



# AGENDA

## ADA Advisory Board Regular Meeting

Thursday, May 11, 2023 at 5:00 PM

City Hall Cowles Council Chambers In-Person & Via Zoom Webinar

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### Homer City Hall

491 E. Pioneer Avenue

Homer, Alaska 99603

[www.cityofhomer-ak.gov](http://www.cityofhomer-ak.gov)

### Zoom Webinar ID: 998 6324 0301 Password 404451

<https://cityofhomer.zoom.us>

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or 877-853-5247

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1. **CALL TO ORDER, 5:00 P.M.**
2. **AGENDA APPROVAL**
3. **PUBLIC COMMENTS ON ITEMS ALREADY ON THE AGENDA** (3 Minute Time limit)
4. **RECONSIDERATION**
5. **APPROVAL OF THE MINUTES**
  5. A. Unapproved Meeting Minutes  
Unapproved Regular Meeting Minutes for April 13, 2023 **page 3**
6. **VISITORS/PRESENTATIONS**
7. **STAFF & COUNCIL REPORT(S)/COMMITTEE REPORT(S)**
  7. A. ADA Coordinator Report ADA 23-009 **page 7**
8. **PUBLIC HEARING(S)**
9. **PENDING BUSINESS**
  - 9.A. Status of Advocacy Efforts for ADA Compliance in Homer
10. **NEW BUSINESS**
  10. A. Developing a Strategic Plan & Goals AIR ADA 23-010 **page 9**
  10. B. Scheduling Site Visits for City Trails Transition Plan **page 12**
11. **INFORMATIONAL MATERIALS**
  11. A. ADA Annual Calendar 2023 **page 125**
  11. B. City Manager's Report  
CM Report for April 24, 2023 City Council Meeting **page 126**  
CM Report for May 8, 2023 City Council Meeting **page 135**

- 12. COMMENTS OF THE AUDIENCE**
- 13. COMMENTS OF THE STAFF**
- 14. COMMENTS OF THE COMMISSION**
- 15. ADJOURNMENT**

The next regular meeting is Thursday, June 8, 2023 at 5:00 p.m. All meetings are scheduled to be held in City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska, 99603 and via Zoom webinar.

Session 23-02 a Regular Meeting of the ADA Advisory Board was called to order by Chair Donna Aderhold at 5:00 p.m. on April 14, 2023, from the Cowles Council Chambers, City Hall, located at 491 E. Pioneer Avenue, Homer, Alaska and via Zoom webinar.

**1. CALL TO ORDER, 5:00 P.M.**

**PRESENT:** BOARD MEMBERS ADERHOLD, GEISLER, DEADRICK, PARSONS, SAFRA AND THORSRUD

**STAFF:** ADA COORDINATOR KRAUSE, PUBLIC WORKS DIRECTOR KEISER, CITY CLERK JACOBSEN

**2. AGENDA APPROVAL**

GEISLER/SAFRA MOVED TO APPROVE THE AGENDA

There was no discussion.

VOTE: NON OBJECTION: UNANIMOUS CONSENT

Motion carried.

**3. PUBLIC COMMENTS UPON MATTERS ALREADY ON THE AGENDA (3 minute time limit)**

**4. RECONSIDERATION**

**5. APPROVAL OF MINUTES**

5. A. Unapproved Meeting Minutes

Unapproved Meeting Minutes for February 9, 2023

GEISLER/SAFRA MOVED TO APPROVE THE MINUTES FROM FEBRUARY 9<sup>TH</sup>.

Board Member Safra noted reference to her comment “the demographic that makes up the majority” if she said that it was an error, and it should have been “a significant number” (page 5 first bullet point).

Board Member Geisler noted page 3, 4<sup>th</sup> bullet needs the word parking added; and page 4 2<sup>nd</sup> to last bullet there is a reference Bunnell Street Art Gallery having an ADA accessible restroom, she noted it isn’t ADA Accessible.

Mr. Parson’s clarified he did say that, but later discovered it was not.

City Clerk Jacobsen explained staff will review those instances on the recording and amend or include clerk’s notes as needed.

VOTE: NON OBJECTION: UNANIMOUS CONSENT.

Motion carried.

**6. VISITORS/PRESENTATIONS**

**7. STAFF & COUNCIL REPORT/COMMITTEE REPORTS**

7. A. ADA Coordinator Report ADA 23-008

ADA Coordinator Krause provided summary of Staff Report ADA 23-008, explaining that City Manager Dumouchel had approved distribution of the survey.

The group discussed and agreed to have the survey open from April 15 to September 15, 2023 to capture the tourist season, and identified locations for paper copies.

7. B. Public Works Report

**8. PUBLIC HEARING(S)**

**9. PENDING BUSINESS**

9. A. Parks Transition Plan - Status Update

Memorandum ADA 23-007

ADA Coordinator Krause reviewed Memorandum ADA 23-007 and advised that the Parks Transition Plan should be complete by the next meeting.

Board Member Geisler shared the first few pages provide good background for readers to understand. She noted Ben Walters Park and WKFL are not included. Ms. Krause said she'll follow up with Project Technician Meyer on those locations.

Chair Aderhold noted wordsmithing items and requested they be clarified. She also noted the mission statement and that it should be included on all of their documents.

9. B. Status of Advocacy Efforts for ADA Compliance in Homer

Chair Aderhold opened the floor for status updates. She reported that she has not met with KBBI.

Board Member Safra reported she broached the subject with the Executive Director who referred her to Visitor Center Coordinator, who she'll contact. Ms. Safra noted they didn't seem to feel there was a problem. She hasn't talked to anyone with Economic Development yet.

Board Member Parsons shared regarding recent ADA National Network trainings in a live seminar form that are also recorded. It doesn't bode well for attendance at their meetings, but there might be resources he could share out to the Board. He echoed his statements from the last meeting and recognized there are community members that are already doing a lot of this advocacy work. He doesn't think they should be reaching out to some of these groups without talking to some folks who are already investing time into it.

Board Member Thorsrud said she's stepping back from contacting Homer News, for the reasons Mr. Parson just shared. She noted the Pioneer Avenue entrance to the college that has the trip step issue that she's mentioned to them before. At an informal staff meeting the college she informally mentioned the ADA will be taking a look at it.

Chair Aderhold requested this item be included on their next agenda to see where things are at.

9. C. ADA Compliance – Budget Requests

Memorandum ADA 23-006



ADA Coordinator Krause reviewed the memorandum included in their packet and addressed the quote for the purchase of the Mobi-mats. Board Member Geisler said shared an experience with getting the mats delivered to Kodiak and that she'd provide Ms. Krause shipping information.

There was brief discussion that they would address storage and access to the mats at a future meeting.

THORSRUD/SAFRA MOVED TO SUPPORT THE MOBI-MAT PURCHASE.

There was no discussion.

VOTE: NON OBJECTION: UNANIMOUS CONSENT.

Motion carried.

Public Works Director Keiser arrived at the meeting at 5:26 p.m. and Chair Aderhold asked for a motion to suspend the rules to hear her report.

GEISLER/SAFRA MOVED TO SUSPEND THE RULES TO HEAR THE PUBLIC WORKS DIRECTOR'S REPORT.

There was no discussion.

VOTE: NON OBJECTION: UNANIMOUS CONSENT

Motion carried.

Public Works Director Keiser reported there is funding available to refurbish Bayview Park and there will be opportunity for public input on the improvements, the City has received Fire Marshall approval to fix the ramp at City Hall so that will move forward when the ground thaws, and that the conceptual designs for the accessible bathroom at the airport have been developed and funds are being recommended to implement that. She also shared that Ben Walters sidewalk design is nearly complete and we're working towards funding to build this year. Presentation of the Master Transportation Plan has been given to the various commissions, this plan has a strong emphasis on non-motorized transportation; the final draft is expected next week and will be circulated around with updated presentations. She commented regarding the plan for renovation of Heath Street and the work being done with the project with a goal of starting work this year. She thanked Board Member Parsons for his work in drafting sections of the text they'll be working with as part of the Transportation Plan.

## **10. NEW BUSINESS**

### **11. INFORMATIONAL MATERIALS**

11. A. City Manager's Report

CM Reports for Council Meeting March 28, 2023 and April 10, 2023

11. B. 2023 Annual Calendar

The Board discussed scheduling for summer site visits for City Trail Transition Plan and it was requested that scheduling be included on their May agenda. Public Works Director Keiser noted Matt Steffy's transition from Public Works to City Hall and that he'll be working with Economic Development.

Board Member Geisler asked about the process for combining the different sections of the Transition Plan into one document and when they'll be seeing an update of the facilities plan. ADA Coordinator Krause explained she's been in touch with the Public Works Superintendent and hopes to meet with him soon for a facilities update. She added that once the Parks Transition Plan is complete it will be submitted to Council for adoption, and she'll be working with Economic Development Manager Engebretsen and Associate Planner Steffy on determining which trails to survey. Once all the plans are approved by Council, they can be combined into one document.

**12. COMMENTS OF THE AUDIENCE (3 minute time limit)**

**13. COMMENTS OF THE CITY STAFF**

ADA Coordinator Krause said it was nice to be at the table today, it was a good meeting.

Public Works Director had no comments.

City Clerk Jacobsen said it was nice to see everyone tonight.

**14. COMMENTS OF THE BOARD**

Board Member Parsons said kudos for a good meeting. It's great to have Public Works Director Keiser there to provide concise information. As we think about the trails portion of the ADA, there are recreational trails and there's pedestrian corridors, and most of our trails are pedestrian corridors. He hopes they'll use community members who are knowledgeable on where the unofficial trails of Homer are. It may be more of a path of travel discussion.

Board Member Deadrick thanked everyone for a great meeting. Matt Steffy will be missed but she trusts going forward everything will get into place.

Board Member Geisler agreed it was a great meeting and thanked ADA Coordinator Krause.

Board Member Safra commented it was a good meeting, this was a good use of their time and they're moving in the right direction.

Board Member Thorsrud thanked ADA Coordinator Krause and thanked everyone, it's always a pleasure.

Chair Aderhold said it's nice to see progress on the Parks and Campground Transition Plan. She thanked everyone for a good meeting.

**11. ADJOURNMENT**

There being no further business to come before the Board Chair Aderhold adjourned the meeting at 5:54 p.m. The next Regular Meeting is Thursday, May 11, 2023 at 5:00 p.m. All meetings are scheduled to be held in the City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska and via Zoom Webinar.

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Melissa Jacobsen, MMC, City Clerk

Approved: \_\_\_\_\_



# REPORT

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**Prepared For:** ADA ADVISORY BOARD  
**Meeting Date:** MAY 11, 2023  
**Staff Contact:** RENEE KRAUSE, ADA COORDINATOR  
**Subject:** ADA COORDINATOR FOR MAY

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## Changes in City Clerk's Office

Rachel Tussey submitted her resignation effective April 21, 2023 so the City Clerk's Office is short staffed. The tasks and work did not slow down however, and as such I was not able to focus on the completion of the draft parks, play areas & campground transition plan. I plan to keep plugging away at the document as my schedule allows. Until we hire an additional clerk, my available time will be tight. I will be leaving for a training opportunity in Minneapolis tonight right after the meeting and will be out of the office for a week. I will be taking on clerk duties for the other board and commissions until the Clerk's Office is fully staffed and the new Clerk is on their feet. Then the City Clerk has scheduled some well-deserved time away from the office the end of June and first part of July.

I do endeavor to complete this document so that we can get it before City Council by the end of summer.

## Recent Actions Before or Approved by City Council

Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska, Amending the FY23 Capital Budget by Authorizing a Transfer of \$500,000 from Homer Accelerated Roads and Trails (HART) Roads to Establish a HART Roads Match Reserves. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska, Amending the FY23 Capital Budget by Authorizing a Transfer of \$500,000 from Homer Accelerated Roads and Trails (HART) Roads to Establish a Non-Motorized Transportation Opportunity Program within HART Roads. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska, Amending the FY23 Capital Budget by Authorizing a Transfer of \$100,000 from Homer Accelerated Roads and Trails (HART) Roads to Establish a Small Works Roads Maintenance Program within HART Roads. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska, Amending the FY23 Capital Budget by Authorizing a Transfer of \$20,000 from Homer Accelerated Roads and Trails (HART) Roads

to Establish a Small Works Pest Management Program within HART Roads. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska, Amending the FY23 Capital Budget by Authorizing a Transfer of \$100,000 from Homer Accelerated Roads and Trails (HART) Trails to Establish a Non-Motorized Transportation Opportunity Program within HART Trails. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

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Ordinance 23-xx, An Ordinance of the City Council of Homer, Alaska Amending Homer City Code 2.58 Board and Commissions Section 2.58.060 Teleconferencing. Erickson. Recommend Introduction May 8, 2023 Public Hearing and Second Reading May 22, 2023.

Resolution 23-0xx, A Resolution of the City Council of Homer, Alaska Establishing a Policy of Funding Improvements to Various Recreational Facilities within Homer. Mayor.

### **Future Agenda Items or Considerations**

I am working on getting a member of the Parks Commission to participate in the site visits to area trails. An invitation has been extended to the Commissioners to attend this meeting or the June meeting or to send me notification that they would like to participate. I also included Owen Meyer, PW Project Technician to have him participate as well in the site visits.

### **Report to Council**

One of the responsibilities of being on the Advisory Body is to make reports to City Council. Currently Chair Aderhold has been making these reports but since we now have a full Board I would like to start having each member volunteer to provide a report to Council at least once per month. Reports are encouraged to be given in person but attendance at the meeting can be done via Zoom, or in written format. The report to Council contains information on any actions the advisory body is taking or recommending and reports on project progress such as the transition plan for parks, play areas, and campgrounds. It takes approximately 5 minutes. Following are the dates for the upcoming Council meetings and it would be great to line up a volunteer for them ahead of time but certainly can be one month at a time.

Monday, May 22nd 6:00 p.m. \_\_\_\_\_

Monday, June 12<sup>th</sup> 6:00 p.m. \_\_\_\_\_ or June 26<sup>th</sup> 6:00 p.m. \_\_\_\_\_

Monday, July 24<sup>th</sup> 6:00 p.m. \_\_\_\_\_

Monday, August 14<sup>th</sup> 6:00 p.m. \_\_\_\_\_ or August 28<sup>th</sup> 6:00 p.m. \_\_\_\_\_



# MEMORANDUM

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**Prepared For:** ADA ADVISORY BOARD  
**Meeting Date:** MAY 11, 2023  
**Staff Contact:** RENEE KRAUSE, ADA COORDINATOR  
**Subject:** DEVELOPING A STRATEGIC PLAN & GOALS

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City Council was scheduling a joint worksession meeting with the Advisory Bodies but the City Attorney stated that a quorum of each body would be required in order to conduct that meeting. They are now meeting with Advisory Bodies individually as schedules allow. It was also mentioned that City Council would be reviewing each Advisory Body's Strategic Plans & Goals. This made me realize that it would be beneficial for the ADA Advisory Board to have such a document. This will be a living document and act as a guide post for the Board providing direction for the members and staff throughout the year.

Staff recognizes the various ways a strategic plan can be written; the main purpose of having one is to establish the direction of an organization. Yet in the Board's case, many documents detailing out the actions needed to achieve these goals already exist and do not need to be re-created. Additionally, the ADA is an advisory board to the City, not a company or stand-alone organization. For the purpose of aiding the direction of this Board, staff recommends utilizing already adopted plans/policies to create a simple, concise document (a 2-3 page list) that breaks down the Board's goals into ongoing, short-term, and long-term groups. This document would be decided on/reviewed annually by the Board and maintained/provided by the City Clerk's Office (similar to how other commissions/boards' strategic plans/goals are handled).

The existing documents staff recommends pulling from include:

- ADA Advisory Board Bylaws – Article II: Purpose
- City of Homer Comprehensive Plan - [City of Homer 2018 Comprehensive Plan](#)
- Adopted Transition Plan(s)

## RECOMMENDATION

Review the attached plans/policies and sample draft for the Board's Strategic Plan; be prepared to make change suggestions. Any action for approval must be made by way of motion.

**ADA ADVISORY BOARD (ADAAB)**  
**2023-2024 STRATEGIC PLAN & GOALS**  
First Draft 05 11 2023

**MISSION STATEMENT**

The ADA Advisory Board shall act in an advisory capacity to the City Manager and City Council concerning Title II Regulations of the Americans with Disabilities Act within the borders of the City of Homer which covers programs, activities, and services of public entities and its facilities pursuant to the powers and duties in Homer City Code 2.70.040.

**VALUES, ROLES & RESPONSIBILITIES**

<b>General Administrative</b>	Advise the City Manager and the City Council on necessary steps to ensure city facilities, programs and services are compliant with Title II of the ADA.
<b>Policy</b>	Work with the ADA Coordinator to establish written policies to effect and maintain accessibility for and in City facilities, programs, and services to include but not limited to parks, play areas, recreational programs, trails and campgrounds for approval by the City Council.
<b>Planning</b>	Ensure that the ADA Transition Plan(s) have implementation and evaluation components.
<b>Fiscal</b>	Advocate for an annual budget line item for ADA Compliance in the City Budget and assist in the preparation and presentation of budget requests in accordance with city code.
<b>Advocacy</b>	Promote the mission of the Board within the community. Advocate for ADA Compliance with legislators and local businesses.
<b>Meetings</b>	Participate in all advisory board meetings. Report on ADAAB and ADAAB activities to City Council once each month at their meetings.

**GOALS**

**1. Advocate for ADA Compliance Budget**

- a. Ensure the budget is sufficient to address compliance issues outlined in the adopted transition plan(s) on an acceptable progress level that expresses the commitment Council made by adoption of Resolution 17-075(A) to become a universally accessible city to all and to maintain compliance.
- b. Review needs for physical upkeep and efficiencies.
- c. Promote future funding of ADA needs.
  - i. (List capital projects that may require large sums to complete)

**2. Explore opportunities to increase and improve service to all community members, especially students and underrepresented user-groups.**

- a. Work with various department representatives and the ADA Coordinator to develop and include programs, with schedules that aid with completing non-compliant repairs and or providing accessible programming.

