenance.

Dave Brann points out most of the easements are private property easements. They are not a big stumbling block, especially with approval of City Council. Utility easements are outside DOT right of way. Until we survey each section on the ground we won't know for sure but huge section of road is outside DOT right of way.

Dave Clemens: suggests we send DOT a package about the DOT right of way on the sections that are in DOT right of way. Kevin Walker says Jennifer Bailey says this is already in DOT planning. Next step is contacting the City Manager, the City Manager contacting state planning, and getting back to us.

Bumppo: what we need is for city to officially to buy into this and deal with DOT.

New Business: page 67, reformulating the Resolution on the Kachemak Drive pathway. City Council sent it back to Zoning and Planning, who sent it back to us. It was too muddy last time.

Add to the resolution; include a funding plan mentioning that there will be \$305,000 by the end of 2012.

Beth suggests we send to the city this resolution, the Kachemak Drive Path specifics packet created by Kevin Walker, and a funding plan.

FUNDING PLAN: the city can borrow money at good rates, against the HART money, need not be bonded, borrow against the revenue source for 10 years. Julie Engebretsen, Planning Department, says this is a common practice. Funding plan would include private donations and HART funding. Page 95 of our packet gives an outline of the Planning and Approval process.

WHAT WE MUST ACCOMPLISH: Bumppo: We want City Council to direct city administration to start focusing on this pathway. Dave Brann commented, to direct city staff to do what they can to keep this moving along. Give the city a plan for maintenance. Bumppo interjected that we still need to contact landowners, how will they feel about this. Dave Brann: this is on top line of things to talk about with the City Manager.

Beth: Two City Council members commented, "We want to know what you have in mind."

DOCUMENTS WE NEED: (1) resolution (2) packet from Kevin is ready. Complete these two items tonight. Then Renee can advise us on form of (3) funding plan (not ready) and (4) a letter to private landowners will come from the City. (5) Maintenance plan.

We revise the resolution. See resolution on next page.

Beth moves that we approve Resolution 11-090 as revised and send it on to Parks and Recreation and the City Council; Dave seconded. Unanimous approval. See resolution on next page.

Bumppo moves that we assign a council member to each committee member, and well in advance of the City Council meeting bring to them the packet including: resolution, Kevin Walker's packet, and funding plan. Dave Brann says we'll not have a projected cost until the survey. Dave Clemens suggests we mention that we've scaled back from the East End Bike Path to less than \$1.5 million per mile.

Beth proposes changing our Committee's mission statement: "to build a safe non-motorized separate path along Kachemak Drive, connecting East End Road to Spit Road via the airport. Kevin's packet is called our preliminary engineering packet.

Beth proposes a fund-raising event to raise \$5,000 to \$10,000 petty cash. She has engaged a professional event producer, Sally Oberstein, to produce the event, and is proceeding with these plans as a private citizen. Funds would be used for chain saws, Surveying, signage, and other uses.

Dave Clemens mentions that the Homer Cycling Club approves increased signage to increase safety on Kachemak Drive for those commuting cyclists who will continue to use the road. If Kachemak Drive is improved, the cycling club would support widening the shoulder for the safety of commuting cyclists.

Beth wonders if the Mud Bay Trail is included. We see the Mud Bay Trail is included in Kevin Walker's pre-engineering packet.

We discuss the plastic walkway and decide a request has gone to the city for the walkway.

Report on December 10 Field Trip: Dave Brann said it was valuable, going from the airport lot, down, looked at possibility of cut-off trail. It will be challenging. Walked along the beach to the road. Site visitation was valuable. Clarified and provided yellow line and blue line on the pre-engineering packet. Lynn agrees with Dave. Beth says the foundation is there on Aviation Leasing land for a full quarter mile, bridge would be used to cross the gully, then pleasant going among the trees. Yes, it drops steeply and bicyclists could walk there. Bicycle trail would be fairly easy along the Mud Bay Trail.

Kevin Walker: revised mission statement as noted. Will include resolution. Lindianne will e-mail resolution to Kevin as well as to Renee. Will leave off last three pages.

homerkev@gmail.com, clerk@ci.homer.ak.us,

Re funding and maintenance: Dave, Kevin, Renee, will put those together, and Kevin will send them out to the Committee via e-mail. HART funds: \$234,719 at end of 2011, \$305,000 by end of 2012.

Dates: Next Parks and Rec is 15th of March, City Council will be 26th of March. Possible for this resolution and preliminary engineering packet to go to these meetings. Renee, please put the Kachemak Drive Path on the agendas for both those meetings. Bumppo will call or e-mail each Committee member to assign a City Council member.

Our next meeting: March 22, fourth Thursday in March.

We adjourn. 9:05 p.m.

CITY OF HOMER
THE REVISED RESOLUTION:

City Clerk/Parks and
Recreation Advisory Commission

RESOLUTION 11-090

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA,
SUPPORTING THE CONSTRUCTION OF A NON-MOTORIZED
PATHWAY TO INCREASE SAFETY FOR MOTORIZED AND NON-
MOTORIZED USERS ALONG KACHEMAK DRIVE LOCATED
WITHIN HOMER CITY LIMITS, FROM THE BASE OF THE HOMER
SPIT TO EAST END ROAD

WHEREAS, The Parks and Recreation Advisory Commission established the Kachemak Drive Path Committee to specifically address possible solutions to the hazards presented to non-motorized and motorized users of Kachemak Drive; and

WHEREAS, The Kachemak Drive Path Committee received substantial public input on safety concerns; and

WHEREAS, The Homer City Council has shown support for this non-motorized pathway by inclusion of the Kachemak Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the Homer Non-Motorized Transportation and Trail Plan; the Homer Area Transportation Plan; the Climate Action Plan; and the Homer Accelerated Roads and Trails (HART) Policy Manual; and

WHEREAS, Increasing non-motorized transportation improves public health and safety; encourages tourism; cleans the environment; reduces transportation costs; and enhances livable communities; and

WHEREAS, The City of Homer has available HART trail funds that can, including other sources, form a basis for funding this project; and

WHEREAS, Utility easements can be employed with the permission of property owners.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, hereby supports the construction of a non-motorized pathway along Kachemak Drive within the City of Homer, and that said improvement will enhance safety and benefit the public; and

BE IT FURTHER RESOLVED that the City Council of Homer, Alaska further supports actions increasing safety for motorized and non-motorized users of Kachemak Drive in the following ways:

1. Increasing the usage of signage warning drivers of bicycles and pedestrians on roadway.
2. Build a separated path paralleling Kachemak Drive using utility easements and public property, and increase the use of signage.

VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE PRELIMINARY ENGINEERING PLAN FOR KACHEMAK DRIVE PATH TO CITY COUNCIL AS AMENDED.

There was no discussion.

VOTE. YES. LILLIBRIDGE, BRANN, BREMICKER, LOWNEY.

Motion carried.

Chair Bremicker read the Funding plan into the record and opened discussion on the item.

BREMICKER/LILLIBRIDGE - MOVED TO INSERT TRUCKING IN THE THIRD PARAGRAPH BEFORE GEOBLOCK, INSERT THE LAST NOTE BEFORE OTHER SOURCES OF FUNDING AND ADD OTHER POSSIBLE SOURCES OF FUNDING AS DISCOVERED OR AVAILABLE.

There was a brief discussion on including trucking since it was one of the highest costs in a project, deleting the reference to hiring a contractor. It was noted that the trail will not necessarily be built all at one time and may be built in phases and designating crossing(s) for the path.

VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

BRANN/LILLIBRIDGE - MOVED TO FORWARD THE AMENDED FUNDING PLAN FOR KACHEMAK DRIVE PATH TO COUNCIL FOR APPROVAL.

There was no further discussion.

VOTE. YES. BREMICKER, LOWNEY, LILLIBRIDGE, BRANN

Motion carried.

Chair Bremicker introduced and read the Maintenance plan into the record and opened discussion by commented on dragging the path versus plowing the path during winter.

A discussion ensued on ownership of the path would depend on the location which would be easements on private property so the trail would be the city but the land is the property owners. The city would have the easement and maintain the path.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE MAINTENANCE PLAN FOR KACHEMAK DRIVE PATH TO CITY COUNCIL.

There was no further discussion.

VOTE. YES. BRANN, BREMICKER, LOWNEY, LILLIBRIDGE.

Motion carried.

The Commissioners agreed to schedule a Special Meeting April 10, 2012 at 5:30 P.M. in the Conference Room Upstairs at City Hall. The content of the agenda to focus on Karen Hornaday Park – What and Where Do We Go from Here?

The commission requested the presence of the following staff and vowed to take up only 30 minutes of their time acknowledging the constraints of the departmental budget restrictions: Public Works Director, Recreation Coordinator, Parks Maintenance Coordinator, Julie Engebretsen in Planning and if the City Manager would like to attend his input would be greatly appreciated. The commissioners had questions on the budgeted work items when they would be scheduled, application and funding for the trails within the park especially the Woodard Creek trail, engineering and items approved last year.

C. Discussion and Recommendations on the Kachemak Drive Proposed Non-motorized Pathway and Revised Resolution

Chair Bremicker introduced the item by reading of title then recommended that the commission review and address each item - Resolution, Preliminary Engineering Plan, Funding Plan, Maintenance Plan separately.

Chair Bremicker then read the Revised Resolution 11-090 into the record.

BREMICKER/BRANN – MOVED TO CHANGE LAST TWO LINES IN THE LAST PARAGRAPH TO READ "...THE FOLLOWING WAYS:

1. INCREASING THE USAGE OF SIGNAGE WARNING DRIVERS OF BICYCLES AND PEDESTRIANS ON ROADWAY.
2. BUILD A SEPARATED PATH PARALLELING KACHEMAK DRIVE USING UTILITY EASEMENTS AND PUBLIC PROPERTY.

There was a brief discussion on a proposed revision by Ms. Cumming a member of the committee. Staff was unable to locate the submitted document at the time for review by the commission and was not present. Commissioner Brann commented that he believed she was addressing a change to "contingent on available funding but the remaining committee members were satisfied with the changes made."

VOTE. YES. LOWNEY, LILLIBRIDGE, BRANN, BREMICKER

Motion carried.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE AMENDED RESOLUTION 11-090 TO COUNCIL FOR APPROVAL.

There was a brief discussion on the clarity of the resolution was adequate.

VOTE. YES. BRANN, BREMICKER, LOWNEY, LILLIBRIDGE

Motion carried.

A brief discussion on the Proposed Preliminary Engineering Plan ensued. The Commissioners requested colored maps for City Council, it was noted that these are generalized, not detailed.

BREMICKER/BRANN – RECOMMEND ADDING ON THE GENERAL NOTES PAGE CONSTRUCTION OF THE PROPOSED PATHWAY BE CONDUCTED IN SUCH A MANNER THAT IT CAN BE UPGRADED TO A HIGHER LEVEL OF TRAIL INCLUDING PAVING AND DELETE THE LAST SENTENCE.

A brief discussion on the time frame for submittal to council at the same time as the resolution.

STAFF AND COUNCIL REPORTS/COMMITTEE REPORTS/BOROUGH REPORTS

A. Community Recreation Report – Mike Illg

Mr. Illg reported that they are working with some groups to sponsor some recreational programs for youth, and requests to use the HERC for private parties.

B. Spit Parks and Recreation Committee Report – Tricia Lillibridge

Commissioner Lillibridge distributed a Draft Flyer for the proposed “Meet the Trucks” event at Mariner Park and the next Spit Parks and Recreation meeting is scheduled for February 28th, 2012 at 3:30 p.m. in the conference room at City Hall. She welcomed the audience to attend as they were looking for additional members for the committee.

C. Kachemak Drive Path Committee Report – Dave Brann

Commissioner Brann reported that there was still work being done on preparing for a path design and information packet that he is working on with Kevin Walker; he is still pursuing signage along Kachemak Drive and permission to cross airport property. He is working with the City Manager on getting letters sent to the Property Owners regarding input on the proposed path. He has had a teleconference with Ms. Biloon on the proposed path.

D. Karen Hornaday Park Committee Report – Robert Archibald

Commissioner Archibald reported that at the recent meeting the discussed the improvements and needs of Little League, Vice Chair, Ralph Broshes was elected; discussed the Camp Host Program that there are negatives and positives; the Committee passed a recommendation which is included in the packet tonight, that there should be no additional improvements until engineering and design services are procured specifically for the road, parking, creek trail and drainage issues directly affecting those areas. The committee did discuss funding and the participation of all the user groups it was noted the funding is out there and managing the parks takes money;

Commissioner Lillibridge requested the Little League submit their stats on letterhead so it can be used to by the commission in support of the needed funding.

Commissioner Lowney recommended a copy of the Park Master Plan be displayed on a bulletin board during the HoPP build week, this will let the public know what needs to be done, what benefits there are, and what more is planned for the park.

PUBLIC HEARINGS

No public hearings were scheduled.

PENDING BUSINESS

A. Recommendation to City Council to Establish a Campground Host Program

Chair Bremicker introduced the item into the record.

The commission entertained a brief discussion on the benefits of having this program and getting it implemented this year at Karen Hornaday Park.

There was a brief discussion on the appearance that it was owned by the adjacent property owner and that no contact was made with the owner. The commission thanked him for bringing this idea forward. Mr. Aderhold offered any assistance he could provide if the commissioners decided to look further into this idea.

There was no further discussion.

B. Miranda Weiss, Homer Playground Project (HoPP)

Miranda Weiss, acknowledged members of the group and their roles they have within the project. Ms. Weiss stated that the group intends for a long term commitment, that as a whole they are supportive of the entire Master Plan for the park, and HoPP is supportive of ongoing maintenance. Ms. Weiss further explained the enormity of the proposed playground project and what the recommendation from the commission to City Council would provide for the proposed project and community alike. She emphasized that Build Week would be a kick-off for implementing the Master Plan and it would be an asset to the commission in the future to be in the forefront of this project today.

There were no further questions or comments.

RECONSIDERATION

There were no items scheduled for reconsideration.

STAFF AND COUNCIL REPORTS/COMMITTEE REPORTS/BOROUGH REPORTS

A. Community Recreation Report – Mike Illg

There were no discussions or questions on the information provided in the packet.

B. Spit Parks and Recreation Committee Report – Tricia Lillibridge

Commissioner Lillibridge reported that the committee has not met due to personal schedules and the holidays. She explained to those present the purpose of the committee and named the current recreational areas and parks that are located on the Homer Spit and pointed out a few issues with some of the more used areas.

There was a brief discussion on relocating the access to Marina Park due to line of sight issues and that Carey Meyer is currently drafting some numbers on relocating the entrance.

Commissioner Lillibridge announced the plan to hold a Meet the Trucks event on May 5, 2012 at Mariner Park which will be discussed in more detail at the next meeting.

C. Kachemak Drive Path Committee Report – Dave Brann

Commissioner Brann reported the progress made so far by the Committee in getting a proposed pedestrian bicycle path along Kachemak Drive. He has received a call from AKDOT personnel and will be talking with the City Manager on that discussion that was held.

D. Karen Hornaday Park Committee Report – Robert Archibald

Commissioner Archibald reported that the committee has met and discussed several issues and recommended removal of the red shed and several other issues that are before the Commission this

There was no further discussion.

F. Kachemak Drive Path Committee Report

Chair Bremicker reported on the progress of the committee during the last month. He stated that they had a visitor at the most recent meeting, Lynn Whitmore who represents the Moose Habitat and were concerned what the committee was proposing and what the design would be through the property. Chair Bremicker stated that the information and advice that the committee received from Mr. Whitmore who is experienced with the different state agencies was very helpful and especially the best types of paths to put through that property that will not deter or hamper a moose passage through the area.

It was noted that the committee made progress on defining the proposed path and where it should be located along Kachemak Drive. They have received very helpful advice and efforts from Kevin Walker also. An option that was discussed was to use an established easement for a road that goes from East End Road in front of Redden Marine to Kachemak Drive. The committee has reached consensus on the proposed location and the next steps will be the design cross sections and funding.

The members of the committee responded to questions from the commission on portions at the easterly end of the proposed path and connection to the beach access.

The committee will have a meeting on November 22, 2011 at 5:30 p.m.

There was no further discussion.