**3. Actively advocate for ADA compliance with local businesses, programs, and services.**

**DUTIES OF THE BOARD & STAFF**

Boardmembers

- Attend City Council meetings as assigned.
- Attend worksessions and training opportunities.
- Come to meetings prepared to take action through motions, or ask staff before the meeting for more information.
- Communicate with City Council members to gain support on ADA-related projects that need sponsorship at the Council level.

Staff Liaison

- Aid the Chair in setting the agenda.
- Compile packet material from boardmembers, write backup memos, and submit to City Manager.
- Provide information on items the board needs to review annually.
- Inform the board of City Council actions and discussion of ADA-related issues.

Clerk's Office

- Track yearly items such as reappointments and election of officers.
- Advertise any seat vacancies and process new appointment applications.
- Help the board learn to be more efficient and effective in their meetings.
- Support the board's ability to communicate with the Mayor and City Council (through Memorandums, Resolutions, and Ordinances).



# MEMORANDUM

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**Prepared For:** ADA ADVISORY BOARD  
**Meeting Date:** MAY 11, 2023  
**Staff Contact:** RENEE KRAUSE, ADA COORDINATOR  
**Subject:** SCHEDULING SITE AUDITS FOR CITY TRAILS

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The City has a number of known and unknown trails and walking paths or shortcuts that are used by the residents of Homer. Many of these trails are not maintained City facilities and as such are not mandated to be ADA Compliant. There are also trails that are located within right of ways that are actually owned by the State or federal governments but are maintained by the City and are considered Pedestrian trails or even a multi-modal trail. These should be and for the most part are ADA Compliant but are in need of better maintenance.

Below are trails that are being mentioned in the Transportation Plan draft:

Homer Spit Trail  
Beluga Slough Trail  
Poopdeck  
Library Trail  
Lucky Shot

Economic Development Manager Engebretsen has recommended waiting until after the final draft of the Transportation Plan so that work can be coordinated and be in tandem.

Other City Trails:

Calhoun  
Fairview  
Reber  
Calvin & Coyle  
Diamond Creek  
Homestead

## RECOMMENDATION

Determine which trails would be beneficial to the Community as whole to be ADA Compliant. Such as Reber Trail due to steep slope would be an exception.

Schedule site audit visits in accordance with available schedules of staff, Boardmembers and additional participants.





**UNITED STATES ACCESS BOARD**

**trails    camping & picnic facilities    viewing areas    beach access**

# **outdoor developed areas**

**a summary of  
accessibility standards  
for Federal outdoor developed areas**

**MAY 2014**

**ADVANCING FULL ACCESS AND INCLUSION FOR ALL**

Cover photo: J. Strom Thurmond Lake, courtesy of U.S. Army Corps of Engineers.

Reproduction and dissemination of this publication is encouraged.

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## Acknowledgments

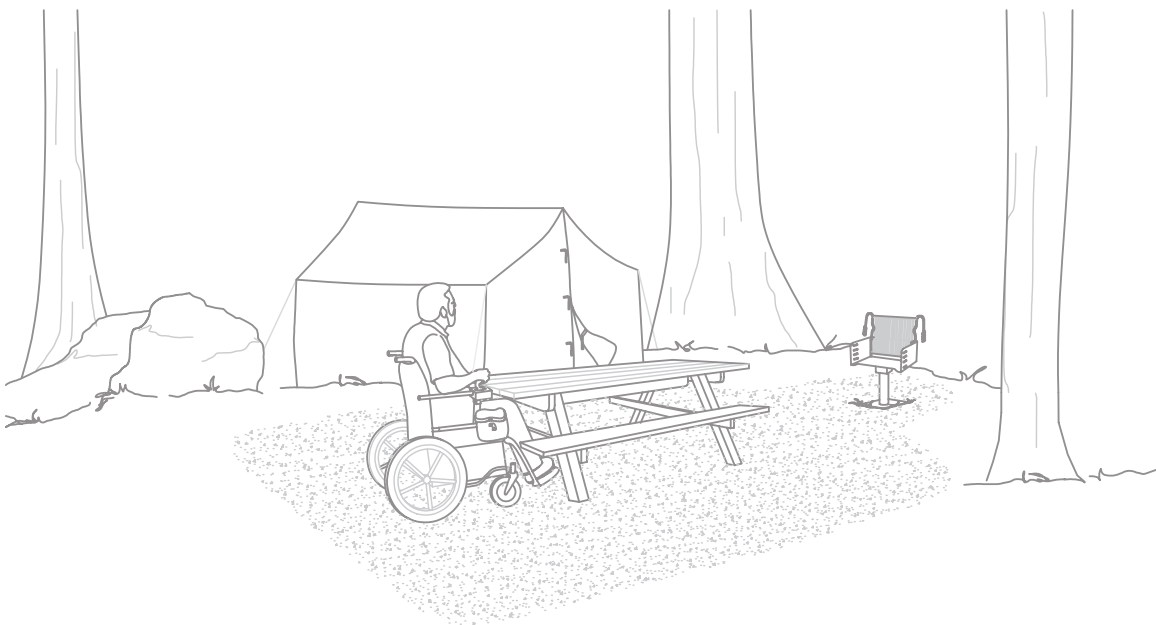
The Architectural and Transportation Barriers Compliance Board (Access Board) developed the accessibility guidelines for trails, camping and picnic facilities, viewing areas, and beach access routes through extensive public input from the disability community, Federal land management agencies, and other interested individuals and organizations.

This information has been developed and reviewed in accordance with the Access Board's information quality guidelines [www.access-board.gov/the-board/policies/information-quality](http://www.access-board.gov/the-board/policies/information-quality).



## Introduction

The Access Board is responsible for developing accessibility guidelines for the construction and alteration of facilities covered by the Americans with Disabilities Act (ADA) of 1990 and the Architectural Barriers Act (ABA) of 1968. The guidelines ensure that the facilities are readily accessible to and usable by people with disabilities. The Access Board issued the current guidelines in 2004. The 2004 guidelines contain provisions for several types of recreation facilities, including boating facilities, fishing piers and platforms, golf facilities, play areas, sports facilities, and swimming pools. The Access Board amended the 2004 guidelines in 2013 by adding new provisions for trails, picnic and camping facilities, viewing areas, and beach access routes constructed or altered by Federal agencies or by non-federal entities on Federal land on behalf of a Federal agency pursuant to a concession contract, partnership agreement, or similar arrangement.



The U.S. Department of Defense, the U.S. General Services Administration, and the U.S. Postal Service have adopted the Access Board's 2004 guidelines, including the 2013 amendments, as the enforceable standards for the ABA. The ABA standards adopted by these agencies are available on the Access Board's Web site at [www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-aba-standards/aba-standards](http://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-aba-standards/aba-standards). The ABA Standards are comprised of two types of provisions: scoping requirements and technical requirements. The scoping requirements specify when and where elements must be accessible. The scoping requirements also specify the number of a particular type of element that must be accessible when more than one is provided. The scoping requirements are in chapter 2 of the ABA Standards and the section numbers are preceded by the capital letter "F." The technical requirements specify the design criteria for individual elements. The technical requirements for trails, picnic and camping facilities, viewing areas, and beach access routes are in chapter 10 of the ABA Standards. The scoping and technical requirements are minimum requirements. Designers, owners, and operators are encouraged, but are not required, to exceed the minimum requirements where possible to provide increased accessibility and opportunities for people with disabilities to enjoy trails and other outdoor developed areas.

This guide is intended to help designers, owners, and operators understand and use the ABA Standards for trails, picnic and camping facilities, viewing areas, and beach access routes. Guides for other recreation facilities, including boating facilities, fishing piers and platforms, golf facilities, play areas,

sports facilities, and swimming pools, are available on the Access Board's Web site at [www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/guide-to-the-ada-standards](http://www.access-board.gov/guidelines-and-standards/buildings-and-sites/about-the-ada-standards/guide-to-the-ada-standards). The Access Board is also developing guides for toilet and bathing facilities, parking facilities, and other elements and spaces that are required to be accessible by the ABA Standards. These guides will be available on the Access Board's Web site at [www.access-board.gov/guidelines-and-standards/recreation-facilities](http://www.access-board.gov/guidelines-and-standards/recreation-facilities) as they are completed.

Incorporating accessibility into the design of outdoor developed areas must begin early in the planning process, with careful consideration given to the location of accessible elements and the routes that connect them. Emphasis must be placed on ensuring that people with disabilities are able to access these unique facilities and use a variety of elements that serve these facilities.

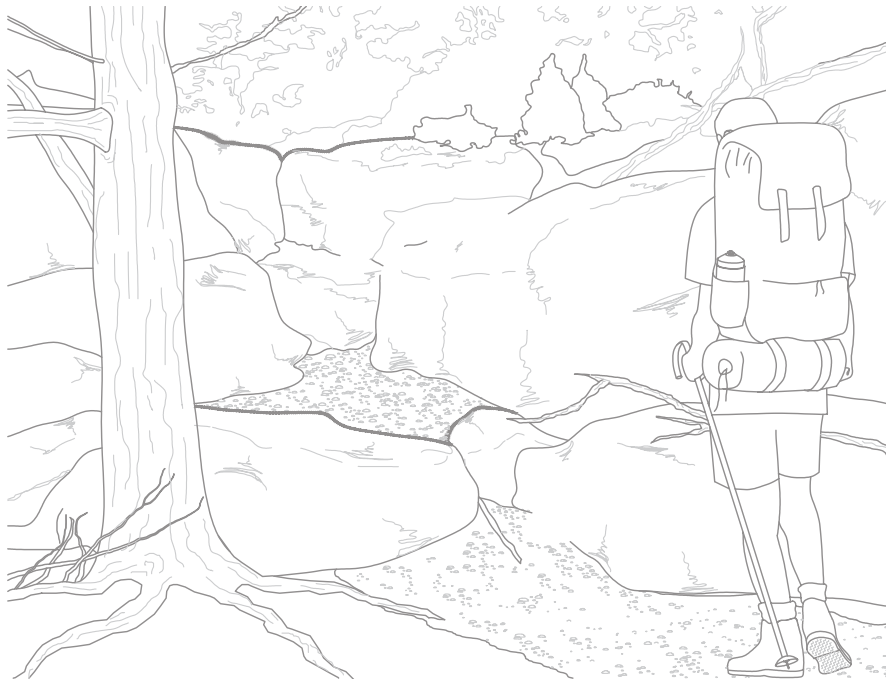
The new provisions for trails, picnic and camping facilities, viewing areas, and beach access routes are not included in the Department of Justice's (DOJ) 2010 ADA Standards and have no legal effect on State and local governments and private entities subject to DOJ's ADA regulations. State and local governments and private entities may, however, use the provisions for guidance when designing trails, picnic and camping facilities, viewing areas, and beach access routes. State and local governments and private entities are cautioned to check with DOJ about using the technical requirements for outdoor recreation access routes, instead of accessible routes, to connect elements at picnic and camping facilities, viewing areas, and trailheads.

## Conditions for Exceptions [1019]

The ABA Standards recognize the existence of constraints and limitations in the outdoor environment and allow for exceptions from specific provisions in the technical requirements where certain circumstances, referred to as “conditions for exceptions,” apply. When an entity determines that any of the conditions for exceptions do not permit full compliance with a specific provision in the technical requirements, compliance with that provision is required to the extent practicable. The phrase “to the extent practicable” means reasonably doable under the circumstances.

The conditions for exceptions should be used only after all other design options are thoroughly explored. Where a condition for exception applies to only part of a trail, a beach access route, or an outdoor recreation access route, the rest of the trail or route must comply with all the technical requirements for the trail or route.

The following sections describe the four conditions for exceptions and provide examples of situations where they might apply.



## **Condition for Exception 1**

Compliance is not practicable due to terrain. The phrase “not practicable” means not reasonably doable.

For example, where a trail is constructed in a steeply sloped area, compliance with the running slope provision may not be practicable on parts of the trail where it would require extensive cuts or fills that are difficult to construct and maintain, cause drainage and erosion problems, significantly lengthen the trail, and create other adverse environmental impacts.

## **Condition for Exception 2**

Compliance cannot be accomplished with the prevailing construction practices.

This condition does not require the use of construction equipment or methods other than those typically used in a particular type of setting.

For example, where handtools would normally be used to construct a trail in order to minimize the impact on a sensitive adjacent stream and the prevailing construction practices for this type of setting do not include blasting, blasting does not have to be used to remove a rock outcrop in order to comply with the clear tread width provision. Compliance with the clear tread width provision is required to the extent that it can be accomplished using handtools.

Prevailing construction practices are those used by most contractors or designers faced with the same or similar projects in the area. Preferences or practices used by a single contractor or designer are not necessarily prevailing construction practices.

### **Condition for Exception 3**

Compliance would fundamentally alter the function or purpose of the facility or the setting.

This condition recognizes that public lands provide a wide variety of recreational experiences, from highly developed areas to wilderness areas that appear unchanged from primeval times and provide opportunities for people to experience primitive and challenging conditions. The condition applies where compliance with specific provisions in the technical requirements would fundamentally alter the function or purpose of the facility or the setting.

For example, people using primitive trails experience the outdoor environment in a nearly natural state, with limited or no development. Use of manufactured building materials or engineered construction techniques to comply with specific provisions in the technical requirements for trails could fundamentally alter the natural or undeveloped nature of the setting and change the recreational experience. Trails that are intended to provide a rugged experience, such as a cross-country training trail with a steep grade, a fitness challenge course with abrupt and severe changes in elevation, and a trail that traverses boulders and rock outcroppings to provide users with the opportunity to climb the rocks, are other examples. To remove the obstacles on these trails or to reroute the trails around the obstacles would fundamentally alter the function or purpose of the trails.

## Condition for Exception 4

Compliance is limited or precluded by any of the following laws, or by decisions or opinions issued or agreements executed pursuant to any of the following laws:

- Endangered Species Act (16 U.S.C. §§ 1531 et seq.)
- National Environmental Policy Act (42 U.S.C. §§ 4321 et seq.)
- National Historic Preservation Act (16 U.S.C. §§ 470 et seq.)
- Wilderness Act (16 U.S.C. §§ 1131 et seq.)
- Other Federal, State, or local law, the purpose of which is to preserve threatened or endangered species; the environment; or archaeological, cultural, historical, or other significant natural features

The laws specified in this condition prescribe certain activities or require certain analyses to be prepared or procedures to be followed when planning projects that may impact features protected under the laws. The condition does not require full compliance with a specific provision in the technical requirements where compliance is limited or precluded by the laws, or by decisions or opinions issued or agreements executed pursuant to the laws.

### Example

Situations where the condition may apply include:

- Congressionally designated wilderness areas. The condition applies if work that is necessary to comply with a specific provision in the technical requirements can't be accomplished using handtools, because motorized equipment is prohibited by law in Congressionally designated wilderness areas.
- Designated wetlands or coastal areas where construction methods and materials are limited.
- Tribal sacred sites where the physically undisturbed condition of the land is an important part of the sacred observance.
- Areas where water crossings are restricted to safeguard aquatic features protected under Federal or State laws.

Archaeological and cultural features include burial grounds and cemeteries, protected tribal sites, and other properties considered sacred by an organized religion. Historical features are properties listed or eligible for listing on the National Register of Historic Places or other places of recognized historic value. Significant natural features include objects, such as large boulders, rocky outcrops, and bodies of water; or unique trees or vegetation, such as giant sequoia groves, that are regarded as distinctive or important locally, regionally, or nationally and are therefore placed under legal protection.



Table 1 summarizes the conditions for exceptions and when and where they may apply.

Table 1— Permissible Uses of Conditions for Exceptions		
Technical Requirements	Specific Provisions	Applies To
Outdoor constructed features	Clear ground space	Alterations only
Tent pads and tent platforms	Any specific provision	New construction or alterations
Camp shelters	Any specific provision	New construction or alterations
Outdoor recreation access routes at camping facilities, picnic facilities, and trailheads	Any specific provision	Alterations only
Viewing areas	Any specific provision	Alterations only
Outdoor recreation access routes to viewing areas	Any specific provision	New construction or alterations
Trails	Any specific provision	New construction or alterations
Beach access routes	Any specific provision	New construction or alterations

## Documenting Use of the Conditions for Exceptions on a Portion of a Trail or Beach Access Route [F201.4.1]

When using the conditions for exceptions on a portion of trail or a beach access route, Federal agencies should document in writing why they could not fully comply with a specific provision in the technical requirements for trails or beach access routes. The documentation should be retained with the project records. The documentation should include the date the decision is made and the names and positions of the people making the decision. The Access Board is responsible for investigating complaints alleging violations of the ABA Standards and will request documentation when a complaint involves a trail or beach access route.



## **Exemptions for an Entire Trail or Beach Access Route [1017.1, Exception 2 and 1018.1, Exception 2]**

When extreme or numerous conditions for exceptions make it impractical to construct a trail or beach access route that complies with the technical requirements, the ABA Standards provide an exemption for the entire trail or beach access route. The exemption for an entire trail or beach access route can only be used after applying the conditions for exceptions to portions of the trail or beach access route. When determining whether to exempt an entire trail or beach access route, consider the portions of the trail or beach access route that can and cannot comply with the specific provisions in the technical requirements and the extent of compliance where full compliance cannot be achieved.

## **Notifying the Access Board When an Entire Trail or Beach Access Route is Exempted [F201.4.1]**

In the rare cases where an entire trail or beach access route is exempted, Federal agencies must notify the Access Board about the exemption. Sample notification forms are in the appendix of this guide and are also available on the Access Board's Web site at [www.access-board.gov/guidelines-and-standards/recreation-facilities/outdoor-developed-areas/final-guidelines-for-outdoor-developed-areas/notification-forms](http://www.access-board.gov/guidelines-and-standards/recreation-facilities/outdoor-developed-areas/final-guidelines-for-outdoor-developed-areas/notification-forms). Federal agencies do not have to obtain approval from the Access Board to exempt an entire trail or beach access route. The Access Board will monitor the use of exemptions for an entire trail or beach access route and may provide additional guidance on the use of such exemptions. Federal agencies are encouraged to seek technical assistance from the Access Board when considering exempting an entire trail or beach access route.

## Trails

### Definition [F106.5]

A trail is defined as a pedestrian route developed primarily for outdoor recreational purposes. Pedestrian routes that are developed primarily to connect accessible elements, spaces, and buildings within a site are not a trail.

The Access Board is developing accessibility guidelines for sidewalks and shared-use paths. The key differences between accessible routes, sidewalks, shared-use paths, and trails are outlined in the appendix of this guide.

### New Trails [F247.1]

When a trail is designed for use by hikers or pedestrians and directly connects to a trailhead or another trail that substantially meets the technical requirements for trails, the trail must comply with the technical requirements.

#### Do the Standards Apply?

- Is the trail designed for hiker or pedestrian use?
- Is the trail connected to a trailhead or an existing trail that substantially meets the technical requirements for trails?

The ABA Standards for trails apply when the answer to both questions above is “yes.”

The Federal Trail Data Standards (FTDS) classify trails by their **designed use** and **managed use**. Under the FTDS, a trail has only **one designed use** that determines the design, construction, and maintenance parameters for the trail. A trail can have more than **one managed use** based on a management decision to allow other uses on the trail. Trails that have a **designed use** for hikers or pedestrians are required to comply with the technical requirements for trails. Trails that have a **designed use** for other than hikers or pedestrians, such as mountain bike or equestrian trails, are not required to comply with the technical requirements for trails.

A trail system may include a series of connecting trails. Only trails that directly connect to a trailhead or another trail that substantially meets the technical requirement for trails are required to comply with the technical requirements for trails. A trail that complies with most of the technical requirements for trails is considered to substantially meet the technical requirements.

## Existing Trails [F247.2]

When the original design, function, or purpose of an existing trail is changed, regardless of the reason, and the altered portion of the trail directly connects to a trailhead or another trail that substantially meets the technical requirements for trails, the altered portion of the existing trail must comply with the technical requirements for trails.

The term “**reconstruction**” is not used in the ABA Standards, though the term is used frequently by the trails community. For the purposes of the ABA

Standards, actions are categorized as either new construction or an alteration. Routine or periodic maintenance activities are not considered an alteration that would trigger the application of the ABA standards. The difference between an alteration and maintenance is as follows:

- An **alteration** is work done to change the original design, purpose, intent, or function of an existing trail.
- **Maintenance** is the routine or periodic repair of existing trails or trail segments to restore them to their originally designed and built condition. Maintenance does not change the original design, purpose, intent, or function for which a trail is designed. Maintenance may include:
  - ◆ Removing debris and vegetation, such as fallen trees or broken branches on the trail; clearing the trail of encroaching brush or grasses; and removing rock slides
  - ◆ Maintaining trail tread, such as filling ruts, reshaping a trail bed, repairing a trail surface or washout, installing riprap to retain cut and fill slopes, and constructing retaining walls or cribbing to support trail tread
  - ◆ Performing erosion control and drainage work, such as replacing or installing drainage dips or culverts
  - ◆ Repairing or replacing deteriorated, damaged, or vandalized trail or trailhead structures or parts of structures, including sections of bridges, boardwalks, information kiosks, fencing and railings; painting; and removing graffiti



## Technical Requirements [1017]

The technical requirements for trails include specific provisions for the surface, clear tread width, passing spaces, tread obstacles, openings, running slope, cross slope, resting intervals, protruding objects, and trailhead signs.

### Using the Trail Exceptions [1017.1, Exceptions 1 and 2]

When a condition for exceptions does not permit full compliance with a specific provision in the technical requirements on a portion of a trail, that portion of the trail must comply with the specific provision to the extent practicable.

When extreme or numerous conditions for exceptions make it impracticable to construct a trail that complies with the technical requirements, the entire trail can be exempted from complying with the technical requirements. An entire trail can be exempted from the technical requirements only after applying the conditions for exceptions to portions of the trail. When determining whether to exempt an entire trail from the technical requirements, consider the portions of the trail or beach access route that can and cannot comply with the specific provisions in the technical requirements and the extent of compliance where full compliance cannot be achieved.

Additional information on the conditions for exceptions, including documenting use of the exceptions on portions of a trail and notifying the Access Board when an entire trail is exempted from the technical requirements, is provided in the section of this guide on the conditions for exceptions.

**Surface [1017.2]**

The surfaces of trails, passing spaces, and resting intervals must be firm and stable. A firm trail surface resists deformation by indentations. A stable trail surface is not permanently affected by expected weather conditions and can sustain normal wear and tear from the expected uses between planned maintenances.

Paving with concrete or asphalt may be appropriate for highly developed areas. For less developed areas, crushed stone, fine crusher rejects, packed soil, soil stabilizers, and other natural materials may provide a firm and stable surface. Natural materials also can be combined with synthetic bonding materials to provide greater stability and firmness. These materials may not be suitable for every trail.

**DESIGN TIP—Building a firm and stable surface**

A firm and stable surface does not always mean concrete and asphalt. Some natural soils can be compacted so that they are firm and stable. Other soils can be treated with stabilizers without drastically changing their appearance. Designers are encouraged to investigate the options and use surfacing materials that are consistent with the site's level of development and that require as little maintenance as possible.

**CONSTRUCTION TIP—Stable materials**

Generally, the following materials provide firmer surfaces that are more stable than the alternative:

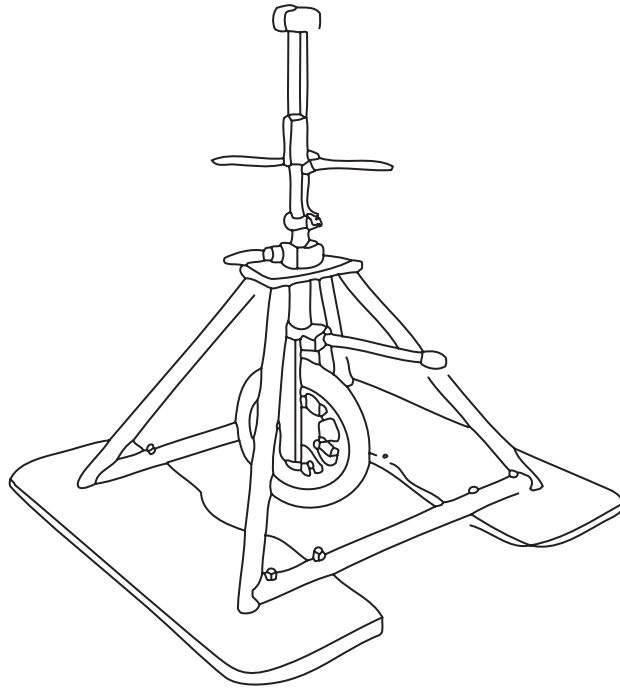
- Crushed rock (rather than uncrushed gravel)
- Rocks with broken faces (rather than rounded rocks)
- A rock mixture containing a full spectrum of sieve sizes, including fine material (rather than a single size)
- Hard rock (rather than soft rock that breaks down easily)
- Rock that passes through a ½-inch screen (rather than larger rocks)
- Rock material that is compacted in 3- to 4-inch layers (rather than thicker layers)
- Material that is moist (but not too wet) before it is compacted (rather than material that is compacted when it is dry)
- Material that is compacted with a vibrating plate compactor, roller, or by hand tamping (rather than material that is laid loose and compacted by use)



## Measuring Surface Firmness and Stability

The rotational penetrometer (RP) is a precision surface-indenter measuring tool for evaluating the firmness and stability of ground and floor surfaces (figure 1).

To measure firmness, the precision spring applies force to the penetrator and the caliper measures the vertical displacement of the penetrator into the surface. The penetrator is then rotated and the total displacement

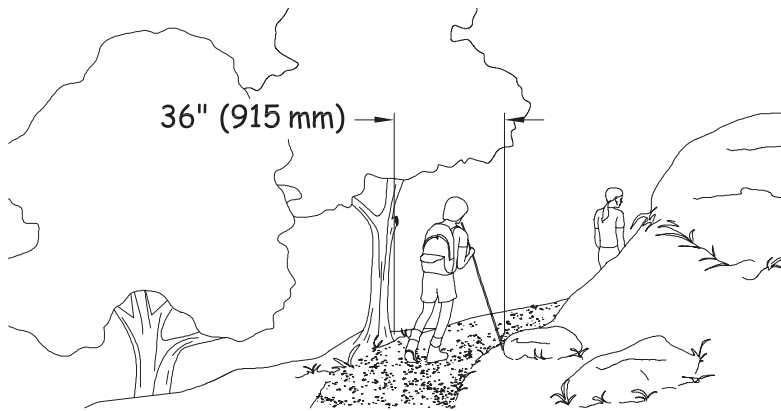
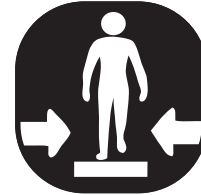


**Figure 1—The rotational penetrometer is a portable precision surface indenter that is used for measuring the firmness and stability of surfaces.**

ment into the surface is measured, indicating surface stability. The Access Board has conducted several research projects using the RP to evaluate the firmness and stability of trail and play area surfaces. Additional information about these projects is available at [www.access-board.gov/research/completed-research/accessible-exterior-surfaces](http://www.access-board.gov/research/completed-research/accessible-exterior-surfaces). Slip resistance is not required for the surface of trails because leaves, dirt, ice, snow, and other surface debris and weather conditions are part of the natural environment that would be difficult, if not impossible, to avoid.

**Clear Tread Width [1017.3]**

The clear tread width of trails must be a minimum of 36 inches (figure 2). The 36-inch-minimum clear tread width must be maintained for the entire distance of the trail and may not be reduced by gates, barriers, or other obstacles unless a condition for exception does not permit full compliance with the provision.



**Figure 2—Minimum clear trail tread width.**

Where gates and barriers require users to make 90-degree or 180-degree turns, sufficient space should be provided for people using mobility devices to make the turns (figure 3). Mobility devices that are used in the outdoors typically have a longer wheel base and are wider than mobility devices that are used indoors. The Access Board and National Institute on Disability and Rehabilitation Research sponsored research to collect anthropometric

data from a sample of about 500 people who use manual wheelchairs, power wheelchairs, and scooters. The Center for Inclusive Design and Environmental Access in the School of Architecture and Planning, University at Buffalo, The State University of New York conducted the “Anthropometry of Wheeled Mobility Project.” The final report for this project is available at [www.udeworld.com/documents/anthropometry/pdfs/AnthropometryofWheeledMobilityProject\\_FinalReport.pdf](http://www.udeworld.com/documents/anthropometry/pdfs/AnthropometryofWheeledMobilityProject_FinalReport.pdf). The report recommends that, in order to accommodate 95 percent of the users of manual wheelchairs, power wheelchairs, and scooters in the project sample, a minimum clear width of 43 inches is needed to make a 180-degree turn around a barrier similar to a chicane-style gate.

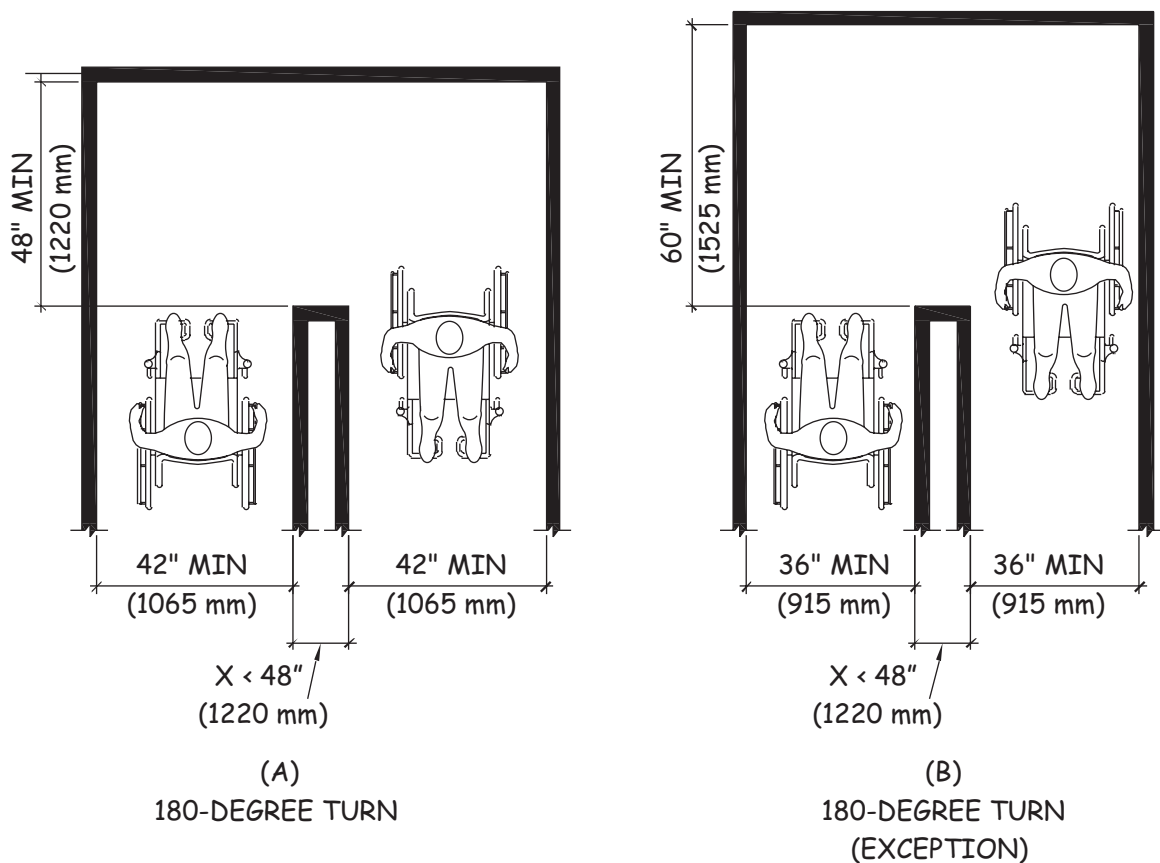


Figure 3—Clearance needed to negotiate a 180-degree turn.

## Passing Spaces [1017.4]

A trail tread width less than 60 inches does not permit two people using mobility devices to pass each other. Consequently, where the tread width is less than 60 inches, passing spaces must be provided at intervals of at least 1000 feet. Where the trail is heavily used or the trail is not at the same level as the adjoining ground surface, such as a bridge crossing a ravine, increasing the frequency of passing spaces or widening the tread width to a minimum of 60 inches provides greater access. People using mobility devices also use passing spaces to turn around.

Where the full length of a trail does not fully comply with the trail technical requirements, a passing space must be located at the end of the trail segment that complies fully with the technical requirements. This enables people who use mobility devices to turn around and proceed back to where they started. Consider ways to alert people using mobility devices when a passing space provides the last opportunity on a trail to turn around, because this may not always be apparent. Printed materials, trail Web sites, trailhead information signs, and signage at the end of the trail segment that fully complies with the technical requirements could be used to indicate the location of the last place on the trail to turn around.

Passing spaces must be:

- A minimum of 60 by 60 inches (figure 4) or

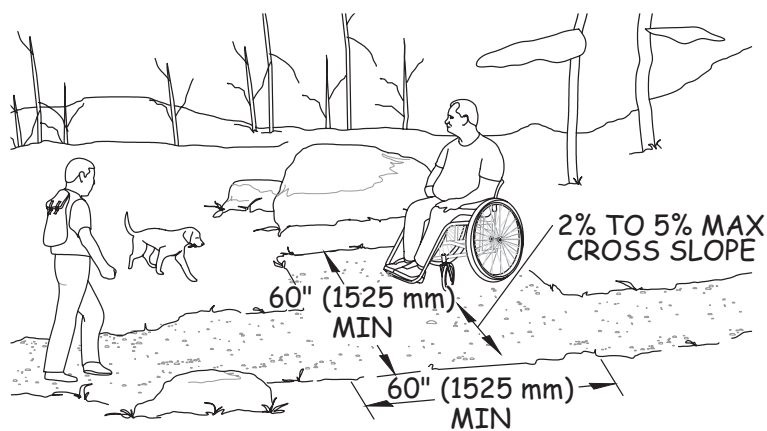
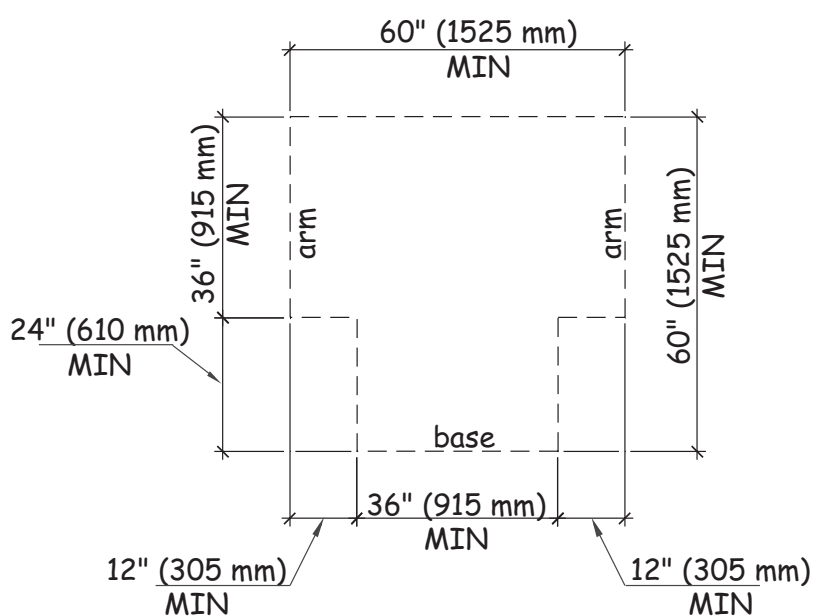
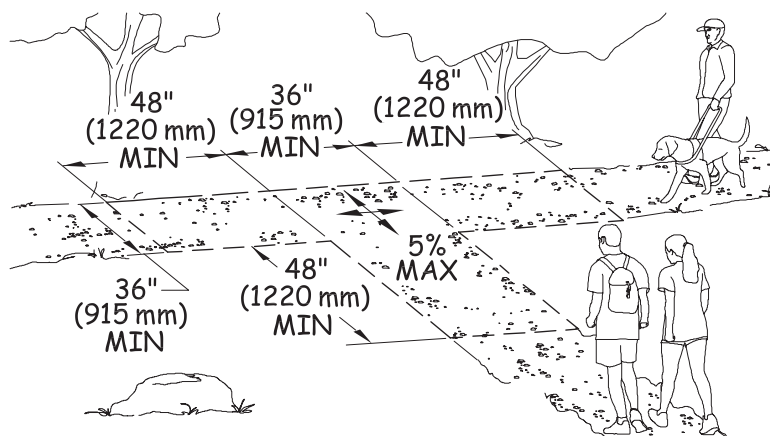


Figure 4—Minimum dimensions for a passing space.