PUBLIC HEARINGS

No public hearings were scheduled.

PENDING BUSINESS

A. Transient Camping Along Trails in City Limits – Discussion and Recommendations

Chair Bremicker opened discussion by asking clarification on the Staff recommendation.

A lengthy discussion covered the following topics:

- offering a place where someone could camp for free for a limited time
- to prevent garbage and debris being left behind from transient campers

It was noted that the information provided by Ms. Krause did contain several simple suggestions such as clearing brush and shrubbery to make the problem areas more open to visual inspection, bring problems to the attention of local police enforcement, etc. It was discussed in depth that the police department needs to respond and react to complaints that are filed by residents. This is an issue that affects all areas of trails and the like. Including advertisement or notice encouraging reporting incidents of unlawful camping in a resolution forwarded to council.

BRANN/ARCHIBALD - MOVED TO DIRECT STAFF TO SEND A MEMORANDUM TO THE HOMER POLICE CHIEF MARK ROBL THROUGH THE CITY MANAGER WALT WREDE TO ENFORCE CITY CODE, 19.08.030 AND 19.08.070 AS APPLICABLE TO THE AREAS NOTED FOR TRANSIENT CAMPERS AND THE ASSOCIATIVE PROBLEMS AND TO INSTALL ADDITIONAL SIGNAGE PROHIBITING CAMPING.

There was a brief discussion.

BREMICKER/BRANN - MOVED TO OFFER FRIENDLY AMENDMENT, AND TO ISSUE PUBLIC SERVICE ANNOUNCEMENTS ENCOURAGING RESIDENTS TO REPORT ILLEGAL CAMPING TO THE POLICE DEPARTMENT AND MAKING SURE OFFICERS FOLLOW-UP ON THE ALLEGED VIOLATIONS.

concern Commissioner Cumming expressed regarding work to be completed in the area of the catch basins and that this work has been completed. The contractor did a great job too. He stated that the preliminary engineering to establish the cost of the road relocation would be completed in November, he cited a "cup runneth over" the past few months but assured the commission that he felt he was capable of performing the valuation and there was no need to expend monies unnecessary for engineering work to be done by an outside firm. Commissioner Cumming commented on terracing the parking lots. Mr. Meyer did acknowledge that there would be more earth-work required in relation to the road but that is not the most expensive component of the work required and he believed that there was enough talent with Public Works to handle that aspect of the job.

Commissioner Lilibridge would like to propose a recommendation to remove the red shed at Karen Hornaday Park. She stated that the committee discussed it during the site visit and the HoPP committee has reviewed the possible uses and voted that it is not needed and it needs to be removed this winter if possible but definitely before build week in May and re-purposed as the city sees fit.

Mr. Meyer agreed that they have reviewed what is being stored and if any of the items can be relocated or permanently disposed of and they are in the process of looking for a group that would be interested in the wood from the building. He did not believe that they needed a recommendation from the commission but if it came down to needing a minimal amount of funding approved then it may be good to have the support of its removal from the commission to include with the request from council.

There was a brief discussion on the possible uses and how it would distract from the beautiful new design being created for Karen Hornaday Park playground. There were further comments on showing the state that they are spending the money but that the City has something to show for the work.

Commissioner Archibald thanked Mr. Meyer for coming to the meeting and clearing the air over a few issues. He further noted for the record that he was supportive in re-purposing the red shed if possible.

Mr. Meyer praised Ms. Otteson for her efforts and work done since assuming the position of Parks Maintenance Coordinator.

There was no further discussion.

C. Kachemak Drive Path Committee – Commissioner Harrauld

1. October 6, 2011 Meeting Minutes
2. Draft property Owner Notices
3. Memorandum and Attachments on Recommendations from Planning Commission
4. HEA Response

Commissioner Brann spoke about addressing the transient camping along the portion of the Mud Bay Trail along Kachemak Drive.

Commissioner Cumming reported that the committee is arriving at the point that a difficult decision requires a vote. She hopes that the other commissioners will comment on her comments. She was concerned about the content of another resolution submitted to City Council. Chair Bremicker noted that the committee had an in-depth discussion on narrowing the lanes and it was determined that due to public input this option was not recommended and will not be included on the revised resolution.

Chair Bremicker stated that there was a consensus in the committee that an off road trail was needed along the road. He stated that they had the Finance Director attend the last meeting and explain the process pertaining to use of HART funds and the amount that may be available at the end of 2012. Chair Bremicker noted that there is two separate funds – one for roads and one for trails.

The discussion by the committee centered on the final points to be addressed in the revised resolution which will be the following:

- What do we need to build?
- Where do we need to build it?
- How much do we spend to build it?

It is hoped that there will be a specific plan with specific numbers and funding to present to this commission and city council.

Commissioner Brann reported that the committee also discussed about drafting letters to be distributed to the property owners that will be affected by the proposed trail to see how many would be for or against the proposed project. Ms. Krause responded to the commissioner's inquiry that the letter once agreed upon by the commission will have to be forwarded to the City Attorney for review and drafting the formal document that can be distributed.

Chair Bremicker requested the sample letters to be reviewed by the committee since not all the members have seen it to comment on and then at the November meeting the commission can put forth their recommendations and then forward to the City Attorney.

Commissioner Cumming inquired when they can start re-working the resolution. Ms. Krause stated that the information included in the packet has not been before Council however in the meantime before officially being remand from City Council. It was commented that it may be as late as January or February before resubmittal to City Council. Further observations were made on the time that has been expended on this committee and the task they have undertaken.

Commissioner Archibald left the meeting due to a prior commitment at 7:30 p.m.

Commissioner Brann responding to Commissioner Lillibridge' question regarding the placement of the path along Kachemak Drive stated that there are still discussion on all options; because of the work being done with the sewer and water easement currently in progress would be ideal but there are several proponents to use that easement including property owners permission; conservation issues; feasibility of placement. The trail will be constructed from one end to the other but it may be along different avenues for different sections. That is one of the reason to sending a letter to the property owners so that they can voice concerns they may have regarding the trail. He further noted that Homer Electric Association has no objection to a trail placed in the utility easement.

Commissioner Brann briefly explained the plans to establishing the Mud Bay Trail and where it would travel. He briefly described the path starting at the end of the existing Spit Trail progressing up the hill; once on top of the hill there has been cleared out a path to the airport access road. He described further more details in placement and thoughts on locating portions of the trail from the portion of the path known as Mud Bay Trail up to the Bay Club. He commented that this portion may be pedestrian friendly only.

There was no further discussion.

D. Parks & Campgrounds Report – Angie Otteson

Ms. Otteson reported that camping season was closing at the end of the month. She said that the revenues were down for this year compared to 2010. The revenues collected were:

\$125,818.00 Spit Campgrounds
\$19,390.00 Karen Hornaday Park Campground

Ms. Otteson stated what she believed some of the circumstances causing the shortages were economy, lack of fish at the Fishing Hole, and the weather was not the best. There were brief comments about dealing with the conditions of the Fishing Hole. The last of the seasonal workers are done and completing

Commissioners commented on the efforts and work done at Karen Hornaday Park by the various groups and organizations. Commissioner Cumming gave a detailed explanation on the history and efforts by the Friends of Woodard Creek; the effect of the fill that was dumped on the one parking area; the requirement to attend to the priorities as already outlined for the park; the monies as promised by various groups and persons for the betterment of the park. She further elaborated on the hundreds of hours also put into the plan for the park. She was afraid that the funds would be nickel and dimed away.

Chair Bremicker questioned the monies promised by the Little League and if it may be "in kind" services.

There was \$9,000.00 being held by the Kachemak Bay Conservation Society according to Commissioner Cumming which encompassed the funds "promised" by Little League.

There was more discussion on the use of funds currently available for work on Karen Hornaday Park. Chair Bremicker asked if this discussion could be postponed until the October meeting.

There was no further discussion.

C. Kachemak Drive Path Committee – Commissioner HARRALD
1. Synopsis of the September 8, 2011 Meeting (Laydown)

Commissioner HARRALD did not attend the last committee meeting she reported the response on the Resolution presented at the meeting on Monday by the members of City Council. She personally spoke to many members to get their reasoning for not approving the resolution was due to the wording included "Action".

Chair Bremicker reported that he spoke with Julie Engebretsen in Planning since City Council remanded this resolution to the Planning Commission. He opined that they gave the Council too many options and that the Commission should present to Council one plan for approval. He was told that once a project has been approved by the State it still takes 5-9 years to complete. He opined that HART funds should be used to complete the trail, they have a revenue source, and he believes that the community is agreeable that this trail is a priority. Ms. Engebretsen stated that a bond was not needed that the City could even get a loan. He believed that a single plan design option etc., real clear specifics that council can vote on. Commissioner HARRALD agreed but the one red flag she heard was the council being very protective over the HART funds and something about using those funds to extend Waddell Avenue. She stated that they will have to be really clear on the funding options.

Chair Bremicker explained that the HART Funds covered Roads and Trails. There was specific percentage for roads and separate percentage for trails. The Reber Trail was built with HART funds. Chair Bremicker stated that if they decide to use the funds for 10 years to pay off the trail along Kachemak Drive, well with Council approval. Commissioner Cumming noted a report by Lynn Burt regarding a review of the HART funds that could be done annually.

Ms. Krause outlined the steps that will need to be taken regarding the Resolution that was presented to Council on September 12, 2011 for the proposed path/trail along Kachemak Drive.

Ms. Krause called a point of order since the discussion really should be continued in committee not commission and was more appropriate for the committee to have this discussion regarding the proposed resolution.

Ms. Krause will extend an invitation/request to Ms. Mauras, Finance Director to attend the October 6, 2011 committee meeting to explain how the HART funds are used, etc. Ms. Krause will include a copy of the HART in the next committee packet.

Commissioner Harrald reported that comments received from cyclists feel that everyone's solution to safety is to get them off the road and the reality is they belong on the road and it should be addressed in a safe manner.

Commissioner Brann stated that two recommendations from the committee that will be presented at the next commission meeting. One recommendation was to install Share the Road signage and the Radar Feedback Speed signage. This will aid in increasing awareness and following the speed limit.

There was no further discussion.

D. Parks & Campgrounds Report -- Angie Otteson

Ms. Otteson was unable to attend this meeting as planned. She related that she did not have anything of dire necessity to speak about that could not wait until the October meeting.

There was no further discussion.

E. Community Schools & Recreation Report -- Mike Ilg

Mike Ilg reported that he recently attended the Alaska Recreation and Parks Conference in Seward, Alaska. This encompasses all facets of recreation from maintenance to citizen volunteers. It offers a great opportunity to network with others, however it does show him how far behind in recreation that Homer is. He strongly supported and recommended the commissioners attend a conference. They offer scholarships and you can also volunteer to help at the conference. Mr. Ilg reported that the conferences for the next three years will be 2012 Anchorage; 2013 Juneau; 2014 Unalaska; 2015 Fairbanks and he has suggested Homer for 2016. He noted that there is a lot of positive ideas and interaction. He will be attending the quarterly meeting with the School Board on using their facilities with the City Manager. He will keep them updated. They have some new activities to offer table tennis, Zumba dance class, herbalist classes, and youth running program and possible running club soon. He has a strong solid support of the camp host program and believes it will help protect against vandalism.

In response to a question from Chair Bremicker he informed the commissioners that the conference is normally held in September normally after moose season.

Commissioner Cumming inquired if there was any idea on the time frame a campground host would be needed at the park. Mr. Ilg did not consider a time frame in the prospect of having a camp host he was just expressing his support of the concept and figures the commissioners can flesh out the idea under the pending business item.

In response to a question about drainage Mr. Ilg responded that Commissioner Cumming would have to check with Mr. Meyer or Angie Otteson.

Chair Bremicker stated that Commissioner Harrald's inquiry was out of order regarding the award of a contract on drainage.

Mr. Ilg stated that the benefits attending the conference for the commissioners would be very good. In response to a final question on the status of the HERC building and he stated there is currently no update on the status of the building. He would really love to have the building for the community recreation program. He will keep them informed. Mr. Ilg left the meeting at 6:23 p.m.

There was no discussion.

F. Staff Report -- Renee Krause

Ms. Krause inquired if there were any questions. She described the progress with the work being done on the City Hall Renovations. The Budget is on her Staff Report and under the section for Parks and Campgrounds.

Minutes 22 March 2012, K-Drive Path Committee

Conference Room Upstairs at City Hall

Attending: Bumpo Bremicker (chair), Dave Brann, Dave Clemens, Beth Cummings, Lindianne Sarno (recording), Mike Stockburger (new member); Absent: Lynn Burt

Call to order, 5:40 p.m.

Dave Brann moves; Beth seconds approval of agenda, agenda approved.

Dave Brann moves Beth seconds approval of minutes, with this addition: regarding Beth's fundraiser idea, Dave Brann has doubts (1) because of what happened at Karen Hornaday Park, (2) is concerned that privately raised petty cash would be used for items that should be paid from HART funds. Minutes approved.

No public comments, no reconsideration, no visitors, no committee reports, no hearings.

Pending Business: Continued Discussion and Planning. Dave Brann hands out and explains the draft path maintenance plan (see attached handout). Summer and winter maintenance differ. Pack snow or plow it? Dave Clemens advises winter bicyclist's bike on studs. Path in winter will be multi-use: ski, snowshoe, bike, run. If plow in winter, melts quicker in spring. Regarding maintenance, we need to research: who owns the trail? The City? Don't make an assumption. Trail would be maintained at a high level, like a multiuse trail. City plows East End Road path, which is paved. K Drive Path will not be paved and plow vehicle could deform gravel structure.

Dave Brann is working on proposed funding plan. HART funds (Homer Accelerated Roads and Trails), \$304,554 in that account. \$100,000 added annually. Can be used for initial survey, materials, engineering, building, hiring contractor. Bond issue not needed, since HART funds are a reliable income stream, City can borrow against that and repay over time. STIP estimate was \$35-40 million, way too high. K Drive Path Committee is asking for city staff time, not funding. Other potential sources: Homer Foundation, donations, fundraisers, STIP, grants.

We describe path to Mike to get him up to speed. Mike owns Homer Boat Yard on Kachemak Drive and drives heavy rigs on K Drive frequently.

Packet that will go to Parks and Recreation and City Council: Make sure pages 57, 58, and 59 are removed from packet.

Private landowners on Kachemak Drive: will need each individual's permission. City Manager Walt Wrede will write the letter asking for right of way. . It's on his desk.

We agree to lobby City Council. Beth Cummings > Beth Wythe, Dave Brann > David Lewis, Dave Clemens > Mayor Hornaday, Lindianne Sarno > Brian Zak, Mike Stockburger > Barbara Howard, Bumpo Bremicker > Barbara Howard. We will wait till we have the whole packet to complete our lobbying assignments.

Walt Wrede joins us.

Beth Cummings wants to see this summer a retaining wall planned at the west end of the proposed trail, cut trees, get plans going.

Walt comments that Dave should bring a plan. Dave plans this summer to concentrate on making Mud Bay Trail a four foot wide walkable path.

Beth: Mud Bay Trail is inappropriate for bicyclists, OK for pedestrians. Find a way to build a terraced walkway or retaining wall on south side of drive. Also, have a bridge made beyond the west end of Aviation Leasing, over the culvert.

Btumpo: will need real engineering on that section of trail.

Dave Brann: This summer, signage. Small speed feedback signs. Need DOT permission? Letter from city not necessary. Dave will photograph existing signs and include with map to DOT. Share the Road program.

Digital signs are \$3,000 each, differing degrees of information can be harvested, depending on cost: count vehicles, record speeds.

Signs go up on Mud Bay Trail when snows melt. "No Camping." Sign on two 4 x 4 posts, "Mud Bay Trail." Lynn and Beth will arrange this.

We choose meeting dates: April 5, May 24 (no Dave Brann), June 14.

Comments: Thanks to Mike Stockburger for joining the committee. We are making good progress.

Adjourn: 8 p.m.

Kachemak Drive Path Committee

Thursday, February 23, 2012, 5:30 p.m.

Bumppo Bremicker, Chairman, Beth Cumming, Lynn Burt, Lindianne Sarno, recording, Dave Brann, David Clemens, Kevin Walker, visitor.

Motion to Approve Agenda: Beth moves to put New Business B. before New Business A. No objections, we approve this change. Beth feels it's important to get our resolution done and ready to go to Parks and Rec.

Dave Brann moves to approve, Beth Cumming seconds, no objections, approved.

Motion to approve minutes: Beth moves. Dave seconds, no objections, minutes approved.

Public comments: Kevin Walker, when shall we discuss my packet of info? Beth thinks this packet should accompany the resolution to City Council, and it will be discussed then, including Kevin in the discussion.

Pending Business: Continuing Discussion and Planning for Path Design, page 7. Kevin Walker at the microphone. Regarding providing public with access to and from the airport for pedestrians who get off planes. Kevin's letter to Jennifer Bailey cleared up; we are not proposing to trespass on airport property, but rather to provide safe non-motorized transportation corridor for people coming to and from the airport. Dave Brann reports that Jocelyn Biloon and Scott Thomas, say DOT is working on this. They recognize there is a committee, and Dave is their contact. Walt Wrede, City Manager will get in touch with Ms. Biloon regarding a traffic count. Jennifer Bailey is our contact at DOT Public Transportation Facilities, Aviation Leasing. She is in Anchorage. Kevin Jones is the airport manager.

Bumppo points out that if we are in the DOT right of way we have to follow their requirements, regardless of whether DOT is paying for it. (1) Survey (2) Engineered drawings (3) ADA design specs (4) erosion control (5) contract with DOT to provide maintenance (6) liability and insurance coverage. City of Homer sponsorship is crucial, especially for ongoing maintenance.

Dave Brann points out most of the easements are private property easements. They are not a big stumbling block, especially with approval of City Council. Utility easements are outside DOT right of way. Until we survey each section on the ground we won't know for sure but huge section of road is outside DOT right of way.

Dave Clemens: suggests we send DOT a package about the DOT right of way on the sections that are in DOT right of way. Kevin Walker says Jennifer Bailey says this is already in DOT planning. Next step is contacting the City Manager, the City Manager contacting state planning, and getting back to us.

Bumppo: what we need is for city to officially to buy into this and deal with DOT.

New Business: page 67, reformulating the Resolution on the Kachemak Drive pathway. City Council sent it back to Zoning and Planning, who sent it back to us. It was too muddy last time.

Add to the resolution; include a funding plan mentioning that there will be \$305,000 by the end of 2012.

Beth suggests we send to the city this resolution, the Kachemak Drive Path specifics packet created by Kevin Walker, and a funding plan.

FUNDING PLAN: the city can borrow money at good rates, against the HART money, need not be bonded, borrow against the revenue source for 10 years. Julie Engebretsen, Planning Department, says this is a common practice. Funding plan would include private donations and HART funding. Page 95 of our packet gives an outline of the Planning and Approval process.

WHAT WE MUST ACCOMPLISH: Bumppo: We want City Council to direct city administration to start focusing on this pathway. Dave Brann commented, to direct city staff to do what they can to keep this moving along. Give the city a plan for maintenance. Bumppo interjected that we still need to contact landowners, how will they feel about this. Dave Brann: this is on top line of things to talk about with the City Manager.

Beth: Two City Council members commented, "We want to know what you have in mind."

DOCUMENTS WE NEED: (1) resolution (2) packet from Kevin is ready. Complete these two items tonight. Then Renee can advise us on form of (3) funding plan (not ready) and (4) a letter to private landowners will come from the City. (5) Maintenance plan.

We revise the resolution. See resolution on next page.

Beth moves that we approve Resolution 11-090 as revised and send it on to Parks and Recreation and the City Council; Dave seconded. Unanimous approval. See resolution on next page.

Bumppo moves that we assign a council member to each committee member, and well in advance of the City Council meeting bring to them the packet including: resolution, Kevin Walker's packet, and funding plan. Dave Brann says we'll not have a projected cost until the survey. Dave Clemens suggests we mention that we've scaled back from the East End Bike Path to less than \$1.5 million per mile.

Beth proposes changing our Committee's mission statement: "to build a safe non-motorized separate path along Kachemak Drive, connecting East End Road to Spit Road via the airport. Kevin's packet is called our preliminary engineering packet.

Beth proposes a fund-raising event to raise \$5,000 to \$10,000 petty cash. She has engaged a professional event producer, Sally Oberstein, to produce the event, and is proceeding with these plans as a private citizen. Funds would be used for chain saws, Surveying, signage, and other uses.

Dave Clemens mentions that the Homer Cycling Club approves increased signage to increase safety on Kachemak Drive for those commuting cyclists who will continue to use the road. If Kachemak Drive is improved, the cycling club would support widening the shoulder for the safety of commuting cyclists.

Beth wonders if the Mud Bay Trail is included. We see the Mud Bay Trail is included in Kevin Walker's pre-engineering packet.

We discuss the plastic walkway and decide a request has gone to the city for the walkway.

Report on December 10 Field Trip: Dave Brann said it was valuable, going from the airport lot, down, looked at possibility of cut-off trail. It will be challenging. Walked along the beach to the road. Site visitation was valuable. Clarified and provided yellow line and blue line on the pre-engineering packet. Lynn agrees with Dave. Beth says the foundation is there on Aviation Leasing land for a full quarter mile, bridge would be used to cross the gully, then pleasant going among the trees. Yes, it drops steeply and bicyclists could walk there. Bicycle trail would be fairly easy along the Mud Bay Trail.

Kevin Walker: revised mission statement as noted. Will include resolution. Lindianne will e-mail resolution to Kevin as well as to Renee. Will leave off last three pages.

homerkev@gmail.com, clerk@ci.homer.ak.us,

Re funding and maintenance: Dave, Kevin, Renee, will put those together, and Kevin will send them out to the Committee via e-mail. HART funds: \$234,719 at end of 2011, \$305,000 by end of 2012.

Dates: Next Parks and Rec is 15th of March, City Council will be 26th of March. Possible for this resolution and preliminary engineering packet to go to these meetings. Renee, please put the Kachemak Drive Path on the agendas for both those meetings. Bumpo will call or e-mail each Committee member to assign a City Council member.

Our next meeting: March 22, fourth Thursday in March.

We adjourn. 9:05 p.m.

CITY OF HOMER
THE REVISED RESOLUTION:

City Clerk/Parks and
Recreation Advisory Commission

RESOLUTION 11-090

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA,
SUPPORTING THE CONSTRUCTION OF A NON-MOTORIZED
PATHWAY TO INCREASE SAFETY FOR MOTORIZED AND NON-
MOTORIZED USERS ALONG KACHEMAK DRIVE LOCATED
WITHIN HOMER CITY LIMITS, FROM THE BASE OF THE HOMER
SPIT TO EAST END ROAD

WHEREAS, The Parks and Recreation Advisory Commission established the Kachemak Drive Path Committee to specifically address possible solutions to the hazards presented to non-motorized and motorized users of Kachemak Drive; and

WHEREAS, The Kachemak Drive Path Committee received substantial public input on safety concerns; and

WHEREAS, The Homer City Council has shown support for this non-motorized pathway by inclusion of the Kachemak Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the Homer Non-Motorized Transportation and Trail Plan; the Homer Area Transportation Plan; the Climate Action Plan; and the Homer Accelerated Roads and Trails (HART) Policy Manual; and

WHEREAS, Increasing non-motorized transportation improves public health and safety; encourages tourism; cleans the environment; reduces transportation costs; and enhances livable communities; and

WHEREAS, The City of Homer has available HART trail funds that can, including other sources, form a basis for funding this project; and

WHEREAS, Utility easements can be employed with the permission of property owners.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, hereby supports the construction of a non-motorized pathway along Kachemak Drive within the City of Homer, and that said improvement will enhance safety and benefit the public; and

BE IT FURTHER RESOLVED that the City Council of Homer, Alaska further supports actions increasing safety for motorized and non-motorized users of Kachemak Drive in the following ways:

1. Increasing the usage of signage warning drivers of bicycles and pedestrians on roadway.
2. Build a separated path paralleling Kachemak Drive using utility easements and public property, and increase the use of signage.

Minutes, Kachemak Drive Path Committee, November 22, 2011

Attending: Beth Cummings, Kevin Walker (visitor), Bumpo Bremicker (chair), Dave Brann, Lindianne Sarno (recording), Lynn Burt, David Clemens

Call to order, 5:30 p.m. by Bumpo

Agenda approval: Beth moves to approve, Dave Brann seconds, passed.

Minutes approved: Dave Brann moves, Lynn seconds, passed.

Pending Business:

Meeting dates: December 15, 2011, Thursday, 5:30 p.m.

January 11, 2012, Wednesday, 5:30 p.m.

Continuing discussion, planning for path design

Dave Brann shows us a guide to path design from Minnesota Department of Natural Resources and other sources. We are aiming for a ten foot gravel path with sections of wetland and water crossings which will require other techniques. Techniques we are examining are all city approved. Page numbers here refer to hid guide to path design.

Page 27 - bridging, grading discussed. Page 29. NFS means non-frost susceptible. P 6.46, p 6, p. 6.58 boardwalks and bridges, ways to cross drainages. Bridge would be needed to go down to Spit from airport parking lot. That grade is very steep on the road (12%). Regarding airport leasing, there is a 50 foot x 2 DOT right of way.

Dave Brann recommends we use these materials to develop a final packet to present to City Council. Dave will ask Renee if she can create a packet for the committee. He suggests we draw a line on the map and ask Renee to copy it for the committee. Kevin Walker discusses with committee the segment from airport to Spit.

Dave Brann suggests we meet as an announced group and walk that section and other sections. We select December 10, 11-1 p.m. We invite Kevin to join us. Meet at airport parking lot and go to wetlands at other end of path.

Kevin discusses the sheet he created, accurate to +/- 100 feet. We use these numbers to identify suggested areas for trail types. Dave wants to correlate these numbers to the map.

Bring to field day: 100' tape, GPS device, range finder binoculars.

We break for five minutes and reconvene around visitor table. We correlate numbers to map. We will generate even more detail during field trip.

We return to U-shaped table and continue meeting. City council meeting, November 28, Dave Brann and Bumpo will attend, and will advise City Council of level of detail we have attained.

Visitor comments: Kevin is glad to participate

Beth: specifics are wonderful. Is writing to Jennifer Bailey about Aviation Leasing.

Lynn: Excited about specifics

Lindianne: will e-mail dates to Renee, then notes on Monday

David: Excited about field trip

David: Ditto

Bumppo: Ditto

Bumppo adjourns meeting at 8:10 p.m.

Kachemak Drive Path Committee

Meeting, November 7, 2011, 5:30 p.m.

1. Call to Order – 5:30 by Chairman Bumpo Bremicker

Attending, Beth Cummings, Bumpo Bremicker, Dave Brann, David Clemens, Lindianne Sarno, new member. Ingrid has resigned. Bumpo appoints Lindianne Sarno, who is recording minutes.

Agenda approval. Dave moves to approve. Beth seconds. No objections. Agenda approved.

Motion to approve minutes from last meeting. Dave moves, Beth seconds. Minutes approved. Beth wishes to discuss Mud Bay Trail.

Public Comment on Agenda Matters: Lynn Whitmore, Chair, Kachemak Bay Moose Habitat, visitor.

Lynn Whitmore: Lived here forty-odd years. Seven years Homer Fish and Game Advisory Committee. President. Kachemak Drive Moose Habitat, mitigation funds as result of lake of Bradley Lake hydro project. Moose and black bear and goat affected by that project. Ended up with \$187,000 to work with, now have about \$1,000,000 property, \$250,000 in bank. Fairly successful. At request of Fish & Game, trying to acquire habitat around Beluga Lake to save Homer Bench herd. I like to eat moose, I put time in to give them food, get food back from them. Fundamental. Understand a little of what you propose, put trail through our property, how can we help, need to hear what you guys have in mind. Trail design, trail size. My job is to feed moose or give them a quiet resting spot. Beluga land isn't so much food as quiet resting place. They get stressed in March and April, die off occurs then. F&G says not better buying property in lowlands, moose have been coming down to wetlands. Need quiet place to sit and rest. Trails, OK, through good habitat, trails going along right of way, OK.

Bumpo: Conservation easements – is that what your land is called?

Lynn: No. Land Trust deals with conservation easements. We have notes on some our property, in perpetuity agreements. Private donations and parcels, our stuff, some has easements. Around Beluga Lake.

Beth: Three pieces, west of Arctic Tern Road, is Moose Habitat Incorporated willing to let this city-owned trail go across?

Lynn: How wide? Type? Quality? Any place where there is potential for water/sewer services or HEA events, you want to be aware those services could bisect trail. My board would not be glad to have trail in moose habitat. But it would be OK if you keep trail close to Kachemak Drive.

Dave Brann: have not drawn line on map. Starting from Airport Manager's office, keep under power line, all the way to Arctic Tern, that looks do-able. HEA has no objections.

We don't want to do this without full support of landowners. Contacting Moose, Inc is part of education process for us.

Lynn: I recommend you come up with statement of trail width and materials. Do something right on foundation, in case it will have to be paved someday. If I go to talk to my board, they will want to know how big this is. Some landowners have been whacked with assessment and the take. Some people won't be thrilled to have someone else come along. Bisect some other way, may not take it very well. Path of least resistance. Say to folks, look they're changing your front yard anyway, won't plant native vegetation, we'd like to put trail through. Folks may not want trail in front of or behind their house. Easement for water and sewer is already there, concentrate there.

My board will be interested in uninterrupted quiet space, as you get away from road, power line has cushion of trees between road and power line. Keep it quiet. That's what you'd be up against.

Dave: Could you provide us with map of properties you own.

Lynn: City Planning department. On maps. Figure on a ten foot footprint. Water and sewer project. My board includes chair of Alaska Energy Association. Other is in charge of all refuges, McNeil River sanctuary. Both concerned with water flow on surface of Beluga wetland. Water sewer project changed the flow of things. Fish and wildlife made a fuss with city. If you do anything to further change that flow, you'll run into further headwinds. Again, keep it close to water and sewer project.

Bumppo: Most of trail, drains away from the bluff. Most folks didn't get that. It drains towards slough. Put in culverts to deal with that afterwards. Water and sewer takes that into account and is sloped in that direction. Trail on top could interrupt that flow. Needs to be way to let flow get across that trail.

Lynn: Landowners won't soon forget that impact on bay side. Catch it early and design it properly.

Dave: Geoblock. Familiar with? Inch and half thick in delicate areas.

Lynn: Is it a foundation? Can you pave on top of it? PSI? You'll be compressing material.

Bumppo: This helps us a lot. Drainage issues, quiet zones.

Lynn: We've all watched moose die near trees. Need to not get disturbed so they don't burn energy. In a lot of cases, you'll be dealing with already stressed landowners. Better now. I would get a fairly quick professional opinion on trail format and size. Settle what trail will look like, cost at 10 feet, prepping for future pavement. East road, huge substantial trail, had to be designed to repave. State parks will have standards for designs for wetlands. Might be leftover material from Islands and Oceans.

Dave: Bishops Beach material will be replaced. Possible to get that material.

Lynn: High value wetlands. EPA allows people who get in trouble to mitigate properties through us. We get asked to make sure Larry Herndon isn't into wetlands while ground freezes. Did core samples, they

have this amount, allowed to do now, rest after ground is frozen hard. Better to piggy back on water/sewer project.

Lindianne: how best to interface with those guys?

Lynn: (1) decide what trail will look like. (2) High value wetlands, forested HEA right of way, trail design may change. May be able to use Islands and Oceans stuff in drier parts. (3) Where will you put trail? F&W, F&G, ACoE, describe two paths, right of way and other. Have that together, then go sit with them and talk it over. Once you get your route A and route B, and trail design, have accepted building material laid out. Go to entities that have designed trails, those are the two or three types, high value wetlands, drier, highlands, enough due diligence to ask for their opinion. They may say no input until finalize. Or may be helpful. Give them some ownership of project, input up front.

Dave: How do moose deal with boardwalks?

Lynn: Moose would cross without too much problem. Heavy snow year, moose use path of least resistance. Snomads have gone into high country when wolves were getting into big valleys, pack down snow so moose can get around. Make paths to next valley. Next day, moose are out of there. Don't make it high, slick. They go across the highway.