- The intersection of two trails that provide a T-shaped space that complies with section 304.3.2 of the ABA Standards (figure 5), and the base and the arms of the T-shaped space extend a minimum of 48 inches beyond the intersection (figure 6)



**Figure 5—A T-shaped turning space (304.3.2).**



**Figure 6—Minimum dimensions for a T-shaped passing space.**

Where the intersection of two trails serves as a passing space, the vertical alignment of the trails at the intersection that form the T-shaped space must be nominally planar so that all the wheels of a mobility device remain on the ground when turning into and out of the space. Nominally planar means on the same nominal table surface (same nominal geometric surface plane) and the slopes of the table surface correspond to the running slope and cross slope of the trail tread. For example, if the trail tread has a 2 percent cross slope and 5 percent running slope, the nominal surface plane of the trail tread and passing space must both have a 2 percent cross slope and a 5 percent running slope. This allows people using mobility devices with three or four wheels to better maintain contact with the surface when moving from the main trail into a passing space. This makes it less likely that the mobility device will tip or overbalance to one side.

**Passing spaces and resting intervals can overlap.** When passing spaces and resting intervals overlap, the technical requirements for resting intervals apply and the slope of the ground surface must be no steeper than 1:48 (2 percent) in any direction. When the surface is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

**Tread Obstacles [1017.5]**

A tread obstacle is anything that interrupts the evenness of the tread surface. The vertical alignment of joints in concrete, asphalt, or board surfaces, as well as natural features, such as tree roots and rocks, within the trail tread can be tread obstacles.

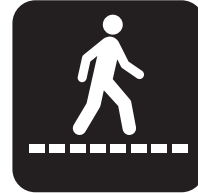


The limit on the height of tread obstacles on trails, passing spaces, and resting intervals is based on the surface material used. When the trail surface is constructed of concrete, asphalt, or boards, tread obstacles cannot exceed one-half inch in height at their highest point. When the trail surface is constructed of materials other than concrete, asphalt, or boards, tread obstacles are permitted to be a maximum of 2 inches high.

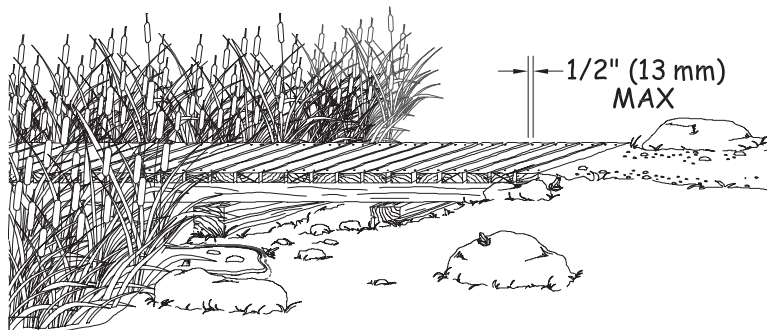
Frequent tread obstacles and tread obstacles that cross the full width of a trail tread can make travel very difficult for people using mobility devices. Where possible, separate tread obstacles by at least 48 inches, particularly when the obstacles cross the entire tread width. This separation allows people using mobility devices to fully cross one obstacle before confronting another.

## Openings [1017.6]

Openings are gaps in the surface of a trail. Gaps, including slots in a drainage grate and spaces between the planks on a bridge or boardwalk (figure 7), that are big enough for wheels, canes, or crutch tips to drop through or become trapped in are potential hazards.



Openings in the surfaces of trails, passing spaces, and resting intervals must be small enough so that a sphere more than one-half inch in diameter cannot pass through. Where possible, elongated openings should be placed perpendicular, or as close to perpendicular as possible, to the dominant direction of travel.



**Figure 7—Where possible, openings in boardwalk decking should be placed perpendicular to the direction of travel.**

## Running Slope [1017.7.1]

Running slope, also referred to as grade, is the length-wise slope of a trail, parallel to the direction of travel. Trails or trail segments of any length may be constructed with running slopes up to 1:20 (5 percent). To accommodate steep terrain, trails may be designed with shorter segments that have a running slope and length, as shown in table 2, with resting intervals at the top and bottom of each segment.





Table 2—Maximum Running Slope and Segment Length		
Running Slope of Trail Segment		Maximum Length of Segment
Steeper Than	But Not Steeper Than	
1:20 (5%)	1:12 (8.33%)	200 feet
1:12 (8.33%)	1:10 (10%)	30 feet
1:10 (10%)	1:8 (12%)	10 feet

To ensure that a trail is not designed as a series of steep segments, no more than 30 percent of the total length of the trail may have a running slope exceeding 1:12 (8.33 percent). The running slope must never exceed 1:8 (12 percent). Resting intervals must be provided more frequently as the running slope increases (figure 8).

#### Trail Running Slope

Whenever possible, trails should be constructed with lesser slopes to provide greater independent access and usability.



Figure 8—The running slope is measured along a trail's length; the cross slope is measured across its width.

### CONSTRUCTION TIP—How is running slope measured?

Running slope is often described as a ratio of vertical distance to horizontal distance, or rise to run (figure 9). For example, a running slope of 1:20 (5 percent) means that for every foot of vertical rise, there are 20 feet of horizontal distance. The technical requirements specify running slope as both a ratio and percentage.

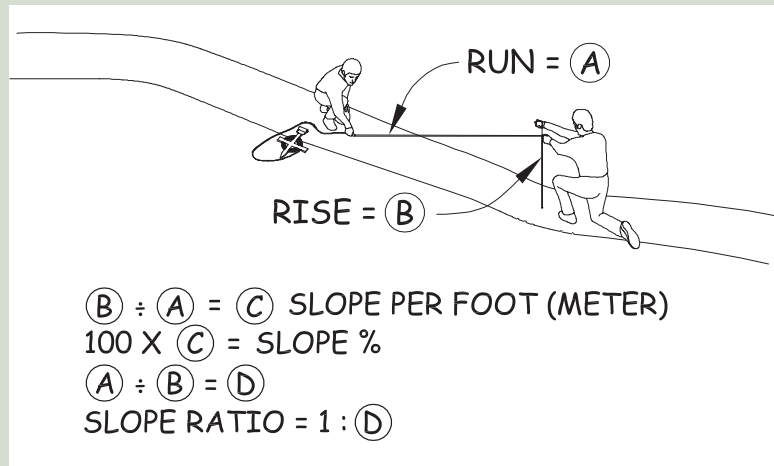


Figure 9—Determining the slope ratio.

### Cross Slope [1017.7.2]

Cross slope is the side-to-side slope of a trail tread. Some cross slope is necessary to provide drainage and to keep water from ponding and damaging the trail surface, especially on unpaved or natural surfaces.



When the trail surface is constructed of concrete, asphalt, or boards, the cross slope must be no steeper than 1:48 (2 percent). When the trail surface is constructed of materials other than asphalt, concrete, or boards, cross slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

### **Resting Intervals [1017.8]**

Resting intervals are level areas that provide an opportunity for people to stop after a steep segment and recover before continuing on. Resting intervals are required between trail segments any time the running slope exceeds 1:20 (5 percent).

Resting intervals may be provided within the trail tread or adjacent to the trail tread. When the resting interval is within the trail tread, it must be at least 60 inches long and at least as wide as the widest segment of the adjacent trail tread.

When the resting interval is adjacent to the trail, it must be at least 60 inches long and 36 inches wide. A turning space that complies with section 304.2.3 of the ABA Standards must be provided. The vertical alignment of the trail tread, turning space, and resting interval must be nominally planar so that all the wheels of a mobility device touch the ground when turning into and out of the resting interval.

When the surface of the resting interval is constructed of concrete, asphalt, or boards, the slope of the resting interval must be no steeper than 1:48 (2 percent) in any direction. When the surface of the resting interval is constructed of materials other than concrete, asphalt, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

### Protruding Objects [1017.9]

Objects that protrude into the trail clear tread width, passing spaces, and resting intervals can pose hazards to people who are blind or have low vision. Constructed elements on trails, resting intervals, and passing spaces must comply with the technical requirements for protruding objects in section 307 of the ABA Standards. Signs and other post-mounted objects are examples of constructed elements that, if located incorrectly, can be protruding objects.



**Figure 10—Protruding object requirements do not apply to natural features such as caves in undeveloped areas.**

The technical requirements for protruding objects do not apply to natural features, such as tree branches, rock formations, and trails that pass beneath rock ledges or through caves because these are not constructed elements (figure 10). Clearing limits for trail construction and maintenance usually require that brush, limbs, trees, and logs be cut back a foot or more from the edge of the trail. However, trail maintenance cycles may be several years for some trails, and vegetation may encroach on the trail in the interim between cycles. While it may not always be possible to control vegetation, it is always possible to place constructed features where they won't pose a hazard to hikers who are blind or have low vision.

## Trailheads

### Definition [F106.5]

A trailhead is defined as an outdoor space that is designated by an entity responsible for administering or maintaining a trail to serve as an access point to the trail. The junction of two or more trails or the undeveloped junction of a trail and a road is not a trailhead. For example, if a trail crosses the road, the crossing does not automatically become a trailhead.



### Trailhead Information Signs [F216.13 and 1017.10]



All hikers need trail information to make informed decisions. For example, hikers want to know which trail is most appropriate for the amount of time they have available, the people in their group, and the type of hike that best suits their needs or desires. Information about the accessibility of a trail enables people with disabilities to decide whether the characteristics of the trail are suited to their abilities. When this information is available on Web sites and in printed materials, it allows all hikers, including people with disabilities, the opportunity to understand the possible challenges of the trail before arriving at the trailhead.

When a new trail information sign is provided at the trailhead on a newly constructed or altered trail designed for use by hikers or pedestrians, the sign must provide information about the accessible characteristics of the trail. This requirement applies to new trailhead information signs regardless of whether the newly constructed or altered trail complies with the technical requirements for trails.

The new trail information signs must include the following information:

- Length of the trail or trail segment
- Type of trail surface
- Typical and minimum trail tread width
- Typical and maximum trail grade
- Typical and maximum trail cross slope

Signs can provide additional information to help people with disabilities decide whether or not to attempt a trail. For example, information about the height of any major obstacles, such as boulders in the trail tread, can help people determine if they can overcome these barriers. It is helpful to have a caution notice indicating that the posted information reflects the condition of the trail when it was constructed or assessed and on what date the information was current. Because conditions in the outdoors are subject to change, knowing when an assessment was made is very helpful.

### International Symbol of Accessibility (ISA)

The ISA is not required or encouraged on trail information signs.



Section F216.2 of the ABA Standards requires exterior signs that identify spaces by name to comply with the technical requirements for visual characters in section 703.5 of the ABA Standards, including the provisions for contrast of the characters and their background, and character size and style. Consequently, if a trail information sign identifies the name of the trail, the name of the trail must comply with the technical requirements for visual characters in section 703.5 of the ABA Standards. Tactile characters, Braille, and the International Symbol of Accessibility are not required on trail information signs.

Section F205 of the ABA Standards requires operable parts in accessible spaces to comply with the technical requirements for operable parts in section 309 of the ABA Standards, including the provisions for clear ground space, reach ranges, and operation. Consequently, if bins or holders for materials, such as maps, brochures, or fee envelopes, are provided at trailheads, a clear ground space at least 30 by 48 inches must be provided at the bins or holders. The bins or holders must be located a minimum of 15 inches and a maximum of 48 inches above the ground surface. If a latch or lid is provided, it must be operable using one hand without tightly grasping, pinching, or twisting the wrist.

## **Outdoor Constructed Features at Trailheads [F247.3.1 and 1011]**

When outdoor constructed features, such as benches, picnic tables, or trash and recycling receptacles, are provided at trailheads, at least 20 percent, but no less than one, of each type of element provided must comply with the applicable technical requirements for the element. The technical requirements for outdoor constructed features are discussed in the section of this guide on outdoor constructed features.

Toilet and bathing facilities, parking facilities, and drinking fountains must comply with the applicable sections of the ABA Standards.

## Outdoor Recreation Access Routes at Trailheads [F247.3.2]

Trailheads are usually accessed by vehicle rather than by hiking. At least one outdoor recreation access route (ORAR) must connect the following at trailheads:

- Accessible parking spaces or other arrival points
- The starting point of the trail
- Accessible elements, spaces, and facilities within the trailhead

The technical requirements for ORARs are discussed in the section of this guide on ORARs. ORARs provide greater accessibility than a trail because areas served by ORARs are typically more developed than trails.

## Existing Trailheads [F202.3]

At existing trailheads, if elements or spaces are altered but the circulation path to the altered space or element is not changed, the circulation path does not have to be altered to comply with the technical requirements of ORARs. If the circulation path at a trailhead is altered and a condition for exception does not permit full compliance with a specific provision in the technical requirements for ORARs, the circulation path must comply with the specific provision to the extent practicable. Additional information on the conditions for exceptions is provided in the section of this guide on the conditions for exceptions.



## Trail Facilities

### Camping and Picnic Facilities and Viewing Areas on Trails [F247.4]

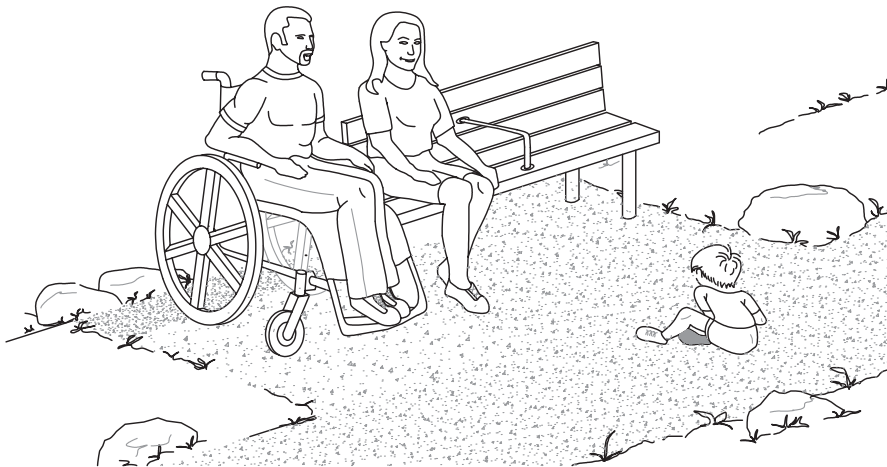
When camping and picnic facilities or viewing areas are constructed or altered along a trail, they must comply with the applicable scoping and technical requirements for the facility, regardless of whether the trail complies with the technical requirements for trails. The scoping and technical requirements for camping and picnic facilities and viewing areas are discussed in the sections of this guide on camping and picnic facilities and viewing areas.

ORARs are not required to connect trails to camping and picnic facilities, viewing areas, and pit toilets along the trails. On trails that comply with the technical requirements for trails, the routes that connect the trails to camping and picnic facilities, viewing areas, and pit toilets along the trails, including accessible elements within the facilities, must comply with the technical requirements for trails. On trails that do not comply with the technical requirements for trails, there are no technical requirements for the routes that connect the trails to camping and picnic facilities, viewing areas, and pit toilets along the trails.

Where bridges are provided along trails that substantially comply with the technical requirements for trails, the bridges must also be constructed to meet the trail technical requirements.

## Outdoor Constructed Features Along Trails [F247.5]

When outdoor constructed features, such as benches (figure 11), picnic tables, or trash and recycling receptacles, are provided along a trail, at least 20 percent, but no less than one, of each type of element provided at each location on the trail (other than within camping and picnic facilities and viewing areas, which have their own scoping requirements for outdoor constructed features) must comply with the applicable technical requirements for the element, regardless of whether the trail complies with the technical requirements for trails. The technical requirements for outdoor constructed features are discussed in the section of this guide on outdoor constructed features.



**Figure 11—A bench is an example of an outdoor constructed feature along a trail.**

## Outdoor Recreation Access Routes (ORARs)

### Scoping Requirements [F244.5, F245.4, F246.4, and F247.3.2]

An ORAR is a continuous, unobstructed path that is intended for pedestrian use and that connects accessible elements, spaces, and facilities within camping and picnic facilities and at viewing areas and trailheads. ORARs cannot be used at other types of facilities, such as educational campuses, office parks, or theme parks.

ORARs must coincide with or be located in the same area as general circulation paths provided for other users.

Within camping and picnic facilities, ORARs must connect accessible elements, spaces, and facilities provided within camping and picnic units with mobility features. ORARs must also connect public and common use areas that serve camping and picnic units with mobility features and accessible elements, spaces, and facilities provided within the public or common use areas. When a circulation path connects camping and picnic facilities and adjacent recreation facilities, such as play areas or boating facilities, at least one ORAR must connect camping and picnic units with mobility features to an accessible route serving the adjacent recreation facilities.

#### Permitted Uses of ORARs

ORARs can only be used within camping and picnic facilities, and at viewing areas and trailheads.

At viewing areas, at least one ORAR must connect accessible parking spaces or other arrival points that serve the viewing area with accessible elements, spaces, and facilities provided within the viewing area.

At trailheads, at least one ORAR must connect:

- Accessible parking spaces or other arrival points serving the trailhead
- The starting point of the trail
- Accessible elements, spaces, and facilities provided within the trailhead

Elements, spaces, or facilities that are not required to be accessible do not have to be connected by an ORAR.

ORARs are not required when accessible camping and picnic facilities, viewing areas, or outdoor constructed features are provided along a trail.