Beth: Who for trail design?

Lynn: Jack Sinclair, in charge of state parks, Kenai Peninsula. DOT for trail design. They've done big ones and small ones. DNR state parks has one landscape architect who does trails. He will send you to somebody.

Dave Brann: Alaska Trails has design people.

Lynn: Homework: pull up borough base maps, turn on feature that shows trees, aerial imaging, look to your designs, you can come along here and see how much of each type, have that available as it goes across different parcels.

Bumppo: We're fortunate to have Kevin Walker, retired highway engineer, here. Thank you for valuable information.

Lynn: My name is in phone book. Also work part time Jay Brandt.

Bumppo: Pending business: specific strategy.

Kevin Walker: I made trip charts, a have a file, can print out to draw a line on a map. This section is wetland, this section is high, steep slope, etc. for different types of trail.
Wet, dry, steep. Three types.

Looked at 38 page sewer and water project, a little less than half the trail. Go up behind the gear shed and boat yard. Legal easement there. Davis Street. There is room for ditch and trail.

SWPP plan, whole plan, say they will grade, and plant. If they are doing that work, park money, access to, equipment is there already, have them lay down tyvar and put some gravel on top of it. Erosion resistant. Compact. Make sure water doesn't pick up dirt when it flows across it. Piggyback on their corps of engineers and permits. We'll help you out, put in some volunteers, in-kind contributions.

Bumppo: Advantage of putting this on top of existing, we don't have to excavate. Just add something on top. Disadvantage is drainage issue Lynn was talking about.

Kevin: while they're excavating, do our foundation as a trough in the middle of their excavation.

Bumppo they quit because they're waiting for it to freeze up for those wet areas. They seeded already.

Dave: Get in touch with Larry Herndon.

Bumppo: Larry is for this project.

Beth: Ongoing conversation with his wife. He will be gone two weeks, be back late November.

Lindianne: Move that we get in touch with Larry and invite to our December meeting.

Bumppo: I will get in touch with Larry Herndon.

Beth: Part of trail from base of spit to Arctic Tern. Few landowners there. We need to work on both at same time. People need to see section that has been accomplished. I am talking with landowners. One landowner said, you should go across my land. He will try to talk to other landowners. This is doable.

Bumppo: To get approved, we need a plan for whole thing.

Beth: Would like to go into detail for first mile and a quarter. 35 landowners from end of sewer and water to east end road. Middle starts at Arctic Tern. Limited number of landowners, most of whom are businessmen, they will get behind it.

Dave Brann: Progress with water and sewer now. I agree, work on that section. Get Larry in here. Specifics. Later in agenda, letter: has to go to commission and then to landowners.

Beth: Talked to Julie Engebretsen, City Planning, I mentioned the letter. She said the letter is not to you. That's the job of the staff to do.

Dave Clemens: Lynn laid this out perfectly. Width, design, materials. What it is. Do our due diligence. Specifics: City trail manual.

Kevin: East End road is ten foot. Major trail. Paving equipment doesn't go less than ten feet. Can pave eight feet. Dig trench and fill, that will be flat, eight feet with no slope.

Bumppo: Put down decent base, could pave later. Just figure out width and surface.
Wetland area, would need special work to pave later.

Dave Brann: Safe Level 3 trail description. Page 16. Informal trail, semi-urban to rural, through neighborhoods. Use primarily pedestrian and include bike and horse. No shoulders beyond surface top. Surface material type is 4 inches not frost acceptable gravel over geo textile, boardwalk, or other reinforced type materials. Steps discouraged, less than 8% grade. Their preferred design manual for trails. If necessary include boardwalks, same width.

Dave Clemens: Page 32, non-motorized transport plan, description. Multiuse path, ten feet. Like East End path.

Bumppo: Kevin have seen material dredging out of harbor? Lots of it above Pier One Theatre. Wonder if that material would do? Sandy gravel.

Beth: Jeff Middleton said, it's mostly sand, would use as fill, would have to put gravel that impacts on top to be useful for bicyclists.

Kevin: Has to be combination of sizes, sharp compactable particles, crushed. 30% or more have to have sharp fracture faces. Mix dredging with crushed gravel. Presents

Bumppo: Ten foot wide, geotextile under it, level three. Boardwalk included in some areas. Tough enough to have snow removed. Small snowblower. Maintenance cost.

Detailed plan:

From base of Spit, flat on trail, until pull off place at base of hill. Fill along side of road, leave grass strip. Or boardwalk along that section. \$100/foot for boardwalk. (Need permission from Jocelyn Baloon for working in that right of way. Boardwalk in right of way, but would need permission from ACOE and F&W for pilings.) Grass strip or barrier separates bike path from road. This takes us to base of hill.

From parking lot at base of hill, trail goes off through patches of trees, drops down to wet area, next patch of trees, over hill; to wet area, then hugs the edge of the grass, mostly at toe of the bluff. Ten foot wide gravel path. Until airport access road. Multi-use recreation bicycle-pedestrian trail. Plan includes signage out on road for bicyclists who continue to commute on the road: Do not pass bicyclists on this hill!

There is (1) option to go up the long slope to the west end of long term parking, rejoining huge road shoulder; would require bridging culvert; (2) easiest option is to go up airport access road. Consult with experts, Dave Brann suggests asking Kevin Myers about this section. Dave Brann will call Kevin Myers. This is our only problem area.

Up access road to long term lot across from airport. Research Reserve building, all part of Aviation Leasing. Follow yellow line, sewer line, to Bay Club Driveway, filled trail along that section. Little bit of leveling, geotextile, and gravel, will take care of that section. Cross the road right there, to right of airport manager's office. Cross there because few driveways for bicyclists to cross, good line of sight. Now you're on northside of K Drive, follow blue line, along road to end of boat yard. A few puddles in there, mostly dry, gets wet last lot of four. Power line easements go along there. From end of boat yard, follow power line easement across Lampert Lake to Caravan Trucking Company, section there is somewhat wet. Use plastic boardwalk, or Geoblock. Geoblock maintains trail structure, but nature works through it. Plants, water, gravel can all get through. Could help us get across muskeg/wetland. Stay close to road, but separate. Stay under power line on Moose, Inc. property. To Arctic Tern.

At Arctic Tern, powerline continues parallel to road. Stay with it. From Arctic Tern to curve, terrain is dryish, muskegish, dry enough for tyvar and 4 to 6 inches of fill. At curve, just before house, cross the drive way, stay close to the road, then cut back to powerline easement. Terrain is relatively dry, use Geoblock, gravel, or turnpike style. Old mill, go in front of mill, stay on powerline. Right across from Morris Avenue. Property owner says OK to go in front of his house. Cut back to road in front of Rob Zolo's property. Join water and sewer.

On water and sewer line, fabric and gravel construction. Easiest for maintenance of water and sewer line. Until high value wetlands. Short high value wetland sections. Where spruce trees grow, fabric and fill gravel OK; where terrain is grassy, will need Geoblock. All the way to boat yard. May be able to use plastic boardwalk from Beluga Lake trail. Boatyard, use water sewer line. To Davis right of way. Big ditch is there, put in large cement culvert, lay in ditch, cover with non-frost fill.

Bumppo: Agenda: Close meeting. 8:30.

Beth: Carey Meyer said we need formal letter, requesting plastic material to be removed from Bunnell Street. Beth will write the letter, Bumppo will sign. Beth moved, Dave seconded.

Bumppo: meeting dates: City Council is 28 November. Go to City Council and ask for resolution back. Do not need vote on it. Need ten days to advertise. Our next meeting will be Tuesday the 22nd of November, 5:30 here, Parks and Rec is 17th of November.

Bumppo will call Larry Herndon and see if we get him to come to that meeting.

Dave: Will keep chain sawing logs out of Mud Bay path.

Bumppo: Adjourns meeting at 8:30.

Minutes for Kachemak Drive Path Committee, Thursday, October 6, 2011

Bumppo Bremicker, chair, Beth Cummings, Dave Clemens, Lynn Burt, Dave Brann, Lindianne Sarno recording

Bumppo called the meeting to order, 5:30 p.m..

Motion to approve agenda by Dave Brann, seconded by Lynn Burt.

Agenda approved.

Beth Cummings moved to approve minutes, seconded by Dave Brann.

Minutes from last meeting approved.

No public comments.

Visitor: Regina Mauras, Finance Director, City of Homer. Regarding HART policy, spoke with Walt Wrede, for better clarification. Walt reiterated that any trails built need to be adjacent to city roads, city property. State road wouldn't be considered. Suggests that path committee come up with exactly what you want, run it through the attorney's office and see if it meets the policy. This is what we intend, is it within what City intended? Funds are available for non-motorized trails. \$110,000/year available for trails. Actually: \$226,222 at end of 2010. \$305,000 will be available at end of 2012. Trail does not have to be along the road, then it's considered diverse, and can be within meaning of HART. Sidewalk has to be along city road, trail not necessarily along city road.

ADA compliance necessary. Dave Clemens suggests we get full wording of Resolution 7-82 because this is referenced by ADA requirement.

Once you get full approval resolution, then ask council if we can access these funds for this trail. Get an Ordinance for however many dollars you think it will be. Can request loan from city from City funds. Look through project section of budget, funds can come from different sources. HART, general fund.

Bumppo: say it costs \$1,000,000, that would be ten years of HART funds, could city access loan using HART funds to pay it off? Regina: there are precedents.

Dave Brann: Once city approves use of those funds, how do you access just the amount you need. Regina: Look at project, e.g. \$5,000. Ask for whole amount. Once it's been approved you have three year window to complete project. Spend it as you use it.

Dave Brann: If this committee were in charge of building this trail. Go to SBS to get materials, they bill city? Regina: Through contractor, we would get bill from contractor, with costs of labor and materials. If you were building, it would go through the city.

Regina: Will get copy of Resolution. (Delivers copies of Resolution 7-82)

Beth: Over water utility easement, one segment or phase from East End road, second segment from base of Spit going as far as Artic Tern Road, third section in between there. Three phases. Regina: Put that in a resolution to get blessing of council. Ordinance would be once we have a dollar amount.

Pending Business:

Dave Brann: Met with HEA and got response: no problem with use of HEA right of way for bike path. Beth questions Dave on water sewer line plan, some electric lines seem to be underground. Discussion: Dave says wouldn't want us to raise with so much fill that maintenance would be difficult under lines.

Dave Clemens: In Anchorage, trails run over easements. Dig up if needed, then patch again. Beth: get letter from Anchorage? Dave Clemens will get a letter from Anchorage. Gas, water, sewer, electric, easements are the best way to go for bike paths.

Dave Brann: Right of ways are reserved for utilities. Bumppo: State right of way? Dave: It varies, 30 to 50 feet. When road was rebuilt, state didn't have money to have consistent right of way. Beth: State claims land along Kachemak Drive that goes across ditches to be able to cut willow. Prescriptive right of way. Dave: advantage not having path on DOT right of way.

Discussion of possible funds source. HART program. Page 15 of packet: Bumppo: we meet most of these criteria. Need to discuss property owner participation.

Beth: Reiber Trail is not wheelchair accessible. ADA applies?

Bumppo: could be done at Kachemak Drive.

Dave Clemens: Re staying off DOT: city council may try to push this off on state, keep in mind. Bumppo: City has been saying for ten years let state build it. I don't see this ever being number one on CIP list. Maybe we can get some partial funding from the State. Dave Brann: State DOT resists building bike trails. This will cost way less than most people imagine. Much can be done by volunteers. I'm concerned about permission from landowners. Once we decide where line on map is, potential costs known, we can possibly look for other sources. But start with HART funds. Bumppo: Use ten years of HART funds: \$1.3 million available. Matching funds from state. Grants. If you have pool of money to start, it attracts other money. City Council is concerned about money. Come up with solid plan.

Beth: I talked to four council members. One said, go for it. Second person said, you need to simplify, direct and be more specific in purposes. Two other people said, I don't want any unforeseen commitments financially for the city. (But I didn't mention HART funds.) We don't even know if there are trails that have higher priorities. Trail between Senior Citizens and Pioneer possible.

Bumppo: I testified at City Council and brought up HART funds. I made it clear that the HART trail funds are a separate fund and have to be used for trails. This won't take any money from road building funds. I made that clear.

Beth: Regarding Reiber Trail. It was not related to roads. It connected a road and another road. Purely connecting two roads and paid for by HART trail funds.
Lynn: Reiber Trail: Is it through private property? Dave Brann: Within piece of property that was donated to city by a homesteader.

Bumppo: At some point we should make recommendation to city. If offroad trail is our intention, we should move to do that and get this buttoned down.

Dave: Any other encumbrances on HART funds? Bumppo: none that I know of.
Regina said this is what's left after encumbrances, at end of 2012.

C. Discussion and Recommendations on Defining Purpose and Goals of this Committee

Beth: Mission statement should stick to one path paralleling Kachemak Drive for safety of non-motorized traffic..Dave Clemens: We have five or six different notions. Beth: would like to get back to having a separated path, separated from K Drive, emphasizing safety of bicyclists and pedestrians. Include reference to need for smooth flow of traffic. Would like to give one choice to go to Parks and Rec and City Council.

Bumppo: change available funding, contingent on "available HART trail funds" and give specific numbers. On page 25, new resolution: path paralleling K Drive.

Dave C: goal of this committee: trail separated from road generally, or will this committee also obtain easements? Is goal to get blessing or is it to get specifics?

Bumppo: Blessing of City Council has been accomplished. Our goal is to make this happen. The more specifics, the better their reaction will be.

Dave: Line it out, this is what we want to do. We should plan out the trail that we want, that makes sense, is workable, is a good trail. Then look at funding. Design a good piece of infrastructure, then go for it.

Bumppo: I agree. Put in clear funding sources. In resolution: this is our plan, here is how to pay for it.

Beth: Would like to provide exact wording for new resolution. Old resolution on page 25. Concept and construction of a non motorized pathway to increase safety for motorized and non motorized users along Kachemak Drive located within city limits. And to encourage the smooth flow of traffic for both commercial and noncommercial users along Kachemak Drive.

Line 7, "these safety and traffic flow concerns, construct separated non-motorized path"
Line 15 leave out "and"

Take out line 16 through 20.

Put in a whereas: before line 22, be it resolved City of Homer Alaska supports concept and construction of a separated non-motorized pathway along Kachemak Drive.

Add: WHEREAS lanes are mainly only 12 feet wide with little shoulder space between road and ditch and mainly prescriptive right of ways,

WHEREAS, this project is dependent on funding, HART funds are available for this project.

Line 29, be it further resolved that the city of Homer, etc ... as well as supports action to encourage smooth flow of commercial and non-commercial traffic,

Line 31 omit

Keep line 32, a separated path paralleling Kachemak Drive utility easements

Bumppo: discussion?

Dave Brann: smooth traffic flow for commercial vehicles. I don't think we have to worry about traffic on road. Separated path is our job, creating smooth traffic flow is not in our purview.

Bumppo: Wider reasons for this path, it doesn't hurt to have them in there.

Dave Clemens: This is a significant change. We're concentrating on a path. This is good. Moving forward. A gravel based trail will not remove all cyclists from the road. Cyclists will say, "gravel path? I'm on the road." Would still like to see wider shoulders and signs on the road because bikers will be on the road. Relieving traffic flow: there will be bikers on the road.

Bumppo: Lindianne looked on internet about surfacing trails. City has amount charged to homeowners. Paving costs half as much as gravel base. Asking Kevin: cost of paving and subsurface?

Kevin Walker, retired state highway engineer. I worked in Nome and Kotzebue where it cost hundreds of dollars a yard to get materials. I know state had a program to pave everything black because maintenance coat is lower. Once it's paved, don't have to grade. Have to tear up once it falls apart. City charges less for paving than for gravel?

Bumppo: page 16, number 6 seems to say gravel costs more than paving.

Kevin: passed two people riding on east end road tonight. Several types of bicyclists. Several kinds of roads. Can ride a bike 30 mph on compacted highly crushed gravel. Pavement great as long as it's maintained. Gravel starts at 30/foot and then the paving goes on it. Crushed high quality gravel surface. Base is expensive.

Dave C: City council meeting, idea of bikes off the road? Bikes have a legal right to road. Bikes won't disappear. Public needs to know it's OK for bikes to ride on Kachemak Drive. Get bikes off the road is wrong message to send. It's not the law.

Beth: whereas all of the north side of K drive is commercial and routinely used by commercial sized vehicles, add that whereas to resolution.
Do you feel there should be mutual courtesy for bicyclists to ride down middle of lane?
Saw two bicyclists with dog on rope with dog wandering all over. If

Dave C: I'm in middle of lane, don't pass me. It's not safe. Trucks go slowly, I couldn't pass truck. If cars stack up behind me I pull over and let them pass. I try not to slow cars for more than 12 seconds on West Hill Road.

Lynn: Everybody is responsible for safety of all. Good citizen.

B: I wait until it's safe to pass cyclists on K Drive.

B: Comment: Not all commercial on North side of Kachemak Drive; it's zoned rural residential where I live. Want to continue this? Hammer out resolution now?

Lynn: great to hear what Beth had to say. Would be good if we had Ingrid here.
Vote to wait on that.

Dave B: wait. We need to be able to answer city councils questions. We don't have the line drawn. Will it all be on water and sewer line easements? We talked about work party concentrating maps and where we want the trail to go, so we have clear picture

Bumppo: Table this discussion. We're starting to get it. We need to get more specific. Thank you, Kevin, for your input.

Kevin: Plans, specifications and estimate. Take care of easements. Huge job. I'd be happy to work with you guys and try to put the line on the map. From here to here, this kind of construction, each section of trail has typical section. Might have three or fifteen types of sections. We need a sketch, a readable document, assume can go across airport leasing, cross south to north side at some place, I volunteer to help get it started. Pencil and paper sketch will result.

B: Work session, you'd be valuable.

K Can work with Dave or whoever, here's where we have to go through trees, make it so when we come to work session we're not starting from scratch.

Bumppo: schedule this: we're now on new business. Leave time for preliminary work.

Dave C: Have connectivity plan all the way from spit to East End,

Kevin: From Artic Tern Road to East End Road it's all laid out. Good to go.

Beth: 35 property owners from Artic Tern to water sewer line. Have started calling. Listened to him for half an hour on Eminent domain. Anne Whelan wildly enthusiastic. No one else was happy. Planning commission voted to put it on the STIP list. Dave: STIP list does not mean right of way. State Transportation Improvement Plan.

Dave: Getting back to worksession, plan work session, get it on paper first. Until we draw lines on paper here, then we are spinning our wheels.

Kevin: I will download data from boro, doing each 100 feet on paper. Need project plans from sewer. Will be a stack of paperwork. Will line out the entire path.

B: Our next scheduled meeting is: Monday November 7th. Kevin will attend. 5:30 in here. That will be the worksession. Two items: Resolution and the segmented plan.

Parks and Rec Commission meets on 20th of October.

Beth: wants to talk about Mud Bay Trail. Simplest part of whole trail, from base of spit to Artic Tern Lane. Five segments. Want to mention segment B. No shoulder. Have faith in Dave Brann being able to do this: Enormous quantities of fill. Two terraced trails, one for eastbound and one for westbound. Only difficult part in whole trail.

Bumppo: Table this discussion til next time.

Comments of committee:

Dave C: good that we can get refocused. Move forward! I'll be leaving committee due to time constraints.

Dave B: envision committee as ongoing, changes in members. Some kind of trail from one end to the other. I like the idea of stuff on the roadway, won't happen in short term until road is rebuilt. We can make real progress if we look at maps and landowners. Put the line on the ground. Drafting letter to landowners. Many do support the trail. Find out who doesn't support the trail. Options range from do nothing to donating easement. Selling easement. Making conservation easement. Tax incentives. Find out their individual concerns. If can't cross, go around. Slow, frustrating but we're getting there.

Lynn: B: I agree, trail will happen. Thanks for Mud Bay trail. Beth and I marked high tide with new stakes. 23.1 tide is second highest this year. Dave has mowed and its wonderful.

Beth: I e-mailed thank you to you Dave. Glad Dave brought up possibility of perk for people who agree. Checking with land trust to see if they have ideas. Nature conservancy. Will call them. See if they have an idea that didn't occur to us. We're ready to be more specific.

Bumppo: Lindianne and I drafted a letter to landowners. Not that many owners. Maybe 100. Get together with Dave Brann and compare notes.

Beth: Can I keep calling people?

Bumppo: That's fine.

Keep plugging away.

Thank you all.

Bumppo adjourns about 8:30 p.m.

Kachemak Drive Path Committee
491 E. Pioneer Avenue
Homer, Alaska

October 6, 2011

To: Property Owners Along Kachemak Drive

The Kachemak Drive Path Committee is working to build a pedestrian and bicycle path along Kachemak Drive. The City of Homer plans to build the bike and pedestrian path along the utility (water and sewer) easement or under HEA power lines. Both forms of easement cross private property such as yours. Therefore the City of Homer would need your permission to build the portion of the path where the easement crosses your land.

Alaskan state law provides that where established trails cross private land, property owners are not liable for any injuries that occur on their lands on those trails.

Do you give permission for the Kachemak Drive bicycle/pedestrian path to cross your property?

Yes.

No. I have further concerns. Concerns: _____

Signed, _____

Name _____

Date _____

Address _____

Kachemak Drive Path Committee
Minutes September 8, 2011

Meeting called to order 5:37 P. M.

Members present: Beth Cummings, Dave Brann, Lynn Burt, Dave Clemens, Bumpo Bremicker

Moved and seconded to approve agenda

Moved and seconded to approve minutes

Public Comments:

Kevin Walker supports developing a path on top of the water / sewer easements. He will help the committee any way he can.

Lindianne Sarno: Expressed concerns about a gravel pathway as it is difficult to ride on. She volunteered to research various path surface types.

Val McLay: He supports a separated pathway but opposes narrowing the travel lanes. He sees narrowing the lanes as creating a bigger safety issue. He thinks bicyclists should ride in the road as allowed (not try to squeeze in to a two foot lane which is not safe for kids) or push for the separated path.

Beth Cumming read a letter from Trina Fellows into the record: "I think turning an industrial road (Kachemak Bay Drive) into three lanes so bikers can have a lane is a very dangerous idea. From February to October oversized boats & gear are being towed from all the boat yards to the Spit road. Large trucks also use this road. This would be an accident waiting to happen. Thank you, Trina B Fellows"

Jeff Middleton, applauded the effort to create a bike trail but is opposed to narrowing the travel lane. He feels that would be very unsafe.

Pending Business:

Bumpo and Dave plan to attend the city council meeting on Monday. The committee didn't establish a schedule of who would attend city council meetings in the future.

Beth mentioned she had contacted city council members about a month ago and may do so again.

No new information on the use of fill at this point although it was noted that dredging is once again going on and that it would be cheaper to haul it once from the source rather than having to store it then haul it again. Beth talked with Bryan Hawkins and the gravel is being advertised to be sold.

McLay did provide some information on how the harbor enterprise fund works and there would still be some expense in getting the dredge materials. He said the city contracts to have the material hauled away. He also provided some history and precedence of using dredge materials off the spit.

Dave has not contacted HEA yet as he is waiting for more information from a couple of sources before making the contact. He stated he is working on it.

No changes, progress or info related to the idea of narrowing the travel lanes. Beth did have emails from businesses along Kachemak Drive that are opposed to narrowing of the travel lanes. She was going to pass them on to the city council.

It was moved and seconded to pass two recommendations on to the Parks and Recreation Commission, the Homer Transportation Committee, the City Manager and the Homer City Council related to signage on Kachemak Drive.

1. It is recommended that two yellow and black "Share the Road" or "Bicyclists / Pedestrians on the Road" signs be added to existing advisory signs along Kachemak Drive. Locations to be: A. Between the Spit Road / Trail and the airport hill, B. After the Boat Yard going towards the spit.

2. It is recommended that two "driver feedback" (digital radar) signs be added to existing advisory signs on Kachemak Drive. Radar signs would flash driver's speed if over the established speed limit.

Homer Non-Motorized Trail Plan (Section 5.0, pg. 56) states:

"Creating safer bicycling conditions on Homer's most traveled bicycle routes is a high priority. The routes include Kachemak Drive."

"To further the continuity of this route, (East Road Bike Path) Kachemak Drive must become safer for non-motorized transportation."

"A rehabilitation project for Kachemak Drive that includes a separated multi-use pathway has been identified as an essential need of the City's Capital Improvement Program."

Section 5.0, page 55-56 Near Term Improvements states:

"Near term improvements for Homer's non-motorized transportation infrastructure should address immediate safety concerns...and the creation of safer bicycle conditions.

"For the purposes of this report, Homer Non-Motorized Transportation Plan, June 28, 2004, a near term improvement is one that will be realized in the next two years."

*Signs could be purchased with a combination of private, city and grant funds. Signs could be installed and maintained by volunteers.

Approx. costs, two yellow advisory signs \$50.00 each
Two radar signs \$3000 each: Total \$6100.00 + or -

The Committee added discussion of the Hart Funds to the agenda.

Lynn gave a brief report on the monies available and noted that Regina would be willing to come to a meeting to explain more about the funds. Currently \$234,719.00 available.

Bumppo suggested that we ask the city to build the trail and use the HART funds to pay it off. There is an annual increase of \$40-\$50,000 based on a percentage of the sales tax collected within the city.

There was discussion of using the water and sewer easements for the trail. There was some question on how best to approach it. Dave B. mentioned that Carey and Walt said that when the city obtained the easements for water and sewer some landowners specifically stated they didn't want a trail on the easement. It was felt that the committee needs to find out what the landowners concerns / objection are in order to explore possible solutions. No action taken or suggested.

NEW BUSINESS:

It was moved and seconded to name the portion of the trail from the Spit Trail to the Airport Beach access road the "MUD BAY TRAIL". Motion passed. A brief discussion on the historic name of Coal Bay as opposed to Mud Bay.

It was moved and seconded to place routed signs along the trail. Beth thought 12-15 signs with directional arrows would be needed. Motion passed.

Discussion on signage related to illegal camping along the Mud Bay Trail and other city properties. Beth felt signs designating the Mud Bay Trail would be enough to discourage campers in the future. She supports a kinder-gentler approach. Dave B. stated he didn't have the patience that Beth did and was frustrated by the abandoned campsites, accumulated camping debris, and waste disposal (or lack of). As an individual (not as a committee member) he will ask the police about impoundment of abandoned tents, etc.

Bumppo brought up the concept of a free camping area to accommodate those in need. Will add it to a later agenda.

Lynn said she did complain to the city police about the illegal camping in the early summer but that no action was taken. She was referred to Angie Otteson but wasn't able to make contact. Someone mentioned that Angie was willing to check out the sites but wanted someone to go with her. It doesn't seem like that happened.

Discussion on Portion of Proposed Pathway from East End Road to the West End of the Kachemak Drive Water / Sewer Phase II Project.

Beth felt that a turnpike type of trail construction would be inexpensive and a good way to cross some of the wetland areas along the sewer / water easements. Dave B. concurred that the turnpike construction, fabric, retaining logs along the edges, sand fill with a cap of gravel or other surface material was a standard trail construction technique in wet areas. It was noted that anyone could recommend this type of construction to the city council but the committee felt it was too early to present construction techniques when permission to utilize the easements hadn't been obtained. Discussion continued on how "how to make this happen". Dave B. said Walt was going to look into the easement language to see what could or couldn't be done, it was suggested that the committee continue to research what the concerns / objections to having a trail on the easement might be. Bumppo felt the city council should decide on how to make the trail happen.

Discussion on the Proposed Implementation for the Segment of Trail from the End of the Spit Bike Trail to Arctic Tern Road. Beth presented ideas on a segment of trail along the edge of the parking areas near the top of the bluff. It would require permission from Airport Leasing. She stated the cars parked there pay nothing for the use of the land and the parking could be adjusted to accommodate a trail. She talked with Paul Seaton's office about the possibility of a waiver from ?? in order to accommodate a trail in that area. Val McLay provided a little history on that section of land noting that it was a dump and trees have grown up on it holding it in place, he was concerned about cutting any trees that might compromise the bluff. Beth felt no trees would need to be cut.

Dave said that with permission of landowners he would be able to use a trail mower to mow the power-line easement from the Airport Managers office to Arctic Tern with only a few wet areas or ditches to be addressed. It wouldn't create a trail but provide a visual representation of the possibility.

Discussion and Recommendation on the Mission Statement of the Committee was postponed until the next meeting.

Comments from the audience: Val supports the turnpike style of construction with the use of D-1 gravel as a surfacing material. He suggested the committee look into flashing signs that could be rider (bicycle) activated as they used the road. The signs are used on various highways and in road tunnels when bicycles are utilizing them. Don't remove any trees from the top of the bluff. He doesn't support the idea of free camping as a way to accommodate current illegal campers.

Comments from the Committee:

Dave C. wants to discuss the Committee goals, feels we have become a little scattershot in our approach.

Dave B. Some recent improvements to the Mud Bay Trail, check it out.

Lynn will write a letter to the editor about illegal campers.

Beth said no trees would be cut with her proposal and that she did contact many business owners along Kachemak Drive.

Bumppo said we would look at the mission statement of the committee and that "We're not done."

Meeting adjourned at approx. 7:30

Next Meeting is October 6, 2011 at 5:30 p.m. in Council Chambers at City Hall

Minutes submitted by Dave Brann

Renee Krause

From: Beaver & Jessie Nelson <bjnelson@alaska.net>
Sent: Tuesday, September 06, 2011 9:25 PM
To: Renee Krause
Subject: Kachemak Drive Bike Path

Sept 7, 2011

City of Homer

ATTN: Rene Krause

RE: Kachemak Bike Path

I am commenting on the various proposals to turn Kachemak Drive into a very unfriendly road for the working people of Homer to gain access between the Spit/Boat Harbor and the businesses along Kachemak Drive and the Gear Shed on East Road.

This road is not a scenic byway - it is an industrial road used to transport boats, trailers with fishing gear, freight hauling 18-wheelers and for airport activities. The entire fishing population east of Kachemak Center, which includes the Russian villages, uses it to access the Spit. At times, several trips are made per day between the Spit and the Gear Shed or other businesses along the road.

Do we really want all that traffic going through town? Narrowing the lanes and implementing a 25 mph speed limit would be ridiculous! There is a mile-long straight stretch with a double yellow line - to go 25 mph with not another car in sight is just not going to happen. This would be like an office worker's computer running at half speed. Incredibly infuriating. Do we want our police force enforcing this limit? I don't. Actually, the speed limit between the Bay Club and Northern Enterprises should be 40-45 mph.

I like bike paths. It just needs to be along the utility right-of-way so the working people do not bear the burden for other peoples' pleasure. Leave Kachemak Drive alone.

Jessie Nelson
Mile 5 East End Rd.

From: "Thomas, Scott E (DOT)" <scott.thomas@alaska.gov>
Subject: RE: signage on Kachemak Drive, Homer
Date: August 26, 2011 11:08:44 AM AKDT
To: Molly Brann <brann@alaska.net>
Cc: "Jones, Kevin L (DOT)" <kevin.jones@alaska.gov>, "High, Carl S (DOT)" <carl.high@alaska.gov>, "Vanderwood, Randy D (DOT)" <randy.vanderwood@alaska.gov>, "Huling, Lawrence K (DOT)" <larry.huling@alaska.gov>, "Rader, Val S (DOT)" <val.rader@alaska.gov>, "Morton, Kenneth M (DOT)" <ken.morton@alaska.gov>

Mr. Brann,

As we discussed, I recommend the following:

1. SHARE THE ROAD signs.

A. A State perspective: DOT/PF has no policy/rules, program, or resource on where best to do this or proof they are effective. If we did, we would likely choose higher priority roads on a southcentral Region basis. Kachemak Bay Drive has visibility, and it is our practice to limit the use of warning signs to things people need to know because they can't see the problem. Bicyclists can be seen on this route and sharing the road should be obvious to the driver due to the lack of shoulders and other facilities.