**These routes must meet the technical requirements for trails.**

## Technical Requirements [1016]

### Using the ORAR Exceptions [F202.3, Exception 4 and 1016, Exceptions 1, 2, and 3]

Where elements or spaces in camping and picnic facilities, viewing areas, or trailheads are altered but the circulation path to the altered element or space is not changed, the circulation path is not required to comply with the technical requirements for ORARs.

In alteration projects, if a condition for exception does not permit full compliance with a specific provision in the technical requirements for ORARs on a portion of an ORAR, then that portion of the ORAR must comply with the specific provision to the extent practicable. The conditions for exceptions can be

used for newly constructed ORARs at viewing areas. Additional information on the conditions for exceptions is provided in the section of this guide on the conditions for exceptions. Although not required, documenting use of the conditions for exceptions on a portion of an ORAR and retaining the documentation with the project records can be helpful if a complaint is filed with the Access Board alleging that the ORAR does not comply with the technical requirements.

If a roadway is the common circulation path for pedestrians within camping and picnic facilities or at viewing areas and trailheads, an ORAR may be provided within the roadway. When an ORAR is provided within the roadway, it is exempted from the technical requirements for cross slope, running grade, resting intervals, and passing spaces. The other technical requirements apply to ORARs provided within the roadway. For instance, where traffic calming devices, gates, or other barriers are provided on a paved roadway, the ORAR must have a clear width of 36 inches and obstacles cannot exceed one-half inch in height at their highest point within the clear width of the ORAR.

### **Surface [1016.2]**

The surfaces of ORARs, passing spaces, and resting intervals must be firm and stable. Additional information on firm and stable surfaces is provided in the trails section of this guide.

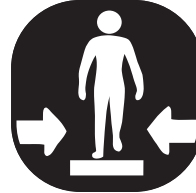


Slip resistance is not required because leaves and needles, dirt, ice, snow, and other surface debris and weather conditions are components of the natural environment that would be difficult, if not impossible, to avoid.

During the design process, evaluating the planned surface material for noticeable distortion or compression during the seasons for which the surface is managed, and for stability under normally occurring weather conditions and expected uses, can be helpful. If the planned surface material does not remain firm and stable during this evaluation, another surfacing product may be a more appropriate choice.

### Clear Width [1016.3]

The clear width of ORARs must be a minimum of 36 inches. The 36-inch-minimum clear width must be maintained for the entire ORAR and may not be reduced by gates, barriers, or other obstacles unless a condition for exception does not permit full compliance with the provision.



ORARs don't all necessarily have to be the same width. ORARs may be different widths, depending on their location. Consider the number of people who are likely to use the ORAR at the same time and how they will want to use it—single file or walking side-by-side. Two people using mobility devices need a clear width of at least 60 inches to pass each other on an ORAR. Consider providing a minimum clear width of 60 inches on ORARs that connect camping units with mobility features to important features, such as toilet and bathing facilities or water hydrants.

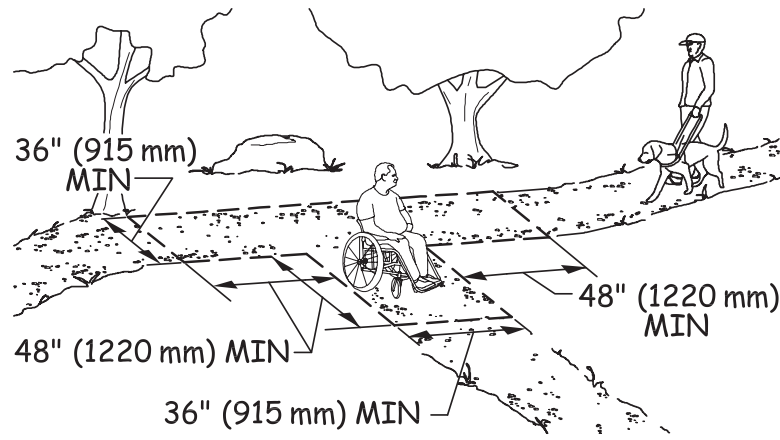
### Passing Spaces [1016.4]

Where the clear width of an ORAR is less than 60 inches, passing spaces must be provided at intervals of at least 200 feet.

Passing spaces must be:

- A minimum space of 60 by 60 inches or
- The intersection of two ORARs that provide a T-shaped space that complies with section 304.3.2 of the ABA Standards, where the base and the arms of the T-shaped space extend a minimum of 48 inches beyond the intersection (figure 12)

Either of these configurations provides enough space for people using mobility devices to move to the side and let other people pass along the ORAR.



**Figure 12—A T-intersection may be used as a passing space on an outdoor recreation access route or a beach access route if it has the dimensions shown (or larger).**

Consider providing either a minimum clear width of 60 inches or passing spaces at shorter intervals where an ORAR is heavily used or adjoins elements, spaces, or facilities that are heavily used. Also, consider providing a minimum clear width of 60 inches if the ORAR is a boardwalk or otherwise not at the same level as the adjoining ground surface or if the edge is bordered by timbers or edge protection.

When the intersection of two ORARs serves as a passing space, the vertical alignment of the routes at the intersection that form the T-shaped space must be nominally planar (i.e., as flat as possible) so that all the wheels of a mobility device touch the ground when turning into and out of the passing space.

Passing spaces and resting intervals can overlap. When passing spaces and resting intervals overlap, the technical requirements for resting intervals apply and the slope of the surface must be no steeper than 1:48 (2 percent) in any direction when the ORAR is constructed of concrete, asphalt, or boards. When the surface is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage. Otherwise, passing-space surfaces must have the same slopes as the adjoining ORAR.

## Obstacles [1016.5]

Obstacles are anything that interrupts the evenness of the surface of an ORAR. Obstacles may occur where a tree root or rock protrudes above the surface or where two different surfaces abut, such as when a concrete path joins a natural surface. When an ORAR is provided within a vehicular route, traffic-calming devices or speed bumps can be obstacles. If obstacles are pronounced, they can pose a serious tripping hazard and make it difficult to travel using a mobility device.



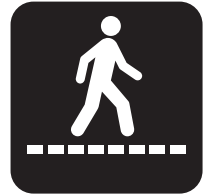
The limit on the height of obstacles on ORARs, passing spaces, and resting intervals is based on the surface material used. When the surface is constructed of concrete, asphalt, or boards, obstacles cannot exceed one-half inch in height at their highest point. When the surface is constructed of materials other than concrete, asphalt, or boards, obstacles cannot exceed 1 inch in height at their highest point.

Frequent obstacles and obstacles that cross the full width of an ORAR can make travel difficult for people using mobility devices. Where possible, separate obstacles that cross the full width of an ORAR by at least 48 inches so people using mobility devices can fully cross one obstacle before confronting another.

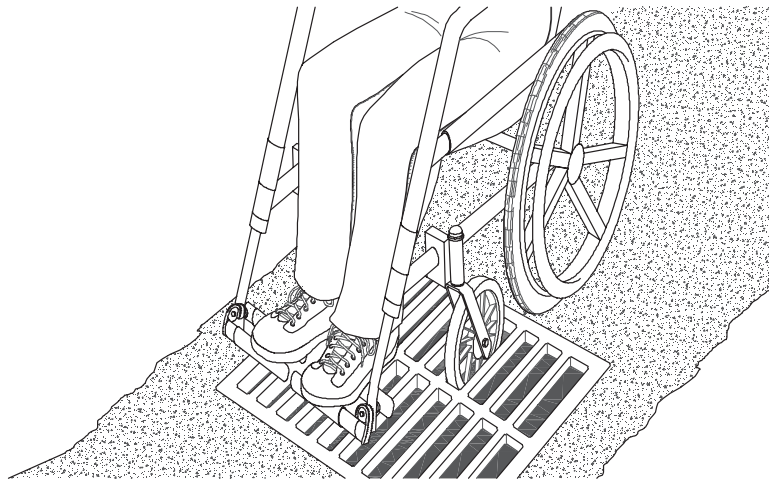


**Openings [1016.6]**

Openings are gaps in the surface of an ORAR. Gaps, including spaces between the planks on a boardwalk or slots in a drainage grate, that are big enough for wheels, cane, or crutch tips to drop through or become trapped in are potential hazards (figure 13). Where possible, drainage grates should be located outside the minimum clear width of the ORAR.



Openings in the surface of ORARs, passing spaces, and resting intervals must be small enough so that a sphere more than one-half inch in diameter cannot pass through. Where possible, elongated openings should be placed perpendicular, or as close to perpendicular as possible, to the dominant direction of travel.



**Figure 13—Whenever possible, elongated openings in outdoor recreation access routes should be placed perpendicular to the dominant direction of travel to avoid creating potential problems.**

## Running Slope [1016.7.1]



Running slope, also referred to as grade, is the lengthwise slope of an ORAR, parallel to the direction of travel. ORAR sections of any length may be constructed with a running slope up to 1:20 (5 percent). To accommodate steep terrain, ORARs may be designed with shorter sections that have a steeper running slope and length, as shown in table 3, with resting intervals at the top and bottom of each section. The running slope of any section of an ORAR must never exceed 1:10 (10 percent).

Table 3—Maximum Running Slope and Segment Length		
Running Slope of Segment of Outdoor Recreation Access Route		Maximum Length of Segment
Steeper Than	But Not Steeper Than	
1:20 (5%)	1:12 (8.33%)	50 feet
1:12 (8.33%)	1:10 (10%)	30 feet

## Cross Slope [1016.7.2]

Cross slope is the side-to-side slope of the surface of an ORAR. When the surface is constructed of concrete, asphalt, or boards, the cross slope must be no steeper than 1:48 (2 percent). When the surface is constructed of materials other than concrete, asphalt, or boards, cross slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.



### Resting Intervals [1016.8]

Resting intervals are level areas that provide an opportunity for people to rest before continuing along an ORAR. Resting intervals are required at the top and bottom of an ORAR section any time the running slope exceeds 1:20 (5 percent).

Resting intervals may be provided within an ORAR or adjacent to an ORAR. When a resting interval is within an ORAR, it must be at least 60 inches long and at least as wide as the widest section of the ORAR leading into it (figure 14).

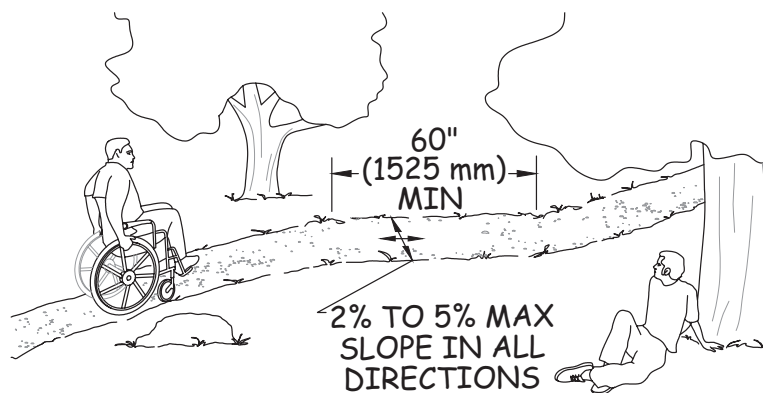


Figure 14—Length and slope requirements for resting intervals.

When a resting interval is adjacent to an ORAR, it must be at least 60 inches long and at least 36 inches wide. A turning space that complies with section 304.2.3 of the ABA Standards must be provided. The vertical alignment of the ORAR, turning space, and resting interval must be nominally planar (i.e., as flat as possible) so that all the wheels of a mobility device touch the ground surface when turning into and out of the resting interval.

When the surface of the resting interval is constructed of concrete, asphalt, or boards, the slope of the resting interval must be no steeper than 1:48 (2 percent) in any direction. When the surface of the resting interval is constructed of materials other than concrete, asphalt, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

### Protruding Objects [1016.9]

Objects that protrude into ORARs, passing spaces, and resting intervals can pose hazards to people who are blind or have low vision. Constructed elements, such as signs on ORARs, resting intervals, and passing spaces, must comply with the technical requirements for protruding objects in section 307.2 of the ABA Standards (figure 15).

The technical requirements for protruding objects do not apply to natural features, such as tree branches and rock formations. To prevent injuries to people who are blind or have low vision, whenever possible, maintain the vertical clearance free of tree branches a minimum of 80 inches above the ground surface along ORARs, resting intervals, and passing spaces.

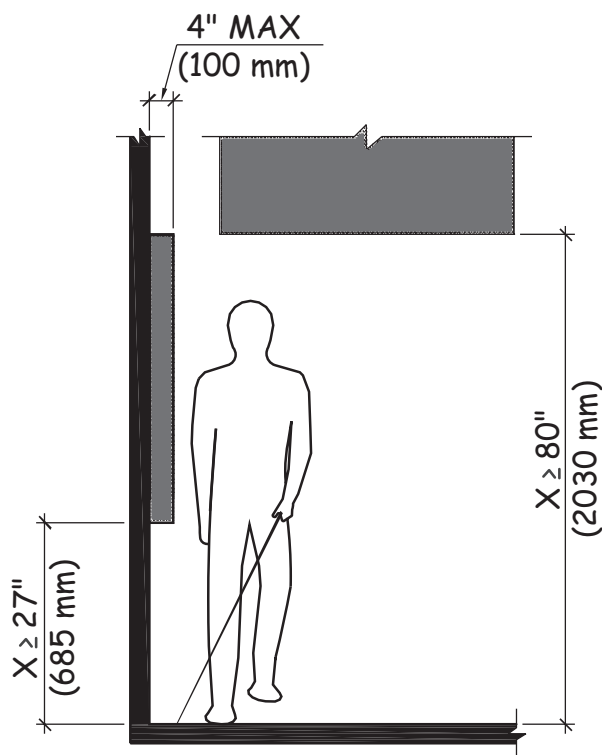


Figure 15—Constructed elements must comply with the technical requirements for protruding objects in section 307.2.

## **Outdoor Constructed Features**

### **Definition [F106.5]**

Outdoor constructed features are picnic tables, fire rings, grills, fireplaces, wood stoves, trash and recycling receptacles, water hydrants, utility and sewage hookups, outdoor rinsing showers, benches, and viewing scopes provided at outdoor recreation facilities.

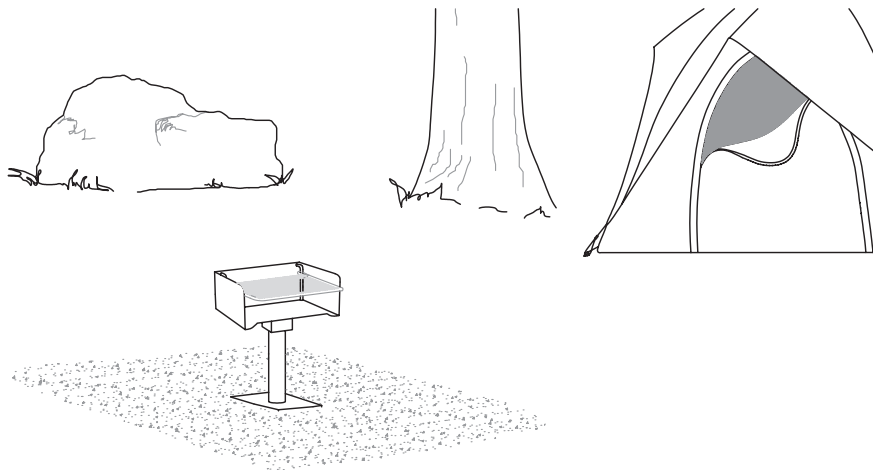
### **Scoping Requirements**

The scoping requirements for outdoor constructed features are explained in the sections of the standards that contain the scoping requirements for camping and picnic facilities, viewing areas, and trails and trailheads. The scoping requirements apply only where outdoor constructed features are provided. For instance, if a bench is provided at a viewing area, the scoping requirements apply to the bench. The scoping requirements do not require a viewing scope to be provided at the viewing area if none is planned. There are no scoping requirements for outdoor constructed features at beaches.

**Camping and Picnic Facilities [F244.2.3.1, F244.3, F245.2.5.1, and F244.5.3]**

Within camping and picnic units with mobility features (figure 16), at least one of each type of outdoor constructed feature must comply with the applicable technical requirements for the feature. Where more than two of the same type of outdoor constructed feature are provided within camping and picnic units required to provide mobility features, at least two of the same type of outdoor constructed feature must comply with the applicable technical requirements for the feature.

Where outdoor constructed features are provided in common use and public use areas that serve camping and picnic units with mobility features, at least 20 percent, but no less than one, of each type of outdoor constructed feature must comply with the applicable technical requirements for the feature.



**Figure 16—This grill is an example of a common outdoor constructed feature within picnic and camping units.**

### **Viewing Areas [F246.3]**

Within viewing areas, at least 20 percent, but no less than one, of each type of outdoor constructed feature must comply with the applicable technical requirements for the feature.

### **Trails and Trailheads [F247.3.1 and F247.5]**

Within trailheads and along trails, at least 20 percent, but no less than one, of each type of outdoor constructed feature must comply with the applicable technical requirements for the feature. Where camping and picnic facilities are provided on trails, the scoping requirements for outdoor constructed features within camping and picnic facilities applies.

## **Common Technical Requirements**

The technical requirements for outdoor constructed features apply only to those outdoor constructed features that are required to comply with the technical requirements by the scoping requirements. For instance, if five benches are provided at a viewing area, at least 20 percent (or one) of the benches must comply with the technical requirements for benches.

### **Clear Ground Space [1011.2]**

A clear ground space must be provided at each outdoor constructed feature. The technical requirements include specific provisions for the size, location, surface, slope, and openings in the clear ground space.

**Exceptions [1011.2, Exceptions 1 and 2]**—When individual outdoor constructed features are altered and the ground surface is not changed, the clear ground space is not required to comply with the specific provisions for surface and slope.

In alterations, when a condition for exception does not permit full compliance with a specific provision in the technical requirements for the clear ground space, the clear ground space must comply with the specific provision to the extent practicable. Additional information on the conditions for exceptions is provided in the section of this guide on the conditions for exceptions.

**Size [1011.2.1]**—The size of the clear ground space is based on the dimensions for maneuvering clearance in section 305.7 of the ABA Standards for clear ground spaces that are confined on all or part of three sides. Providing clear ground spaces that are larger than the minimum required sizes can make the outdoor constructed feature more independently usable by people with varied abilities.