B. A local perspective: State priorities are not the same as local priorities. What I can recommend and support is a local government plan for signing for SHARE THE ROAD or BIKE ROUTE[s] or pathways. This is the approach we are taking in the Municipality of Anchorage, where the city creates a priority plan for bike routes and pathway circulation regardless of road ownership. They identify in a plan the routes and connectivity in an area. Then they are responsible for signing those routes. We would entertain an agreement for route signing that is conducted at the local level with continuity in mind. That will get you away from a Regional priority system for investing in signing and into more of a system that makes sense and is easier to coordinate/plan at the local level.

2. SPEED signs - driver feedback, battery operated.

A. DOT/PF is not opposed to these, but again, is only prioritizing these and maintaining these at the Regionwide level - with our highest priority being to test the effectiveness of these in our Safety Corridors - high speed, high volume highways with a crash problem. For the same reasons as above, (resources, funding, priority) we are not doing this on other roads.

B. Similar to bicycling signs, we have agreed to allow the City of Anchorage to test driver feedback signs on state roads. We do require they take speed measurements and document effectiveness before they are allowed to remain indefinitely. I would recommend a local level approach to the use of these signs for education and policing information to the driver, with a local priority system. Same as Anchorage and local use of speed carts. We would entertain an agreement led by the City of Homer to have a local effort in speed feedback signs, as long as it is part of a prioritized plan, funded by and operated by the City.

Thank you for calling.

Scott Thomas
Traffic Safety Engineer
DOT/PF Central Region
907-269-0633
scott.thomas@alaska.gov

-----Original Message-----

Jo Johnson

From: Renee Krause
Sent: Friday, July 29, 2011 7:50 AM
To: Melissa Jacobsen; Jo Johnson
Subject: FW: Request for Resolution from Councilmember Zak

See below. I can draft one if you are too swamped. I believe the goal is to have it on the agenda for August 22 cc meeting since Francie was gone for the first meeting.

Renee Krause, CMC
Deputy City Clerk I

-----Original Message-----

From: bryanzak@aol.com [mailto:bryanzak@aol.com]
Sent: Thursday, July 28, 2011 10:22 PM
To: Renee Krause
Subject: Re: Excerpt from the Parks and Recreation Minutes of July 21 2011

I will sponsor a resolution to this effect.

-----Original Message-----

From: Renee Krause <RKrause@ci.homer.ak.us>
To: Bryan Zak <BryanZak@aol.com>; Francie Roberts <roberts2@alaska.net>; David Lewis <davelyn@gci.net>
Cc: Beth Cumming <listentothewaters@yahoo.com>
Sent: Thu, Jul 28, 2011 12:05 pm
Subject: Excerpt from the Parks and Recreation Minutes of July 21 2011

The below is an excerpt from the Meeting Minutes of the Parks and Recreation Advisory Commission regarding recommendations submitted on a proposed Kachemak Drive Pedestrian and Bike Path this was requested from Commissioner Cumming to be emailed to you seeking support for the recommendation and a resolution from Council to continue/proceed with the concept.

I believe this may be brought up at the Council Meeting August 8th.

COMMITTEE REPORTS/STAFF REPORTS

- C. Kachemak Drive Path Committee - Commissioner Harrald
 1. Synopsis of the June 17, 2011 Meeting
 2. Synopsis if Surveys Received as of July 13, 2011
 3. Survey received as of July 1, 2011
 4. Recommendations to the Commission
 5. Memorandum dated July 14, 2011 Discussion on Recommendations to Council from the Committee

Chair Bremicker requested a motion and read the motion submitted by the Committee into the record as follows:

THE KACHEMAK DRIVE PATH COMMITTEE OF THE HOMER PARKS AND RECREATION ADVISORY COMMISSION RECOMMENDS THAT THE FOLLOWING BE PASSED AND SUBMITTED TO CITY COUNCIL:

TO SUPPORT THE ACTIONS INCREASING THE SAFETY FOR MOTORIZED AND NON-MOTORIZED USERS ALONG KACHEMAK DRIVE IN ANY OR ALL OF THE FOLLOWING WAYS:

- ALTERATION OF THE EXISTING KACHEMAK DRIVE AND SHOULDER
- A SEPARATED PATH PARALLELING KACHEMAK DRIVE USING THE UTILITY EASEMENTS
- LOWERING THE SPEED LIMIT AND INCREASING THE USE OF SIGNAGE
- THE PARKS AND RECREATION ADVISORY COMMISSION WILL WORK IN COOPERATION WITH THE CITY OF HOMER IN THE PLANNING AND IMPLEMENTATION PHASES.

BRANN/ARCHIBALD - SO MOVED.

CUMMING/LILLIBRIDGE - MOVED TO AMEND THE MOTION TO READ "TO SUPPORT DEVELOPMENT OF PATHS ALONG KACHEMAK DRIVE FOR NON-MOTORIZED USERS INCLUDING WALKERS, RUNNERS AND BICYCLISTS TO INCREASE SAFETY FOR BOTH MOTORIZED AND NON-MOTORIZED USERS IN THE FOLLOWING WAYS.

Discussion proceeded on the wording changes and that the word flow and intent of the motion was the same and the original recommendation was appropriate. The maker and second pulled the amendment from consideration. Chair Bremicker commented that the motion was acceptable as written.

Commissioner Lillibridge re-read the motion as written for clarification.

VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Ms. Krause responded to questions regarding the process and what happens when they submit the recommendations to City Council. She informed the commissioners that since this item was not and has not been on the council agenda, the recommendation can be verbally presented at the August 8, 2011 Council meeting during Commission Reports, a memorandum from the Commission could be submitted for inclusion in the packet. It will be up to the Council if they would like to proceed further on the recommendation and it must have Council support for the recommendation to have anything further done. The commissioners must realize that the recommendation could be presented to council and that is it, nothing more happens even with the support of councilmembers. The issue will then be finished and the job of the Committee may then be complete. She recommended contacting council members to try to gain support for further action and implementation of the recommendations that will be forwarded by this commission. She confirmed that Council has the option to request a resolution to express support also.

Commissioner Cumming commented that she would like to do as much as possible to ensure that the council supports the recommendation. She expounded on her reasons for having this approved by council.

Renee Krause, CMC
Deputy City Clerk I
City of Homer
491 E. Pioneer Avenue
Homer, Alaska 99603
Ph. 907-235-8121 ext. 2224
Fax. 907- 235-3143
rkrause@ci.homer.ak.us

"Successful is the person who has lived well, laughed often and loved much, who has gained the respect of children, who leaves the world better than they found it, who has never lacked appreciation for the earth's beauty, who never fails to look for the best in others or give the best of themselves."

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PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

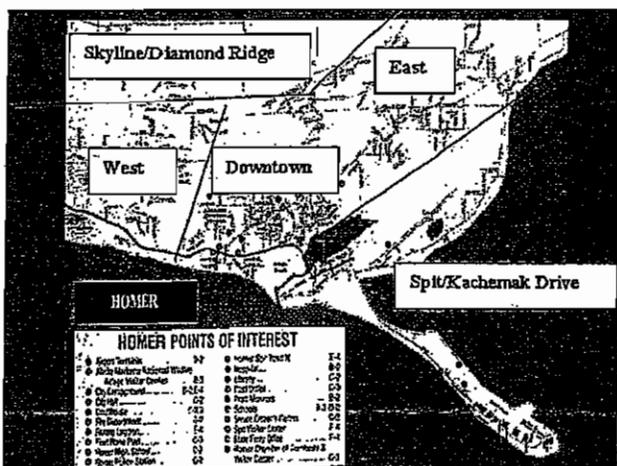
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Lon Zitzmann

Address (Optional) 599 Hidden Way

City, State, Zip Homer, AK



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike

Other Loop - bike ride to, 237

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|--|
| <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input checked="" type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

Support

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

strongly support
safer for all

Support
safer for all

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

No

safety +
preserving scenic
by way drive option

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

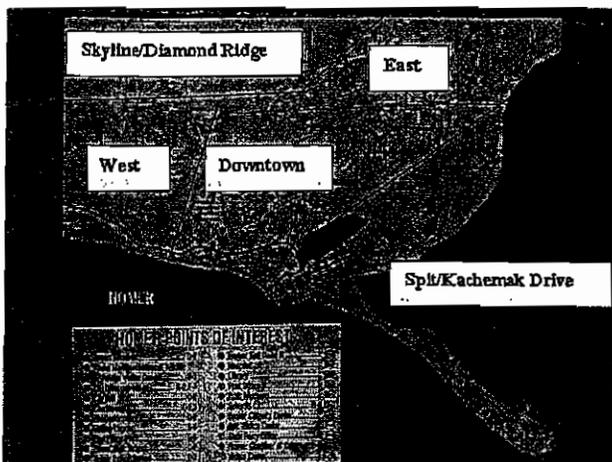
Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Name (Optional)

Address: (Optional)

City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
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- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
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3. How often do you ride a bicycle or walk along Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike or walk, which of the following describes why you use Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

6. If you drive a car, which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|---------------------------------------|--|--|---------------------------------------|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?
 The ability to safely ride a bike or run on the road. Additionally, it should NOT be commercialized

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

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YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Renee Krause

From: chase warren <likes_to_roam@yahoo.com>
Sent: Wednesday, July 06, 2011 11:56 AM
To: Renee Krause
Subject: path survey
Attachments: survey.kdpc_.06.21.11.xml

Excellent idea to put this online!

<?xml version="1.0" encoding="UTF-8"?>

<form1>

<TextField3>41347 mclay rd</TextField3>

<TextField4>kachemak city, ak, 99603</TextField4>

<Answer>East Homer</Answer>

<Answer/>

<Answer/>

<Answer/>

<Answer>Downtown Homer</Answer>

<Answer/>

<Answer>Occasionally, about once or twice a month</Answer>

<Answer>Regularly, once or twice a week</Answer>

<Answer>Routine Errands</Answer>

<Answer/>

<TextField1/>

<TextField1/>

<TextField2>Chase Warren</TextField2>

<Question11>How about 30? Or, and I know the committee has little to do with this but, what if we actually enforced the speed limit? Make it a double fine zone. 25 is kinda pushy, considering nobody wants to go 35.</Question11>

<Question10>Good luck. If the cost, right of way issues, dealing with the airport, dealing with the bog, cooperation with HEA, and the time it would take to deal with all those things were not issues, then yes I am for it. Create a shoulder first, then tie yourselves up with this project for the next 10 years.</Question10>

<Question5>I am in full support of this option. Although it is not an ideal solution, as it does not safely open up the shoulder to children/families, or persons with special needs. However, the cost/effectiveness of this option for out way the others. A consistent shoulder width is not necessary to immediately alleviate the dangers of riding on the current uneven and rock/sand riddled shoulders. No fill is needed, which dramatically reduces the cost of creating a shoulder. In addition, I would propose more signs, both speed limit and be aware of pedestrian/cyclists signs.</Question5>

<Question3>The trail would promote tourism, the shore bird festival would benefit greatly. Perhaps increased traffic would cut down on blatant littering. Also the trail seems to cut right through areas currently being used as squatter camps, the remnants of these camps are evident by the bags of trash and weather logged materials scattered along the bluff. I'd rather see people appreciating the area, not abusing it. The downside of this option is that it really doesn't address the inherent safety hazards of riding and/or walking on K drive, and could distract from the real objective of the committee.</Question3>

<Question1>Ideally, a separated ADA approved path connecting E end road to the spit, with alternate nature trails headed north and through the bog that could be used as ski trails in the winter</Question1>

<Question2>Every year since I have lived here I have seen more cyclists and higher gas prices. If you want people to be able to afford to live here, something that has always been an issue, the city design should be able to accommodate alternative modes of transportation.</Question2>

<Question13>considerations made by the state/city/drivers who speed/drivers who pass a pedestrian or cyclists with another car coming in the other lane at the same time also, the condition or lack of shoulder. the lack of considerations by others could be alleviated by a sufficient shoulder</Question13>

<Question6>This would be absolutely negligent, which is my current opinion of the situation. K dr. has become a major through route for those working/ travelling to the spit or ocean drive. For everyone's safety there needs to be a shoulder. If nothing is done now, when there is the opportunity, any accidents in the future would reflect quite clearly that the state and the city has not been concerned enough about the safety of persons to do anything. </Question6>

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<RadioButtonList>1</RadioButtonList>

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<RadioButtonList/>

<RadioButtonList/>

<RadioButtonList>1</RadioButtonList>

<Question1>pave out to the edge of the dirt, re-stripe the road, narrow the lanes if at all possible, put up signs, enforce the current speed limit. any or all of these I feel would dramatically improve the situation as is</Question1>

<Question1>Your dealing with a situation that places people within 12-24" of being struck by a moving vehicle. I feel that no matter how difficult or significant the investment that the city has made some major oversights in letting this go on for so long and must do something before someone is hurt. I'm all for the narrow lanes and re-paving option!</Question1>

</form1>

Renee Krause

From: Hayley Norris <hayleybird412@gmail.com>
Sent: Wednesday, July 06, 2011 11:16 AM
To: Renee Krause
Subject: Kachemak Drive Survey
Attachments: Survey Kachemak Drive 07.06.11.xml

attached.


```

<?xml version="1.0" encoding="UTF-8"?>
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  <Answer>East Homer</Answer>
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  <Answer/>
  <Answer>Downtown Homer</Answer>
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  <Answer>Occasionally, about once or twice a month</Answer>
  <Answer>Occasionally, about once or twice a month</Answer>
  <Answer>Routine Errands</Answer>
  <Answer>Routine Errands</Answer>
  <TextField1>and 'Trips to the Spit.'</TextField1>
  <TextField1>and 'Trips to the Spit.'</TextField1>
  <TextField2/>
  <Question11>I think 25 mph is too low of a speed limit. I'd support 30 mph.</Question11>
  <Question10>I do support this trail, but only after the narrower lane/shoulder option. This trail
    would accommodate pedestrians, older and younger, but not cyclists. I think cyclists must be
    accommodated for because the mode of travel is so beneficial to our environment and society.
    So, I only support this option if it is included with a plan to put in shoulders along Kachemak
    Drive for cyclists.</Question10>
  <Question5>This is my favorite option and I whole-heartedly support it. There are so many
    benefits that could be realized with the implementation of this option. For one, the speed that
    drivers drive will be less with the narrower lanes. This makes things safer first off. Next,
    cyclists and pedestrians would have a designated spot to walk and ride if a shoulder is painted.
    This improves safety and encourages residents to get out and walk/ride that beautiful road.
    Also, this option seems that monetarily it would be relatively cheap compared to a separated
    bike path. The only thing not good about this option is that some kids and older people might
    not feels safe on a shoulder alone and this is why a separated path should still be considered as
    a long-term goal. This option could be a shorter-term goal.</Question5>
  <Question3>I do support this option because it would be the beginning or end of a full-length
    Kachemak Drive pedestrian path. Also, locals and tourists could more easily enjoy the
    beautiful views and wildlife in the area. That stretch of Kachemak Drive is one of the most
    dangerous spots for walking and cycling on the road; therefore, a trail could make that safer
    for pedestrians at least.</Question3>
  <Question1>I'd like to see bike lanes and a seperated path for other pedestrians. This will mean
    there is a loop of trails: the Spit, East End Road and Kachemak Drive. Maybe even some camp
    grounds and other nature, bird viewing trails.</Question1>
  <Question2/>
  <Question13>It is unsafe to walk or cycle along Kachemak Drive and it is an important road in
    our town.</Question13>
  <Question6>I do not support this option. Kachemak Drive is an important road in our community
    and it needs to be safer. Kachemak Drive being safer makes this a better
    community.</Question6>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>

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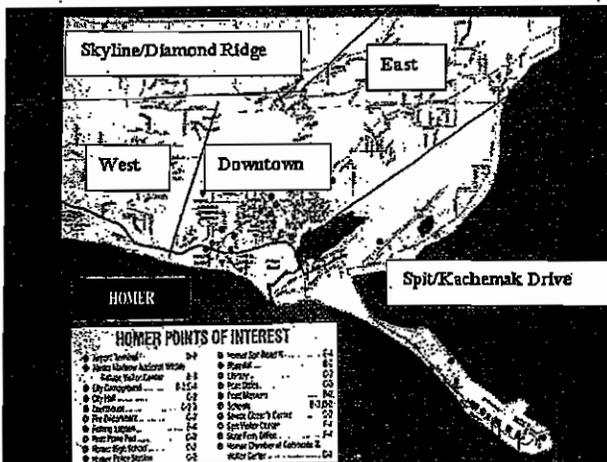


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|--|--|--|--|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input checked="" type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

I can't find where Airport Access Road is.

A better connection to FAA road would be good, esp. the crossing from the W-side of Ocean Drive (heading out the spit) onto the spit trail (E-side) could be improved. A round-a-bout would be a really good idea here.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

Yes, I'd support this option. This is an economical and effective solution. By keeping bicyclists on the road, they are easier for motorists to see, making it safer for everybody.

When going for this option, it is important to implement it on both sides. Cycling in the opposite direction from car traffic is very dangerous because bikes are not expected to come from this direction and their speed is generally underestimated.

As with any of the other solutions, snow plowing and dirt can be

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

Motorists tend to like this option because it gets bikes off "their" road, and inexperienced bikers like it because it gives them a (false!) sense of security.

Unfortunately, this is actually a bad idea in terms of bike safety. Drivers cannot see or do not notice bicycles that far off the road. Riding this kind of trail is often less safe than riding on the road

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

Good idea.

Why limit this to summer months? Driving conditions don't get better during the winter.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

There's a need to improve Kachemak Drive. That said, I would not have given it the highest priority. Main Street, of all places, is an embarrassment for our town. Please go there, get out of the car, and take a walk. Now imagine pushing a baby stroller through the dirt. Now imagine doing this in winter with a foot of snow. It's downright scary.

13. What do you feel are the most pressing issues facing Kachemak Drive?

Intersection with Spit Rd is dangerous.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

Maintain the rural/wilderness character of Kachemak Drive. Widening this drive would be a sad mistake. This is a great scenic drive through wetlands, past baby moose, with views of the bluff and the the bay, and all that right next to town.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

A speed limit along with a narrower road for cars would be a great start. One without the other would be dangerous.

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

Round-a-bout at the base of the spit, intersection Kachemak Drive and Spit Rd. Not that big of an investment, really. Pay-off in smoother traffic and considerable fuel savings compared to the current stop-signs. This would also improve safety for motorists, cyclists and pedestrians.

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Submit by Email

Print Form