Table 4 shows the minimum requirements for clear ground space sizes for each outdoor constructed feature. The location and orientation of the clear ground space is discussed under the applicable outdoor constructed feature.



**Table 4—Clear Ground Space Requirements**

<b>Outdoor Constructed Feature</b>	<b>Minimum Size and Location</b>
Picnic tables	36 inches on all usable sides of the picnic table measured from the back edge of the seats or benches.
Fire rings, grills, fireplaces, and woodstoves	48 by 48 inches on all usable sides of a fire ring, grill, fireplace, and woodstove.  Center the space on each usable side of the grill, fireplace, and woodstove.
Trash and recycling receptacles	36 by 48 inches positioned for a forward approach to the receptacle opening or 30 by 60 inches positioned for a parallel approach to the receptacle opening.
Water hydrants	72 by 48 inches with the long side of the space adjoining or overlapping an ORAR or trail, as applicable, or another clear ground space.  Locate the clear space so that the water spout is a minimum of 11 inches and a maximum of 12 inches from the rear center of the long side of the space.
Utility and sewage hookups	30 by 60 inches with the long side of the space adjoining or overlapping an accessible parking space or pull-up space for recreational vehicles.  Locate the space so that the hookups are at the rear center of the space.  Bollards or other barriers may not obstruct the clear ground space in front of the hookups or restrict their use.
Outdoor rinsing showers	60 by 60 inches centered on the shower heads.  Locate the space so that the shower pedestal or wall supporting the shower head is at the rear of the space.
Benches	36 by 48 inches positioned near the bench with one side of the space adjoining an ORAR or trail, as applicable.  The clear ground space may not overlap the ORAR or trail or another clear ground space.
Viewing scopes	36 by 48 inches positioned for a forward approach to the viewing scope.  Provide knee and toe clearance under the viewing scope that complies with section 306.  Locate the space so that the eyepiece is centered on the space.

### **Surface, Slope, and Openings [1011.2.2, 1011.2.3, and 1011.2.4]**

The surface of the clear ground space must be firm and stable. Additional information on firm and stable surfaces is provided in the trails section of this guide.

When the surface of the clear ground space is constructed of concrete, asphalt, or boards, the slope of the clear ground space must be no steeper than 1:48 (2 percent) in any direction. When the surface of the clear ground space is constructed of materials other than concrete, asphalt, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

Openings in the surface of the clear ground space must be small enough so that a sphere more than one-half inch in diameter cannot pass through. Where possible, drainage grates should be located outside the clear ground space and elongated openings should be placed perpendicular, or as close to perpendicular as possible, to the dominant direction of travel.

### **Operable Parts [1011.3]**

The operable parts of outdoor constructed features, such as handles, levers, and latches, must comply with the technical requirements of sections 309.3 and 309.4 of the ABA Standards, unless an exception applies. The technical requirements for operable parts and exceptions to the technical requirements are discussed under the applicable outdoor constructed feature.



## Technical Requirements Applicable to Specific Features

### Picnic Tables [1011.2 and 1011.4]

The number of wheelchair spaces that must be provided at each table is based on the usable table surface perimeter. At least one wheelchair seating space a minimum of 30 by 48 inches must be provided for each 24 linear feet of usable space around the perimeter of a tabletop. Practically speaking, this means that one space is usually required for tables up to 9 feet long. Tables between 10 and 20 feet long usually require two wheelchair spaces (figure 17). More spaces are required for longer tables.

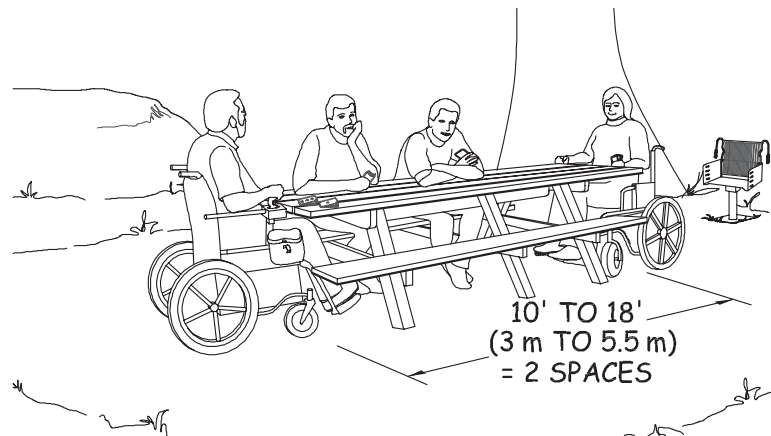
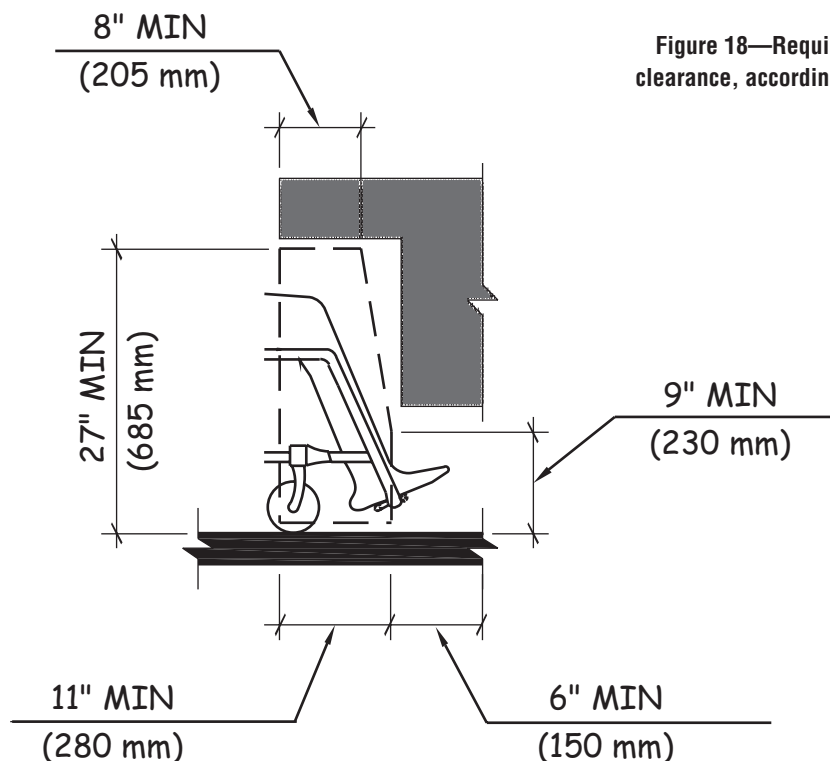


Figure 17—At least two spaces must be accessible at a 10- to 18-foot long picnic table.

The wheelchair space must be positioned for a forward approach and provide knee and toe clearance under the table that complies with section 306 of the ABA Standards. Knee clearance must be at least 30 inches wide and 8 inches deep at 27 inches above the ground surface. Toe clearance must be at least 30 inches wide and 17 inches deep and extend at least 9 inches above the ground surface (figure 18). Knee and toe clearance are required to ensure that a person using a mobility device can sit close to a tabletop, regardless of the table's design. If a table is constructed with one solid leg on each end, as opposed to an A-shaped frame or two individual legs on each end of the table that would allow the wheelchair to fit between, the toes of a person using a mobility device could hit the table leg.

Section 902.3 of the ABA Standards requires that the tops of dining surfaces be between 28 inches and 34 inches above the floor or ground surface. Tabletops



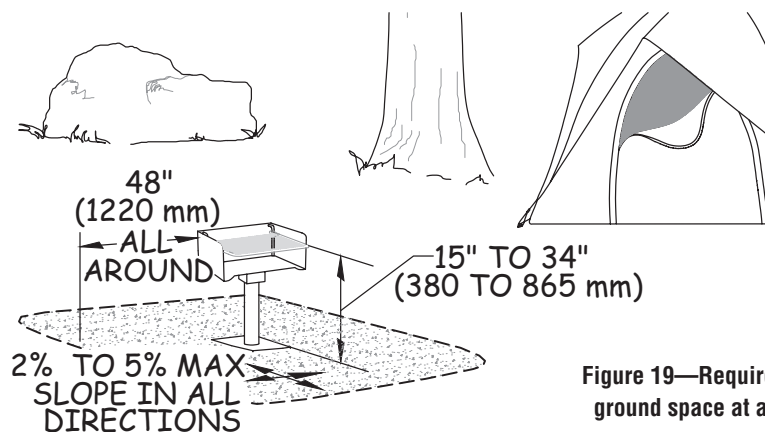
**Figure 18—Required knee and toe clearance, according to section 306.3.**

may be constructed of a number of different types of materials. Some of these materials, such as heavy timbers, may be quite thick and must allow for knee and toe clearance under the table at the wheelchair spaces.

In addition to the clear ground space for each wheelchair space, a clear ground space 36 inches wide must be provided on all usable sides of a table. This allows a person using a mobility device to maneuver around all usable sides of the table. The clear ground space is measured from the back edge of the benches.

### **Fire Rings, Grills, Fireplaces, and Woodstoves [1011.2, 1011.3, and 1011.5]**

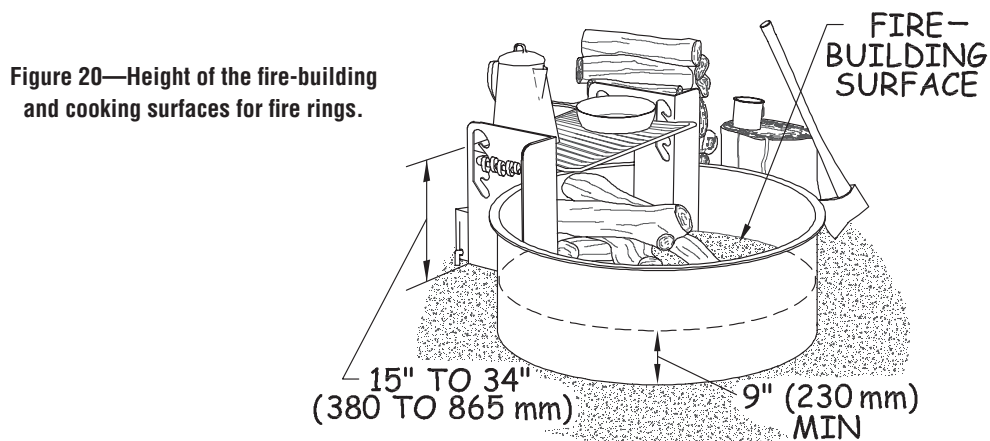
A minimum of 48 by 48 inches of clear ground space must be provided on all usable sides of fire rings, grills, fireplaces, and woodstoves. The usable sides of these elements are the sides that can be used for building a fire or for cooking. All sides of fire rings and grills are generally usable, unless a wall or other structure on a side renders that side unusable. At least 48 inches of clear ground space must be provided around the entire fire ring or grill (figure 19). The front sides of fireplaces and woodstoves are generally the usable sides.



**Figure 19—Requirements for clear ground space at a pedestal grill.**

The fire-building surface within a fire ring must be at least 9 inches above the ground (figure 20). Fire rings that are constructed with two concentric walls reduce the chances that people using mobility devices and children will receive burns when the outside wall is touched.

The cooking surface must be between 15 and 34 inches above the ground. The cooking surface of pedestal grills may be adjustable beyond the required range, but must include adjustments within the specified range.



Some custom-built fire rings and fireplaces may have a raised edge or wall around the fire-building area, perhaps built out of bricks or mortared stone. The depth or thickness of the raised edge or wall must not exceed 10 inches (figure 21).

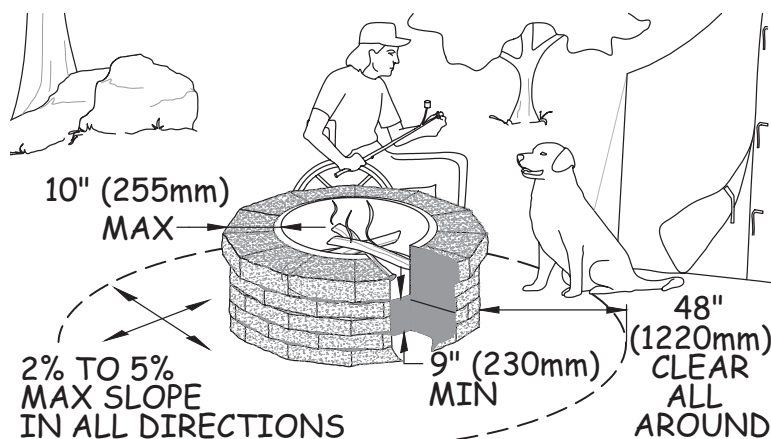
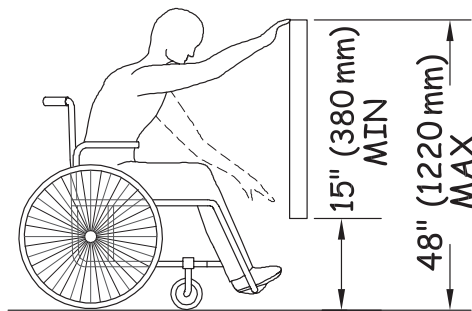


Figure 21—Requirements for custom-built fire rings.

The operable parts of fire rings, grills, fireplaces, and wood stoves must be between 15 and 48 inches above the ground (figure 22). If an operable part, such as a handle or lever, falls outside this range during operation, it is not compliant. Operable parts must also be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force.



**Figure 22—Reach requirements for operable parts.**

Fire rings, grills, fireplaces, and woodstoves with compliant operable parts may not yet be commercially available. Until products with compliant operable parts become commercially available, compliance is required to the extent practicable.

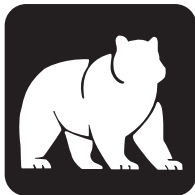


**Trash and Recycling Receptacles [1011.2 and 1011.3]**

At trash and recycling containers, a minimum of 36 by 48 inches of clear ground space positioned for a forward approach to the receptacle opening, or a minimum of 30 by 60 inches of clear ground space positioned for a parallel approach to the receptacle opening must be provided.



The operable parts of trash and recycling containers, such as handles or latches, must be between 15 and 48 inches above the ground. The operable parts must also be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force.



Currently, trash and recycling receptacles with hinged lids and latches to keep out large animals require a person to operate the latch or handle by tightly grasping, pinching, or twisting the wrist, and by applying more than 5 pounds of force. Until products with compliant operable parts become commercially available, compliance is required to the extent practicable.

Dumpster-type trash and recycling receptacles are not required to comply with the technical requirements for operable parts because the receptacle openings are typically more than 48 inches above the ground.

**Benches [1011.2]**

A minimum of 36 by 48 inches of clear ground space must be provided near a bench, with one side of the clear space adjoining an ORAR or trail as applicable. This clear ground space must not overlap the ORAR, trail tread, or another clear ground space.

Although there are no technical requirements for the bench itself, providing a bench with at least one armrest and back support that runs the full length of the bench is helpful to people who need the support or have difficulty standing up



from a seated position. However, armrests on both ends of a bench could prevent a person using a mobility device from being able to transfer from the device onto the bench. One option is to provide a bench with a backrest and one armrest placed in the middle of the bench. Another option is to place a single armrest on the end of the bench farthest from the clear ground space.

The technical requirements in section 903 of the ABA Standards apply only to benches used for dressing and undressing in fitting and locker rooms, and not to benches used for sitting in an outdoor developed area.

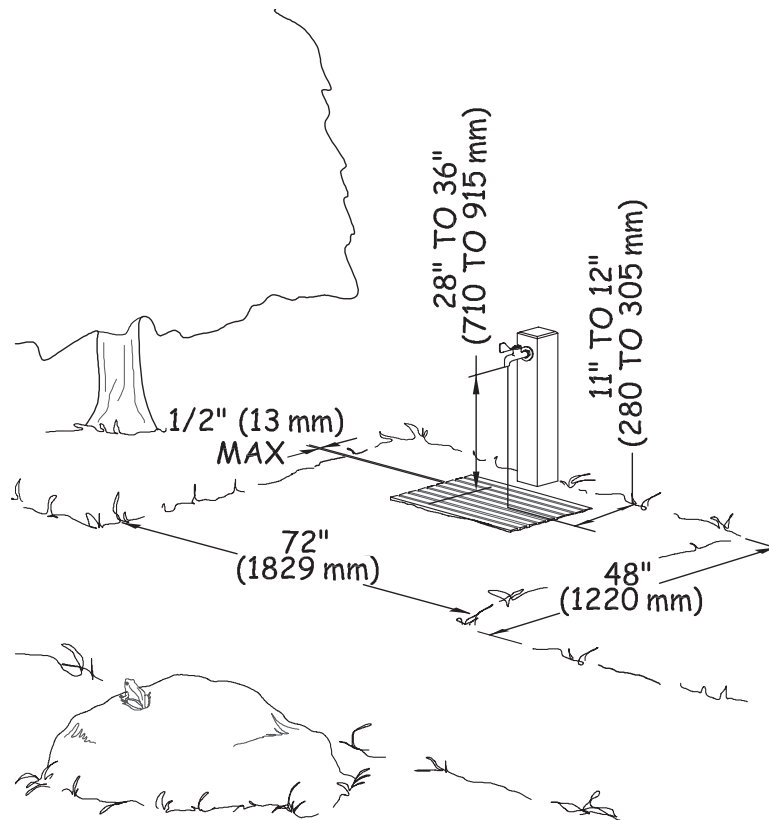
### **Water Hydrants and Water Spouts [1011.2, 1011.3, and 1011.6]**

Water hydrants are outdoor devices for dispensing water, including water faucets on posts and hand pumps. The opening for dispensing the water is called a water spout.



At water hydrants, a minimum of 72 by 48 inches of clear ground space must be provided with the long side of the space adjoining or overlapping an ORAR or trail as applicable, or another clear ground space (figure 23). The clear ground space must be located so that the water spout is between 11 and 12 inches from the rear center of the long side of the space. This allows people using mobility devices to approach and operate the water spout from either the right or left side.

There are different technical requirements for the clear ground space at water utility hookups in camping units required to provide mobility features, which are discussed in the next section. There are also different technical requirements for drinking fountains in section 602 of the ABA Standards.

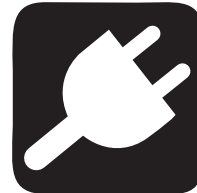


**Figure 23—Technical requirements for water hydrants.**

Water spouts must be between 28 and 36 inches above the ground. The operable parts of water hydrants, such as handles or levers, must be between 15 and 48 inches above the ground. Operable parts must also be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force. Water hydrants with compliant operable parts may not yet be commercially available. Until products with compliant operable parts become commercially available, compliance is required to the extent practicable.

### Utility and Sewage Hookups [1011.2, 1011.3, and 1011.6]

At utility hookups, such as electrical service, water, cable or Internet connections, in camping units with mobility features and at sewage hookups in dump stations, a minimum of 30 by 60 inches of clear ground space must be provided with the long side of the space adjoining or overlapping an accessible parking space or pull-up space for recreational vehicles (RVs). The clear ground space must be designed so that the hookups are located at the rear center of the space. Bollards or other barriers must not obstruct the clear ground space in front of the hookups.



The operable parts of utility hookups must be between 15 and 48 inches above the ground. Water spouts must be between 28 and 34 inches above the ground. Operable parts must also be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force. Sewage hatches do not have to comply with the technical requirements for operable parts.

Because RV dump stations are usually accessed by vehicle, an ORAR is not required to connect to RV dump stations when an accessible vehicle pull-up space is provided at RV dump stations. An accessible vehicle pull-up space must be a minimum of 20 feet wide.

### Outdoor Rinsing Showers [1011.2, 1011.3, and 1011.7]

Outdoor rinsing showers allow people to rinse off sand, dirt, and debris. They are not intended for bathing. Outdoor rinsing showers generally don't offer privacy and people usually are not allowed to disrobe when using them.



At outdoor rinsing showers, a minimum of 60 by 60 inches of clear ground space must be provided. The clear ground space must be centered on the shower head to enable people using mobility devices to turn in the space while rinsing. The shower pedestal or wall with the shower heads must be at the rear end of the clear ground space.

At least one hand-held shower spray unit must be provided. The hand-held shower spray unit must have a hose at least 59 inches long and at least one fixed position between 15 and 48 inches above the ground (figure 24). When vandalism is a consideration, a fixed shower head mounted at 48 inches above the ground is allowed in place of a hand-held shower spray unit. Outdoor rinsing showers can have more than one hand-held spray unit or fixed shower head.

The other operable parts of outdoor rinsing showers, such as handles or levers, must be between 15 and 48 inches above the ground and be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force. If self-closing devices are used, they should remain open and allow water to flow for at least 10 seconds.

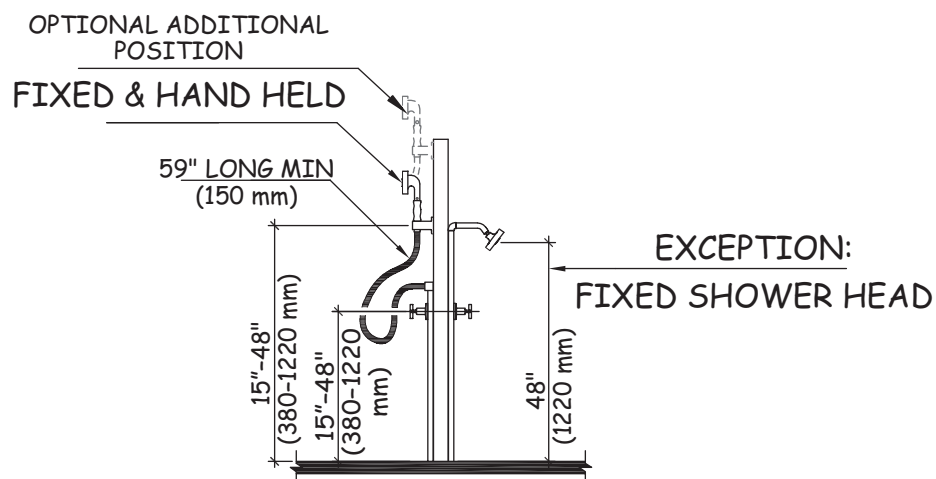


Figure 24— Technical requirements for outdoor rinsing showers.

### Viewing Scopes [1011.2, 1011.3, and 1011.8]

Viewing areas or overlooks are sometimes equipped with viewing scopes to provide visitors with a closer view of a point of interest. When viewing scopes are provided, separate viewing scopes must be provided for use from a standing position and from a seated position. This allows everybody the opportunity to experience similar views. Viewing scopes that can be used from a seated position provide viewing opportunities not only for seated individuals, but also for children and people of short stature. The viewing scopes can be mounted on separate pedestals or on the same pedestal.



At viewing scopes used from a seated position, a minimum of 36 by 48 inches of clear ground space positioned for a forward approach to the viewing scope must be provided. The clear ground space must be centered on the eyepiece of the viewing scope. Knee and toe clearance that complies with section 306 of the ABA Standards must be provided under the viewing scope (figure 25).

The eyepieces of viewing scopes used from a seated position must be between 43 and 51 inches above the ground surface. The other operable parts of viewing scopes, such as handles or levers, must be located between 15 and 48 inches above the ground and be operable using one hand without tightly grasping, pinching, or twisting the wrist, and with no more than 5 pounds of force.

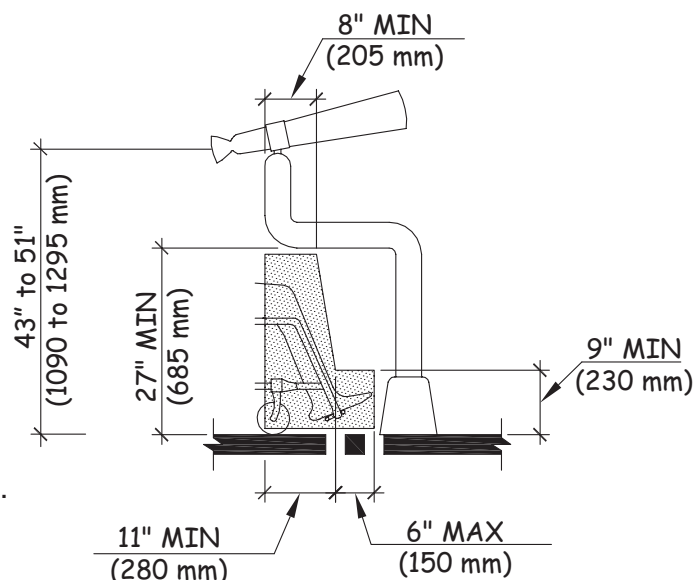


Figure 25—Technical requirements for viewing scopes.

## Other Features

### Sinks [F212.3, Exception 2]

Sinks in camping and picnic facilities are not required to comply with the technical requirements for sinks in Section 606 of the ABA Standards, unless a cooktop or conventional range is provided. Lavatories must comply with the technical requirements for lavatories in section 606 of the ABA Standards.

When hot water is provided, the drainpipes at sinks and lavatories must be wrapped, insulated, or shielded to help prevent a person using a mobility device from accidentally touching the hot drainpipes. This is especially important if a forward approach is provided.

### Toilet and Bathing Facilities [F213 and 603 through 610]

When toilet and bathing facilities are provided, they must comply with the scoping and technical requirements in sections F213 and 603 through 610 of the ABA Standards. When bathing facilities are provided, at least one transfer shower, roll-in shower, or accessible bathtub must be provided.



When multiple single user portable toilet or bathing units are clustered at the same location, no more than 5 percent of the units at each cluster must comply with the technical requirements in section 603 of the ABA Standards.

## Pit Toilets [F213.1, Exception]

Pit toilets are primitive outhouses that may consist simply of a hole in the ground covered by a toilet riser (figure 26). Pit toilets are only provided in low development sites where they are determined to be necessary for resource or environmental protection. Pit toilets on trails and in camping facilities are not required to comply with the scoping and technical requirements for toilet facilities. F247.4.4 requires routes connecting pit toilets located along a trail to comply with the technical requirements in 1017 only if the trail complies with 1017. ORARs are not required to connect facilities on trails.

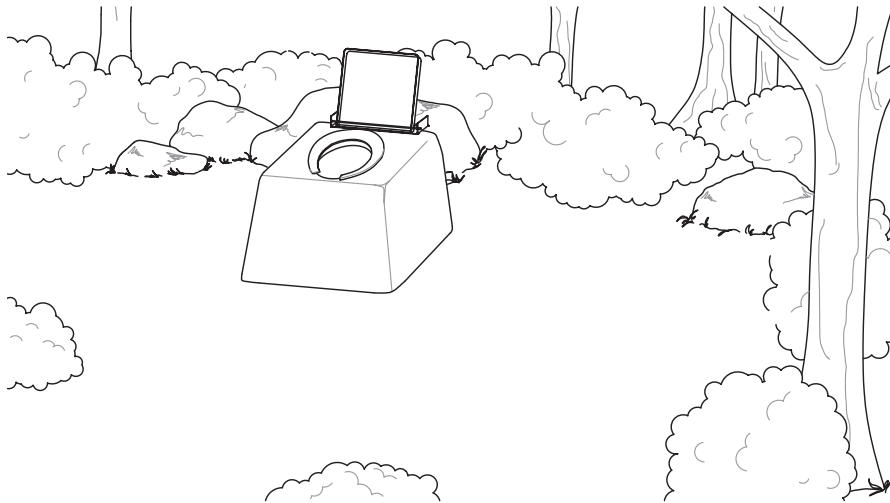


Figure 26—A pit toilet.



## Camping Facilities



### Definition [F106.5]

A camping facility is a site, or a portion of a site, that is developed for outdoor recreational purposes and contains camping units.

A camping unit is an outdoor space in a camping facility that is used for camping and contains outdoor constructed features, parking spaces for RVs or other vehicles, tent pads or tent platforms, or camp shelters.

### Camping Units With Mobility Features [F244.2]

Camping facilities must provide a minimum number of camping units with mobility features based on the total number of camping units provided in the camping facility, in accordance with table 5. When different types of camping units are provided (e.g., camping units for RVs only or tent camping only, or camping units with camp shelters), table 5 applies to each type of camping unit provided. If a camping facility has 15 units for tent camping only and 30 units for RVs only, then at least two of the tent camping units and at least three of the RV units must provide mobility features.

**Table 5—Camping Units with Mobility Features**

<b>Total Number of Camping Units Provided in Camping Facility</b>	<b>Minimum Required Number of Camping Units with Mobility Features</b>
1	1
2 to 25	2
26 to 50	3
51 to 75	4
76 to 100	5
101 to 150	7
151 to 200	8
201 and over	8, plus 2 percent of the number over 200

Camping units with mobility features don't have to be identified by signs. Entities should provide information on the location of camping units with mobility features on Web sites, in brochures, and at bulletin boards or information kiosks at the camping facility. When an entity operates a reservation system for camping units or assigns camping units upon arrival, the entity should establish policies and procedures to ensure that camping units with mobility features are available for people with disabilities until all other camping units are occupied.

### **Alterations and Additions [F244.2.1]**

When altering or adding camping units at an existing camping facility, only consider the number of altered or added camping units when using table 5 to determine the required number of camping units with mobility features. Continue this practice whenever camping units are altered or added until the total number of camping units with mobility features in the camping facility complies with the minimum number required in table 5.



### Example

A camping facility has 50 RV camping units. The facility currently does not have an RV camping unit with mobility features. Twenty-five additional RV units are being added to the facility. With the addition of 25 new units, 2 of the newly added units are required to have mobility features even though, in new construction, 75 new units would require 4 units with mobility features to be provided. The scoping requirements apply to any subsequent alteration or addition until the full number of units with mobility features required on a site is satisfied.

When an entity is implementing a transition plan for program accessibility developed pursuant to regulations issued under section 504 of the Rehabilitation Act, which designates specific camping units to provide mobility features, the entity is not required to provide accessible elements when altering individual elements within camping units that are not designated to provide mobility features. When all the elements within a camping unit are altered, the altered camping unit must provide mobility features unless the minimum number of camping units with mobility features required in table 5 is already provided at the camping facility.

### Dispersion [F244.2.2]

Camping units with mobility features must provide choices of units comparable to and integrated with those available to all other campers. For instance, if camping units are provided near a body of water or on the foothills, then some units with mobility features must also be located in the same settings.

## **Elements Within Camping Units With Mobility Features [F244.2.3]**

At least one of each type of element provided within camping units with mobility features, including outdoor constructed features, parking spaces for RVs, parking spaces for vehicles other than RVs, tent pads and tent platforms, and camp shelters, must comply with the applicable technical requirements for the element. When more than one of the same type of element (e.g., picnic tables, tent pads) is provided within a camping unit with mobility features, at least two of the same type of element must comply with the applicable technical requirements for the element.

## **Outdoor Constructed Features in Common Use and Public Use Areas [F244.3]**

Where outdoor constructed features are provided in common use and public use areas that serve camping units with mobility features, at least 20 percent, but no less than one, of each type of outdoor constructed feature provided at each location must comply with the applicable technical requirements for the feature.

## **Outdoor Recreation Access Routes in Camping Facilities [F244.5]**

The scoping and technical requirements for ORARs in camping facilities are discussed in the section of this guide on ORARs.



### **RV Parking Spaces Within Camping Units With Mobility Features and RV Pull-Up Spaces at Dump Stations [F244.2.3.2.1 and F244.4]**

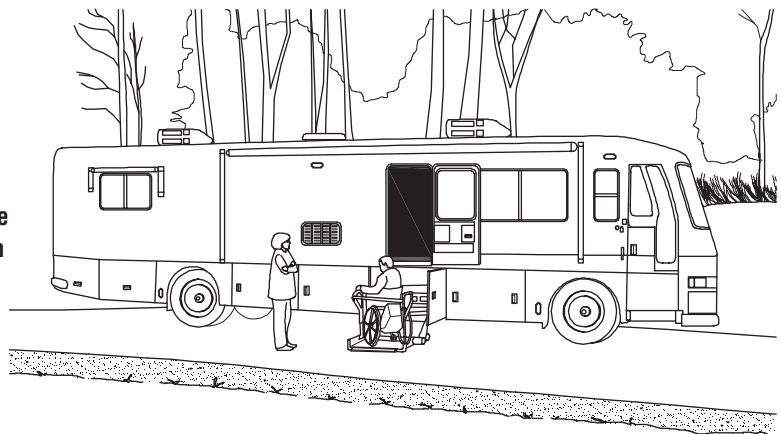
RV parking spaces within camping units with mobility features and RV pull-up spaces at dump stations must comply with the technical requirements for width, surface, and slope of RV spaces.

#### **Width of Spaces [1012.2]**

Parking spaces and pull-up spaces for RVs must be at least 20 feet wide to accommodate RVs equipped with a lift (figure 27). RVs are typically 8 feet wide and require a 9-foot-wide space. RVs equipped with a lift need an additional 8 feet of space on the passenger's side to deploy the lift and to allow people using mobility devices to maneuver onto and off of the lift. Utility hookups for RVs are typically located on the driver's side of the vehicle. An additional 3 feet of space is needed on the driver's side so that people using mobility devices can access the utility hookups.

Where two adjacent parking spaces are provided for RVs, one of the parking spaces is permitted to be narrower, but must be at least 16 feet wide.

**Figure 27—An accessible recreational vehicle with a wheelchair lift.**



### **Surface and Slope [1012.4 and 1012.5]**

The surface of parking spaces and pull-up spaces for RVs must be firm and stable.

When the surface of parking spaces and pull-up spaces for RVs is constructed of asphalt, concrete, or boards, the slope of the parking spaces and pull-up spaces must be no steeper than 1:48 (2 percent) in any direction. When the surface of parking spaces and pull-up spaces for RVs is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

### **Parking Spaces for Vehicles Other Than RVs [F244.2.3.2.2]**

Parking spaces for vehicles other than RVs within camping units with mobility features must comply with the following technical requirements for width, surface, and slope.



#### **Width of Spaces [1012.3]**

Parking spaces for vehicles other than RVs must be at least 16 feet wide. Parking spaces at least 16 feet wide can accommodate vans equipped with a lift or ramp.

When two adjacent parking spaces are provided for vehicles other than RVs, one of the parking spaces is permitted to be at least 8 feet wide.

Parking spaces within camping units with mobility features do not have to be striped or identified by the International Symbol of Accessibility.

#### **Surface and Slope [1012.4 and 1012.5]**

The technical requirements for the surface and slope of parking spaces for vehicles other than RVs are the same as for RV parking spaces.

## Tent Pads and Tent Platforms [1013]



Tent pads and tent platforms are defined spaces with prepared surfaces for setting up and securing tents. No minimum tent pad size is specified because the types of tents commonly used in camping facilities can vary depending on the setting. For example, small tents may be used at a camping facility near a wilderness access point, while large family tents may be used at a more developed camping facility with numerous constructed features. Tent pads and tent platforms must comply with the technical requirements for clear ground space and slope. Tent platforms must also comply with the technical requirements for height.

### Exception [1013.1, Exception]

When a condition for exception does not permit full compliance with a specific provision in the technical requirements for tent pads and tent platforms, the tent pads and tent platforms must comply with the specific provision to the extent practicable. Additional information on the conditions for exceptions is provided in the section of this guide on the conditions for exceptions.

### Clear Ground Space [1013.2]

A clear ground space at least 48 inches wide must be provided on all usable sides of tent pads and tent platforms. This clear ground space enables people using mobility devices to set up and take down a tent (figure 28). The usable sides of tent pads and tent platforms are the sides that can be used for setting up and taking down a tent. All sides of tent pads and tent platforms are generally usable, unless the tent pad or tent platform is located next to a natural feature, such as a rock or tree, that renders a side unusable. The

surface of the clear ground space must be firm and stable, but must also accommodate the use of tent stakes or other devices to secure the tent.



Figure 28—Clear space requirements for tent pads and platforms.

### Slope [1013.3]

When the surface of tent pads, tent platforms, and clear ground spaces is constructed of materials other than asphalt, concrete, or boards, the surface must be no steeper than 1:48 (2 percent) in any direction. When the surface is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

### Floor Height [1013.4]

If the tent platform floor is raised above grade, the floor height must be no more than 19 inches when measured from the clear ground space to the tent platform surface. This height allows people using mobility devices to transfer from the device to the platform surface.