```

<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3>41640 Gladys Ct</TextField3>
  <TextField4>Homer</TextField4>
  <Answer>East Homer</Answer>
  <Answer/>
  <Answer>Kachemak Drive and Spit</Answer>
  <Answer/>
  <Answer/>
  <Answer/>
  <Answer>Never</Answer>
  <Answer>Regularly, once or twice a week</Answer>
  <Answer> Other </Answer>
  <Answer>Trips to the Library, Museums, Spit, Parks, etc.</Answer>
  <TextField1>To access the spit.</TextField1>
  <TextField1/>
  <TextField2>Duane Howe</TextField2>
  <Question11>That would be too confusing to motorists to have changing speed limits. They
    already drive 50 mph in the 25 zone. </Question11>
  <Question10>That could work if the easements are far enough from the road to allow several feet
    of separation from the roadway. I walk frequently on the spit and along East Road, and I can
    tell you I would not feel safe with any less space than there is along those paths, and there is
    several feet most of the time. I have known two people who were killed trying to walk or ride
    a bike too close to traffic. I would not risk doing it myself.</Question10>
  <Question5>That would not allow enough space to walk or ride a bike safely. </Question5>
  <Question3>No. It would not be of much use just to go that far. </Question3>
  <Question1>Pretty much like it is now.</Question1>
  <Question2>That road is primarily to access the homes along it, and it should stay that way. There
    is little place for businesses along it because it is mostly wetland which should not be
    disturbed or filled in.</Question2>
  <Question13>Surface water drainage and domestic water supply.</Question13>
  <Question6>Since there is already a path on East End Road we may not need another one to go
    essentially to the same place. Extending the present one farther east would make more
    practical sense to me.</Question6>
  <RadioButtonList/>
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  <Question1>Improve the drainage and rezone the commercial portion to conservation. There
    should be no development in the wetlands. This would add to the recreational value of a path,
    but the cost would be questionable. I have seen groups of bikers that ride out via one route and

```

return via the other route, so this might be an excuse to improve Kachemak Drive to accommodate that sort of recreation. </Question1>

<Question1>Little investment should be made here because most of that land will eventually be eroded away along with the houses on it. There is no way to prevent it as we are learning in other parts of town.</Question1>

</form1>

<?xml version="1.0" encoding="UTF-8"?)

<form1>

JUL 01 2011 PM01:53

<TextField3/>

<TextField4/>

<Answer>East Homer</Answer>

<Answer/>

<Answer/>

<Answer/>

<Answer>Downtown Homer</Answer>

<Answer/>

<Answer>Infrequently; maybe every few months</Answer>

<Answer>Frequently, several times a week or every day</Answer>

<Answer> Other </Answer>

<Answer>Other</Answer>

<TextField1>exercise and to go birding</TextField1>

<TextField1>to go to the spit or businesses that are closer that way for me</TextField1>

<TextField2>Lani Raymond</TextField2>

<Question11>I definitely think you need to acknowledge that there is lots of bike and pedestrian traffic in winter also!! Not so much as summer, but still quite a bit. AND, it is a more dangerous situation because of ice on the road. This road is often quite icy and with the curves and limited visibility, can be challenging for drivers. Please also note that if you reduce the speed limit but don't enforce it, the situation will still be very dangerous.</Question11>

<Question10>This might be another good idea. Possibly safer but more costly.</Question10>

<Question5>I think there should be a narrowing of the road and make a walking/biking lane on ONE SIDE ONLY. The speed limit should be reduced and enforced. The speed limit should also be lower in winter because there is still lots of use (biking and walking) even in winter and with icy roads, the lower limit would still be needed. I think also some sort of "watch for pedestrians and bikers along this road" caution signs should be in place at each end.</Question5>

<Question3>It's a good idea. I walked it last week and there were several people camping down there. Concern about it being a possible homeless area? Also how maintained in winter?

</Question3>

<Question1>Not built up with lots of commercial stuff. It is beautiful as it is now. Hopefully there won't be too much more erosion. If it had a bike/walking path somewhere along it to use safely, it would be a fantastic asset. Plus being able to go from town out East End Rd, down Kachemak Dr then back into town as a circle (or triangle).</Question1>

<Question2>More use of Kachemak Drive for driving and walking and biking. More people wanting to use bikes and walk for various reasons--save gas, exercise, etc.</Question2>

<Question13>Safe travel for: bicyclists, walkers and vehicles. I want to say somewhere that your Question #3 might be going to give you a false sense of amount of use this road gets for walking and biking. AS IT IS NOW, I and many others I know avoid using Kachemak Drive. But if it were safe, there would be way, way more use!!!!</Question13>

<Question6>Obviously something needs to be done!!</Question6>

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<Question1>Figure out what is most affordable and weigh the safety. I also think that you need to do something SOON!! Not just have an elaborate plan for 5 years down the road (pardon the pun there). Reducing the speed limit for vehicles is easy and cheap. People may complain but taking a little bit more time is not the end of the world. (But would have to be enforced.)
</Question1>

<Question1>Ideally there would be a bike path along the whole way but that might not be possible due to cost, land owners not wanting to cooperate, and the fact it is wetland part of the way. If enough people think about it and help plan, there will be a way found to accomplish this.</Question1>

</form1>

Renee Krause

From: Adam Bauer <abauer@bauerhaus.ws>
Sent: Friday, July 01, 2011 6:13 PM
To: Renee Krause
Subject: Kachemak Drive survey
Attachments: survey.kdpc.pdf

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH



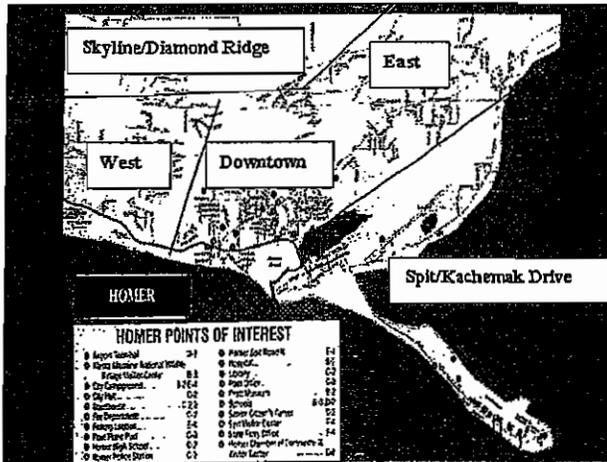
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- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

6. If you drive a car, which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|--|
| <input checked="" type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input checked="" type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
The area will most likely lose more coastline to erosion there will most likely be fewer residents. I doubt there will be any more businesses. The road will become an important transportation link diverting traffic from East Road to Ocean Drive both of

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

```

<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3/>
  <TextField4>Homer, AK 99603</TextField4>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer>Kachemak Drive and Spit</Answer>
  <Answer/>
  <Answer/>
  <Answer/>
  <Answer>Frequently, several times a week or every day</Answer>
  <Answer>Regularly, once or twice a week</Answer>
  <Answer>Commuting - Work or School</Answer>
  <Answer>Commuting to Work or School</Answer>
  <TextField1/>
  <TextField1/>
  <TextField2>Ryan Briscoe</TextField2>
  <Question11>Yes, as long as the speed limit is enforced. Although, you will still have cars that go
    around bicyclists/walkers in unsafe locations.</Question11>
  <Question10>Yes, this would be the ideal situation and would encourage more user groups to use
    the trail.</Question10>
  <Question5>If there is actually enough room to do this safely, then I would support it. I am
    skeptical that there is enough room with the current width of the road to make this
    feasible.</Question5>
  <Question3>Yes, this seems to be the most dangerous spot since cars frequently go around me on
    my bicycle in the middle of the hill where they cannot see what is coming the other
    direction.</Question3>
  <Question1>Kachemak drive should have a trail that connects the Spit trail to the East End Road
    trail. </Question1>
  <Question2>I don't know the history beyond three years, so I can't speak to long term changes,
    but the city should provide safe travel options for multiple user groups.</Question2>
  <Question13>Providing a safe road for all user groups (cars, walkers, bicycles,
    etc.).</Question13>
  <Question6>I do not support taking no action.</Question6>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
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  <RadioButtonList/>
  <RadioButtonList/>
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  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>
  <Question1>For starters, you could add the half mile trail from the spit to the airport access road
    and widen the shoulders on the remainder of the road. </Question1>

```

<Question1>Ultimately, it would be best to have a trail connecting the spit to East End Road.</Question1>

</form1>

Renee Krause

From: Ryan & Erin Briscoe <rebriscoe602@hotmail.com>
Sent: Thursday, June 30, 2011 10:38 AM
To: Renee Krause
Subject: Kachemak Drive Path survey
Attachments: survey.kdpc_06.21.11_0.xml

Hello,

I filled out the survey online and clicked on submit via email. It said to save file and attach to an email, so I did that. It saved it as an .xml file instead of a .pdf file. Hopefully, you can read the .xml file and see my comments.

Thanks,
Ryan

Dear Homer Community Member,

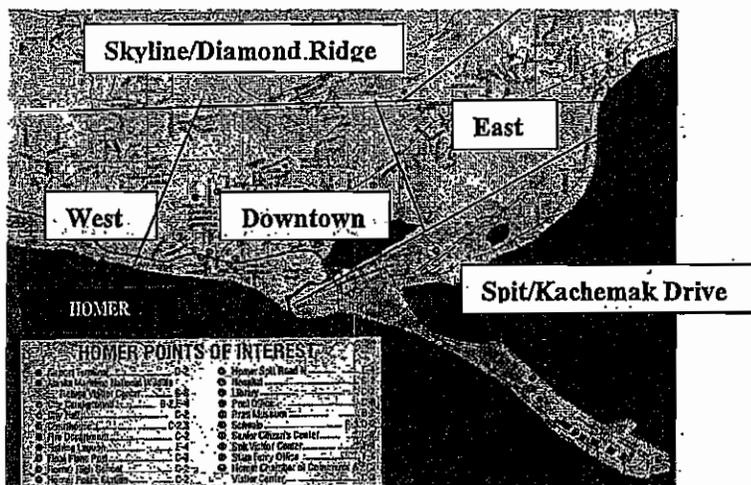
Thank you for attending the Kachemak Drive Proposed Bike Path Open House.

Background: Earlier this year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented tonight are ideas created and supported by the subcommittee, but not put forth as concrete plans. Your input and opinions will help us formulate further actions, if any, that should occur along this issue.

Please help our efforts by completing the survey form below.

1. Please refer to the map below and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge



Please refer to the map and indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

How often to do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently: maybe every few months
- Occasionally: about once or twice a month
- Regularly: once or twice a week
- Frequently: several times a week to every day

If you ride a bike, which of the following describes why you use it on Kachemak Drive. Check all those that apply.

- Regular exercise or workout
- Commuting to work or school
- Routine errands
- Trips to the library, museums, parks, and similar places
- Family outings or touring
- Other (please indicate)
- I do not ride a bike

How often do you drive a car on Kachemak Drive?

- Never
- Infrequently: maybe every few months
- Occasionally: about once or twice a month
- Regularly: once or twice a week
- Frequently: several times a week to every day

If you drive a car, which of the following describes why you use it on Kachemak Drive. Check all those that apply.

- Commuting to work or school
- Routine errands
- Trips to the library, museums, parks, and similar places
- Other (please indicate)

If you could re-allocate space on the road for the following user groups, circle the action you would take:

| Space For | Less Road Space | Same Road Space | More Road Space |
|-------------|----------------------------------|-----------------------|----------------------------------|
| Cars | <input checked="" type="radio"/> | <input type="radio"/> | <input type="radio"/> |
| Pedestrians | <input type="radio"/> | <input type="radio"/> | <input checked="" type="radio"/> |
| Cyclists | <input type="radio"/> | <input type="radio"/> | <input checked="" type="radio"/> |
| Buses | <input checked="" type="radio"/> | <input type="radio"/> | <input type="radio"/> |

The following page refers to the ideas presented at the Kachemak Drive Open House:

1. Half Mile Trail from base of the spit to the airport access road.

Do you have any comments or concerns about this option?

I LIKE IT CAN BE A NICE WALKABLE/BIKABLE RECREATIONAL ROUTE.

Do you support this option? Why or why not?

YES, VERY APPEALING GREAT EASY WALK CLOSE TO SPIT PARKING, A WALKABLE ACCESS TO GRAVEL BAR-FOR BIRDING

2. Narrowing width of road on Kachemak Drive and adding widened painted shoulder for bike lane.

Do you have any comments or concerns about this option?

I THINK IT CAN/SHOULD BE DONE, EASY TO ACCOMPLISH, RELATIVELY INEXPENSIVE

Do you support this option? Why or why not?

YES INCREASES SAFETY, CONNECTS EXISTING BIKE PATHS.

3. Creating a separated recreational trail following current water and sewer right of ways.

Do you have any comments or concerns about this option?

BEST OPTION UNTIL POWER LINE EASEMENT CAN BE UTILIZED.

Do you support this option? Why or why not?

YES, MAKES GOOD USE OF DISTURBED GROUND/PROPERTY

4. Decreasing speed to 25 mph during summer months (defined as non-studded tire months) and making it a "scenic byway" during this time.

Do you have any comments or concerns about this option?

A GOOD OPTION - WOULD BE MORE SCENIC WITH 2-3 POCKET PARKS/OUTLOOKS

Do you support this option? Why or why not?

YES, INCREASES SAFETY, EASY TO IMPLEMENT

5. Take no action on Kachamak Drive.

Do you have any comments or concerns about this option?

NOT A VIABLE OPTION FOR ME, SOMETHING NEEDS TO BE DONE.

Do you support this option? Why or why not?

NO. IT MEANS GIVING UP, ACCEPTING NO FOR AN ANSWER

In addition, we would appreciate your thoughts on the following questions:

What do you feel are the most pressing issues facing Kachamak Drive?

SPEEDING, LACK OF BICYCLE/PEDESTRIAN PATH

How are the community and the uses of Kachamak Drive changing?

BICYCLE USE INCREASING, URBAN USE INCREASING, MORE DEVELOPMENT IN GENERAL

How should we respond to these changes?

SLOW VEHICLE TRAFFIC, PROVIDE SAFE OPTIONS FOR BICYCLES + PEDESTRIANS

What do you want Kachamak Drive to look like 10 and 20 years into the future?

A SCENIC BYWAY WITH A MIX OF BUSINESS, INDUSTRY, RESIDENTIAL - ACCESSED BY IMPROVED ROAD/BIKE/PEDESTRIAN PATH

WIKI

4. Please identify any actions on Kachamak Drive that seem easy, affordable and effective.

WEEKLY PLACEMENT OF DIGITAL SPEED CAP, RADIO/NEWSPAPER EDUCATIONAL SPOTS, SHARE THE ROAD SIGNS, SPEED LIMIT ENFORCEMENT, OTHER SIGNS, PEDESTRIANS/BICYCLES DOUBLE LINE

5. Please identify any actions on Kachamak Drive that you feel we need to take even though they seem hard, but worth it. These actions may be difficult to achieve or may require a significant investment.

PURCHASE TRAIL EASEMENT(S), PURCHASE PROPERTY FOR TRAILHEADS, PARKING, SCENIC OVERLOOKS, ENCOURAGE BUSINESSES/HOMEBOWNERS TO CLEAN UP PROPERTIES.

```

<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3>PO Box 2355 </TextField3>
  <TextField4>Homer, AK 99603</TextField4>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer/>
  <Answer/>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer>Never</Answer>
  <Answer>Occasionally, about once or twice a month</Answer>
  <Answer/>
  <Answer>Routine Errands</Answer>
  <TextField1/>
  <TextField1/>
  <TextField2>Ryjil Christianson</TextField2>
  <Question11>No, if anything the speed limit should be lowered during the winter time when it's
    icy. I think the current speed limit is fine. I do think this would be a wonderful spot to make a
    Scenic Byway though. A trail or walking/bike lane should also be added. </Question11>
  <Question10>On Kachemak Drive? I think this would be wonderful. Kachemak Drive is a lovely
    stretch of road to walk or bike. Unfortunately, there is no room right now for walkers and
    bikers to safely travel. </Question10>
  <Question5>What road are you talking about? The base of the Spit or Kachemak Drive?
    Kachemak Drive is already a narrow road. I would be strongly in favor of adding a bike/
    pedestrian lane. But not if it means narrowing the roadway. The road at the base of the Spit
    may be wide enough to accommodate the a bike/walking lane. </Question5>
  <Question3>Extending this trail would be wonderful. Right now there is a narrow point between
    the Airport Access Road and the beginning of the Spit trail. I often feel uncomfortable walking
    or cycling this stretch. </Question3>
  <Question1>I would like to see turn out spots along to road for trucks towing boats. In the winter
    these areas could also be used as parking for people skating on the lake. </Question1>
  <Question2>When the road was paved the traffic speed seemed to increase drastically. This made
    it even more terrifying to walk or bike along the road in some respects. More people have
    discovered the lake as a great skating area in the winter. It is difficult to park though. More
    homes are being built along the roadway. </Question2>
  <Question13>Safety of walkers and bikers! This is a very scary roadway to travel if you are not in
    a car. </Question13>
  <Question6>NO Something should be done on Kachemak Drive. I have never felt safe walking or
    biking on that road. </Question6>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>

```

<RadioButtonList/>

<RadioButtonList/>

<RadioButtonList/>

<Question1>We could lower the speed limit until we build a seprate walking/ biking lane. I would not be thrilled by this but it would be an affordable change. </Question1>

<Question1>put in a walking/ biking lane that is set back from the main roadway and make the road a scenic byway (like the done along East End Road)</Question1>

</form1>

```
<?xml version="1.0" encoding="UTF-8"?>
```

```
<form1>
```

```
<TextField3/>
```

```
<TextField4>Homer, Alaska 99603</TextField4>
```

```
<Answer>Downtown Homer</Answer>
```

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<Answer/>
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<Answer/>
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<Answer/>
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```
<Answer>Downtown Homer</Answer>
```

```
<Answer/>
```

```
<Answer>Never</Answer>
```

```
<Answer>Infrequently; maybe every few months</Answer>
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```
<Answer/>
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```
<Answer>Other</Answer>
```

```
<TextField1/>
```

```
<TextField1>Occasionally to get from East End Road to the Spit or vice versa, or to Ocean Drive</TextField1>
```

```
<TextField2>Anne Marie Holen</TextField2>
```

```
<Question11>I would love to see traffic slow down but I don't think this is a realistic option.
```

Narrowing the lanes would help (with visual cues). Scenic Byway status requires an application to Alaska DOT. I have never heard of a seasonal Byway designation. I doubt such a short section would be granted Scenic Byway designation. (I know something about this topic.)</Question11>

```
<Question10>Clearly the best option in terms of non-motorized user experience. Undoubtedly more expensive. However, I think we need to quit thinking of non-motorized transportation infrastructure as "optional." For years, handicapped access to buildings was thought of as "optional" until enough pressure was put on public officials to make it mandatory. It should be the same with non-motorized transportation, for many reasons.</Question10>
```

```
<Question5>Not as good/safe/pleasant as a separated trail, but better than current situation.</Question5>
```

```
<Question3>I support all trail development in the Homer area, including this proposal.</Question3>
```

```
<Question1>It should have a separated bike/walking/running trail along the entire length.</Question1>
```

```
<Question2>not sure</Question2>
```

```
<Question13>not sure</Question13>
```

```
<Question6>I am glad that the Parks and Rec Commission has not let this issue (biking/pedestrian pathway) die. It would be a shame if that happened.</Question6>
```

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<RadioButtonList>1</RadioButtonList>
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<RadioButtonList/>
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<RadioButtonList/>
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<RadioButtonList>1</RadioButtonList>
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<RadioButtonList>1</RadioButtonList>
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<RadioButtonList/>
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<RadioButtonList/>
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<RadioButtonList/>
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<RadioButtonList>1</RadioButtonList>

<Question1>Narrowing the lanes and creating bike/walking shoulders of a different color (this is important) would be easy and affordable but not as effective as a separated pathway.</Question1>

<Question1>Kachemak Drive is a State road, not a City road. The City needs to advocate strongly for improvements via the STIP (Statewide Transportation Improvement Plan) process.</Question1>

</form1>



PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

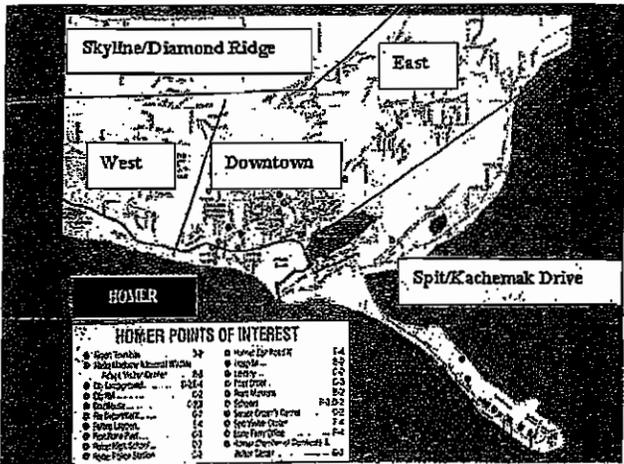
PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Address: (Optional)
City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not ride a Bike
- Other

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

If you could re-allocate space on the road for the following user groups, circle the action you would take:

CARS	PEDESTRIANS	CYCLISTS	OTHER VEHICLES
<input checked="" type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input checked="" type="radio"/> Same Road Space
<input type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input type="radio"/> More Road Space
<input type="radio"/> Less Road Space			

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

This doesn't make sense...
Why would anyone just want to
Bike to the airport.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

Yes, concern is road is
small to begin w/

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

Possibly depending on how
Far.

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

No! Will just create more
traffic.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

No - the shoulder needs to
be paved at least!

13. What do you feel are the most pressing issues facing Kachemak Drive?

No Bike lane, or at least No
paved shoulder

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

Bike commuters are more
common - also tourist bike trips
create hazard in summer - need safe path
for the loop

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

Much the same but w/
Bike path.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

Paving shoulders

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

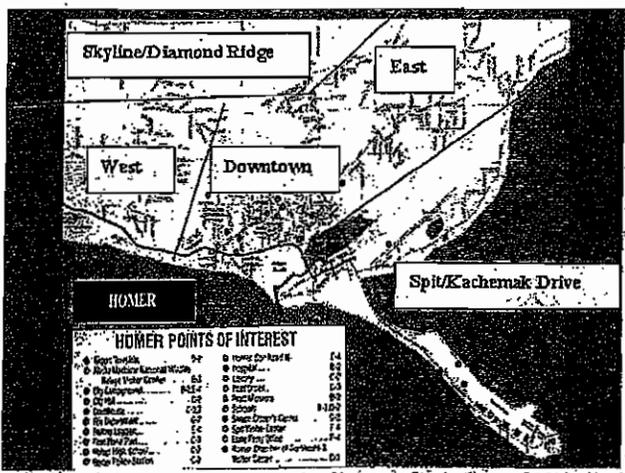
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Tom & Sandy Early

Address (Optional) _____

City, State, Zip _____



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
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- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

*running
several
times
per
week*

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other _____

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

go to boat harbor, athletic club, Starvin Marvin's, beach

If you could re-allocate space on the road for the following user groups, circle the action you would take:

CARS	PEDESTRIANS	CYCLISTS	OTHER VEHICLES
<input type="checkbox"/> Same Road Space	<input type="checkbox"/> Same Road Space	<input type="checkbox"/> Same Road Space	<input type="checkbox"/> Same Road Space
<input type="checkbox"/> More Road Space	<input checked="" type="checkbox"/> More Road Space	<input checked="" type="checkbox"/> More Road Space	<input type="checkbox"/> More Road Space
<input checked="" type="checkbox"/> Less Road Space	<input type="checkbox"/> Less Road Space	<input type="checkbox"/> Less Road Space	<input type="checkbox"/> Less Road Space

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

would be nice as continuation of spit bike trail - Homer needs to add bike paths whenever possible

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

The road is not very wide now - but this would be better than nothing

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

Yes - this would be best

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

No - drivers don't usually observe the speed limit now - but would be better than nothing also

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

Nope - needs something

13. What do you feel are the most pressing issues facing Kachemak Drive?

more use - needs to be upgraded

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

More bikers, runners - they need safer accommodations

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

Wider, with room for everyone

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

Bike/pedestrian path

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Submit by Email

Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

JUL 19 2011 AM 08:54



PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

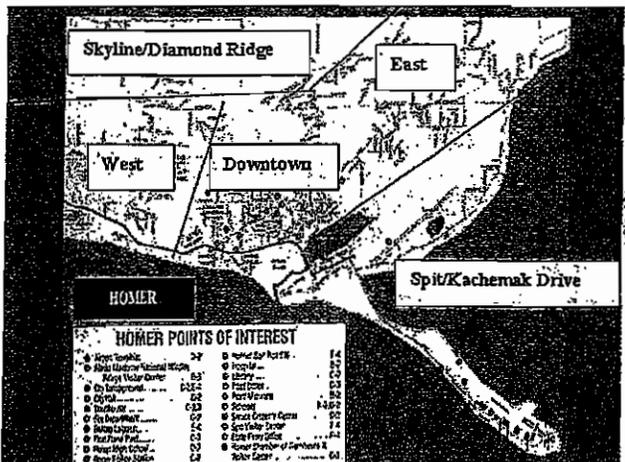
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Bruce Bezon

Address (Optional) 304 W. Pioneer

City, State, Zip 99603



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike

Other Bird watching

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|---|---|--|
| <input type="checkbox"/> Same Road Space | <input type="checkbox"/> Same Road Space | <input type="checkbox"/> Same Road Space | <input type="checkbox"/> Same Road Space |
| <input type="checkbox"/> More Road Space | <input checked="" type="checkbox"/> More Road Space | <input checked="" type="checkbox"/> More Road Space | <input type="checkbox"/> More Road Space |
| <input type="checkbox"/> Less Road Space | <input type="checkbox"/> Less Road Space | <input type="checkbox"/> Less Road Space | <input type="checkbox"/> Less Road Space |

3. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

Yeah right!

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

a bike path

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

more cyclists, & also more car traffic w/ little space. Bad mix!

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

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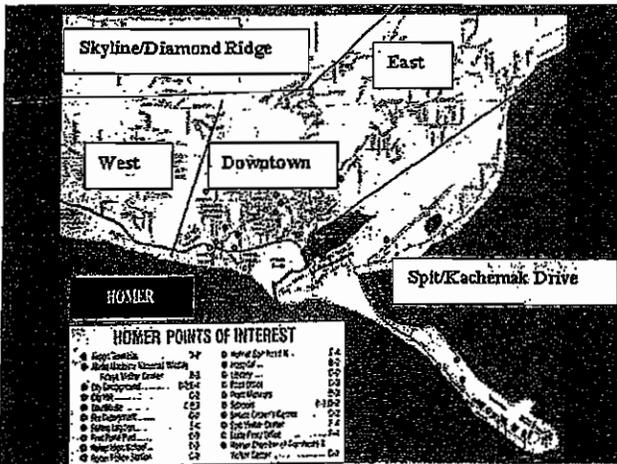


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Name (Optional) Address (Optional) PO Box 1757
City, State, Zip HOMER, AK 99603



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
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- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
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- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- *CURRENTLY TOO DANGEROUS*
- Never
 - Infrequently; maybe every few months
 - Occasionally, about once or twice a month
 - Regularly, once or twice a week
 - Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
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6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

If you could re-allocate space on the road for the following user groups, circle the action you would take:

CARS	PEDESTRIANS	CYCLISTS	OTHER VEHICLES
<input checked="" type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space
<input type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input type="radio"/> More Road Space
<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input checked="" type="radio"/> Less Road Space

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?
YES, FOR PEDESTRIAN/BYCECLE ACCESS TO LOCAL/CHARTER AIR SERVICES.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?
MAYBE, NOT CERTAIN IT WOULD IMPROVE SAFETY FOR ANYONE.

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?
YES, A PEDESTRIAN/BYCLE PATH IS NEEDED TO COMPLETE THE LOOP BY TIEING EAST END RD TO SPIIT TRAILS.

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?
YES, WOULD IMPROVE SAFETY FOR ALL USERS.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?
NO

13. What do you feel are the most pressing issues facing Kachemak Drive?
SAFETY & DRAINAGE.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?
MORE & FASTER TRAFFIC, ESPECIALLY IN SUMMER - BOAT/TRAILOR TRAFFIC TO BOAT YARD + RVs.

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
MORE LIKE PORTION OF EAST END ROAD THAT HAS BIKE PATH.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?
?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.
SEPARATED BIKE/PEDESTRIAN LANE

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<input type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input type="radio"/> More Road Space
<input checked="" type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input checked="" type="radio"/> Less Road Space

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?
 Is this the beach trail option? This is a good pedestrian option, but only mountain bikers with lots of time would ~~and~~ cycle on this trail. I would strongly support a paved or crushed gravel surface parallel to the road - this is a very dangerous section as cars are going fast with limited visibility over the hill, and uphill bike riders may be going very slow and weaving a bit.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?
 I strongly support this option as an immediate, low cost solution where the right of way and topography permit.

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?
 This is an excellent long term solution. Act fast, the easements on the west end are dug up and construction equipment is on site!

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?
 I support this if it could be enforced. Making 9.5-10 lanes would help slow down traffic.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?
 I travel this road frequently, both on a bike and by car. There is often a lot of car / truck traffic and they are going too fast. I often see 4-8 bicyclists on my trips along Kachemak Drive. I can not support no action.

13. What do you feel are the most pressing issues facing Kachemak Drive?
 Safety, safety, and safety.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?
 With the rising price of gas, more people are riding bikes.

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
 I would like to see a separate, paved bike and pedestrian trail, similar to the spit trail. I would discourage an adjacent trail where possible, as the proximity of speeding cars and trucks detracts from a pleasant commuting / traveling experience.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?
 Take advantage of the current construction activity at the west end of Kachemak Drive to eliminate mobe - demobe costs. Repaint the lines to allow up to 3' on each side for a paved shoulder for bikes and walkers.

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.
 Acquire necessary easements for trails under the power lines and over the sewer and water lines.

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