## Camp Shelters [1014]

### Definition [F106.5]

A camp shelter is a partially enclosed structure that provides campers and hikers cover from weather and that does not contain plumbing fixtures or kitchen appliances. Camp shelters are often located on long-distance trails. Camp shelters are not cabins, which are typically larger and must comply with the ABA Standards for transient lodging.



### Exceptions [1014.1, Exceptions 1 and 2]

When a condition for exception does not permit full compliance with a specific provision in the technical requirements for camp shelters, the camp shelter must comply with the specific provision to the extent practicable. Additional information on the conditions for exceptions is provided in the section of this guide on the conditions for exceptions.

The technical requirements for protruding objects in section 309 of the ABA Standards do not apply to camp shelters. Many times, the roof of a camp shelter is sloped or does not permit someone to stand fully erect while in the shelter.

## **Entrance [1014.2]**

Camp shelters must provide either transfer access or roll-in access at the entrance. Providing shelters with roll-in access enables people using mobility devices to also shelter the device.

### **Transfer Access [1014.2.1]**

To provide transfer access at the entrance to a camp shelter, a clear ground space at least 36 by 48 inches positioned for a parallel approach must be provided along the open side of the camp shelter. One full, unobstructed side of the clear ground space must adjoin or overlap an ORAR or trail, as applicable, or another clear ground space. The surface of the clear ground space must be firm and stable. When the surface of the clear ground space is constructed of asphalt, concrete, or boards, the slope of the clear ground space must be no steeper than 1:48 (2 percent) in any direction. When the surface of the clear ground space is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

The camp shelter floor at the entrance or opening must be no higher than 19 inches when measured from the clear ground space. This enables people using mobility devices to pull alongside of the shelter and transfer from the mobility device to the shelter floor. For people using power mobility devices, raised floor shelters that provide transfer access may make it difficult to bring the mobility device into the shelter to protect it from the weather.

### **Roll-in Access [1014.2.2]**

To provide roll-in access into a camp shelter, a level or sloped entry route that complies with the technical requirements for an ORAR or trail, as applicable, must be provided along the open side of the camp shelter. Handrails and edge protection are not required on a sloped entry into a camp shelter but may be useful to people with and without disabilities.

When roll-in access is provided, a turning space at least 60 inches in diameter or a T-shaped space with an arm at least 60 by 36 inches and a base at least 36 inches wide and 24 inches long must be provided inside the camp shelter.

### **Floor [1014.3]**

The floor surface within a camp shelter must be firm and stable. When the floor surface is constructed of asphalt, concrete, or boards, the slope of the floor surface must be no steeper than 1:48 (2 percent) in any direction, regardless of the type of access provided (e.g., sloped entry or transfer access). When the floor surface is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

## Picnic Facilities



### Definition [F106.5]

A picnic facility is a site, or a portion of a site, that is developed for outdoor recreational purposes and contains picnic units.

A picnic unit is an outdoor space in a picnic facility that is used for picnicking and contains at least one outdoor constructed feature.

### Picnic Units With Mobility Features [F245.2.1 and F245.2.2]

When only one or two picnic units are provided in a picnic facility, each picnic unit must provide mobility features. When more than two picnic units are provided in a picnic facility, at least 20 percent, but no less than two, of the picnic units must provide mobility features.

Picnic units with mobility features don't have to be identified by signs. Entities should provide information on the location of picnic units with mobility features on Web sites, in brochures, and at bulletin boards or information kiosks at the picnic facility.

### Alterations and Additions [F245.2.3]

When altering or adding picnic units to an existing picnic facility, the scoping requirements apply only to the picnic units that are altered or added until the required minimum number of picnic units with mobility features is provided at the picnic facility.

### **Example**

A picnic facility has 10 picnic units. None of the picnic units provide mobility features. Ten picnic units are being added to the facility, for a total of 20 units. In order for the facility to comply with the minimum requirement of 20 percent of picnic units providing mobility features, a minimum of two new units, not four, must be constructed to include mobility features. When any of the existing picnic units are altered in the future, at least two of the existing units must include mobility features. The requirements only apply to the picnic units that are altered or added until the required minimum number of picnic units with mobility features is provided at the picnic facility.

When an entity is implementing a transition plan for program accessibility developed pursuant to regulations issued under section 504 of the Rehabilitation Act, which designates specific picnic units to provide mobility features, the entity is not required to provide accessible elements when altering individual elements within picnic units that are not designated to provide mobility features. When all the elements within a picnic unit are altered, the altered picnic unit must provide mobility features until the required minimum number of picnic units with mobility features is provided at the picnic facility.

### **Dispersion [F245.2.4]**

Picnic units with mobility features must provide choices of picnic units comparable to, and integrated with, those available to others.

## **Elements Within Picnic Units With Mobility Features [F245.2.5]**

At least one of each type of element provided within picnic units with mobility features, including outdoor constructed features and parking spaces, must comply with the applicable technical requirements for that element. When more than one of the same type of element (e.g., picnic tables, grills) is provided within a picnic unit with mobility features, at least two of the same type of element must comply with the applicable technical requirements for that element.

## **Outdoor Constructed Features in Common Use and Public Use Areas [F245.4]**

Where outdoor constructed features are provided in common use and public use areas that serve picnic units with mobility features, at least 20 percent, but no less than one, of each type of outdoor constructed feature provided at each location must comply with the applicable technical requirements for the feature.

## **Outdoor Recreation Access Routes in Picnic Facilities**

The scoping and technical requirements for ORARs in picnic facilities are discussed in the section of this guide on ORARs.

## Viewing Areas

### Definition [F106.5]

A viewing area is an outdoor space developed for viewing a landscape, wildlife, or other points of interest.



### Distinct Viewing Locations [F246.2, 1015.2, and 1015.3]

Viewing areas often provide more than one distinct viewing location. For example, a viewing area can provide a distinct viewing location for observing a mountain range and another distinct viewing location for observing a river. Distinct viewing locations within a viewing area don't have to be identified, but may be designated by signs or other markers.

A clear ground space at least 36 by 48 inches that is positioned for either a forward or parallel approach must be provided at each distinct viewing location. One full, unobstructed side of the clear ground space must adjoin or overlap an ORAR or trail, as applicable, or another clear ground space.



Each distinct viewing location must provide a viewing space that is adjacent to the clear ground space through which the point of interest may be viewed. The viewing space must be free and clear of obstructions between 32 and 51 inches above the ground and must extend the full width of the clear ground space.

Guards or similar safety barriers can obstruct the viewing space only to the extent that the obstruction is necessary for the guard or other safety barrier to serve its intended purpose. See-through panels may be used to provide safety while still allowing a person using a mobility device or a person of short stature to view the point of interest.

## Turning Space [1015.4]

A turning space at least 60 inches in diameter or a T-shaped space with an arm at least 60 by 36 inches and a base at least 36 inches wide by 24 inches long (figure 29) must be provided within the viewing area to allow people using mobility devices to turn around.

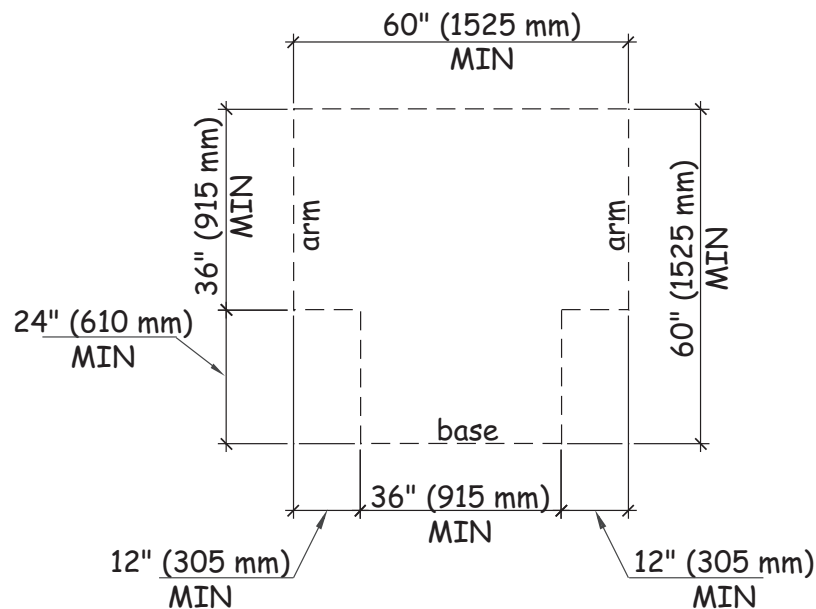


Figure 29—A T-shaped turning space that complies with section 304.3.2.



## Surface and Slope [1015.5 and 1015.6]

The surface of clear ground spaces and turning spaces must be firm and stable. Additional information on firm and stable surfaces is provided in the trails section of this guide.



When the surface of clear ground spaces and turning spaces is constructed of asphalt, concrete, or boards, the slope of the surface must be no steeper than 1:48 (2 percent) in any direction. When the surface is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.

## Outdoor Constructed Features Within Viewing Areas [F246.3]

At least 20 percent, but no less than one, of each type of outdoor constructed feature provided within each viewing area must comply with the applicable technical requirements for the feature. The technical requirements for outdoor constructed features, including viewing scopes, are located in the section of this guide on outdoor constructed features.

## Outdoor Recreation Access Routes in Viewing Areas [F246.4]

At viewing areas not located along trails, at least one ORAR must connect accessible parking spaces or other arrival points that serve the viewing area with accessible elements, spaces, and facilities provided within the viewing area. Additional information on the scoping and technical requirements for ORARs at viewing areas are discussed in the section of this guide on ORARs.

## Beach Access Routes



Beach access routes allow pedestrians to cross a beach so that they can play, swim, or participate in other beach- or water-related activities. A beach access route is a continuous, unobstructed path that crosses the surface of the beach and provides pedestrians access to the water. Beach access routes are not required where pedestrian access to the beach is not allowed.

Beach access routes can be permanent or removable. Removable beach access routes may be an option where restrictive permits are issued in coastal and shoreline areas, where seasonal tides or high flows may remove or damage a permanent structure, or in areas where the beach erodes or builds up quickly each season and causes a permanent beach access route to become inaccessible. Removable beach access routes can be moved to a protected storage area during storms and other periods when the routes are subject to damage or loss.

There are no scoping requirements for outdoor constructed features provided on beaches. Outdoor constructed features provided on beaches don't have to be connected by beach access routes, ORARs, trails, or accessible routes. Whenever possible, providing access to outdoor constructed features on a beach gives people with disabilities more independence and opportunities to participate in beach- or water-related activities.

## Scoping Requirements

### Facilities Serving Beaches [F248.1.1]

Beach access routes are required when an entity that administers or manages a beach constructs or alters any circulation paths, parking facilities, toilet facilities, or bathing facilities that serve the beach. The entity is not required to spend more than 20 percent of the costs of constructing or altering these facilities to provide beach access routes.



### Beach Nourishment [F248.1.2]

Beach access routes are required when the entity that administers or manages the beach undertakes a beach nourishment project. The entity is not required to spend more than 20 percent of the costs of a beach nourishment project to provide beach access routes.

### Minimum Number of Beach Access Routes [F248.2]

At least one beach access route must be provided for each one-half mile of beach shoreline administered or managed by the same entity. The number of beach access routes is not required to exceed the number of pedestrian access points provided to the beach by the entity. Pedestrian access points to a beach include parking facilities, dune crossings, and stairways or ramps leading from boardwalks to the beach. In high-density population areas, entities should consider providing beach access routes more frequently than the minimum of every one-half mile to prevent people with disabilities from traveling extensive distances to access the beach.

## **Location [F248.3]**

Beach access routes must coincide with or be located in the same general area as pedestrian access points that serve the beach.

## **Technical Requirements [1018 and 1018.1, Exception 3]**

The technical requirements for beach access routes include specific provisions for connections, the surface, clear width, obstacles, openings, running slope, cross slope, resting intervals, protruding objects, and dune crossings. Removable beach access routes are not required to comply with the specific provisions for running slope, cross slope, resting intervals, and dune crossings.

## **Using the Beach Access Route Exceptions [1018.1, Exceptions 1 and 2]**

When a condition for exception does not permit full compliance with a specific provision in the technical requirements on a portion of a beach access route, that portion of the route must comply with the specific provision to the extent practicable. After applying all the applicable conditions for exceptions to a beach access route, if an entity determines that it is impracticable to provide a beach access route that meets the technical requirements, then a compliant beach access route is not required. Additional information on the conditions for exceptions, including documenting use of the exceptions on portions of a beach access route and notifying the Access Board when it is impracticable to provide an entire beach access route, is provided in the section of this guide on the conditions for exceptions.

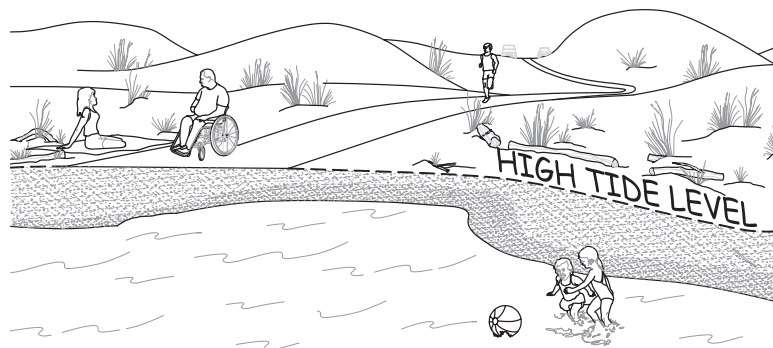
### Removable Beach Access Route Requirements

Removable beach access routes are not required to comply with the specific requirements for running slope, cross slope, resting intervals, and dune crossings.

### Connections [1018.2]

Beach access routes must connect an entry point to the beach to the high tide level at tidal beaches (figure 30); the mean high water level at river beaches; and the normal recreation water level at lake, pond, and reservoir beaches. Whenever possible, providing a beach access route that extends into the water will allow people to remain in their mobility devices and to transfer directly into the water.

**Figure 30—The high tide level at a coastal beach.**

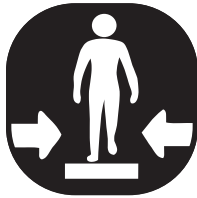


### Surface [1018.3]

The surface of beach access routes and resting intervals must be firm and stable. Sand is not a firm and stable surface. Additional information on firm and stable surfaces is provided in the trails section of this guide.



## Clear Width [1018.4]



The clear width of beach access routes must be a minimum of 60 inches to enable people using mobility devices to pass people traveling in the opposite direction without veering off the firm and stable surface into the sand. The clear width of beach access routes that are not removable can be reduced to a minimum of 48 inches at dune crossings.

Beach wheelchairs are not a substitute for providing beach access routes. Beach wheelchairs have large, wide wheels that can roll across sand without sinking but do not provide independent access.

When gates or barriers are installed to control beach access, the gates or barriers must allow the passage of mobility devices, including beach wheelchairs. Gates or barriers should provide clear openings at least 48 inches wide for beach wheelchairs.

## Obstacles [1018.5]

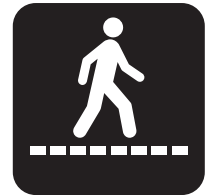
Changes in the vertical alignment of boards or the connection points for removable sections of beach access routes can be obstacles to beach access. When beach access routes are constructed of concrete, asphalt, or boards, obstacles cannot exceed one-half inch in height at their highest point.



When beach access routes are constructed of materials other than concrete, asphalt, or boards, obstacles cannot exceed 1 inch in height at their highest point. Where possible, separate obstacles that cross the entire beach access route by at least 48 inches so that people using mobility devices can fully cross one obstacle before confronting another.

## Openings [1018.6]

Openings are gaps in the surface of a beach access route. Gaps, including spaces between the planks on a boardwalk and drainage holes in temporary or permanent surfaces, that are big enough for wheels, canes, or crutch tips to drop through or become trapped in are potential hazards.



Openings in the surface of beach access routes and resting intervals must be small enough so that a sphere more than one-half inch in diameter cannot pass through. Where possible, elongated openings should be placed perpendicular, or as close to perpendicular as possible, to the dominant direction of travel or outside of the clear width of the beach access route.

## Running Slope [1018.7.1]

The running slope of any segment of a beach access route must be no steeper than 1:10 (10 percent). When the running slope of a segment of beach access route is steeper than 1:20 (5 percent), the maximum length of the segment is specified in table 6, and a resting interval must be provided at the top and bottom of each segment.



Gradual running slopes provide more independent use for people with disabilities. Resting intervals must be provided more frequently when running slopes are more severe.

Table 6—Maximum Running Slope and Segment Length		
Running Slope of Beach Access Route Segment		Maximum Length of Segment
Steeper Than	But Not Steeper Than	
1:20 (5%)	1:12 (8.33%)	50 feet
1:12 (8.33%)	1:10 (10%)	30 feet

### **Cross Slope [1018.7.2]**

When beach access routes are constructed of asphalt, concrete, or boards, the cross slope must be no steeper than 1:48 (2 percent). When beach access routes are constructed of materials other than asphalt, concrete, or boards, cross slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.



### **Resting Intervals [1018.8]**

Resting intervals are level areas that provide an opportunity for people to stop after a steep segment and recover before continuing on. Resting intervals are required between beach access route segments any time the running slope exceeds 1:20 (5 percent).

Resting intervals must be at least 60 by 60 inches. When the surface of the resting interval is constructed of asphalt, concrete, or boards, the slope of the resting interval must be no steeper than 1:48 (2 percent) in any direction. When the surface of the resting interval is constructed of materials other than asphalt, concrete, or boards, slopes no steeper than 1:20 (5 percent) are allowed when necessary for drainage.



### Protruding Objects [1018.9]

Objects that protrude into the clear width of beach access routes and resting intervals can pose hazards to people who are blind or have low vision. Constructed elements on beach access routes and resting intervals must comply with the technical requirements for protruding objects in section 307 of the ABA Standards (figure 31). Signs and other post-mounted objects are examples of constructed elements that, if not located correctly, can be protruding objects.

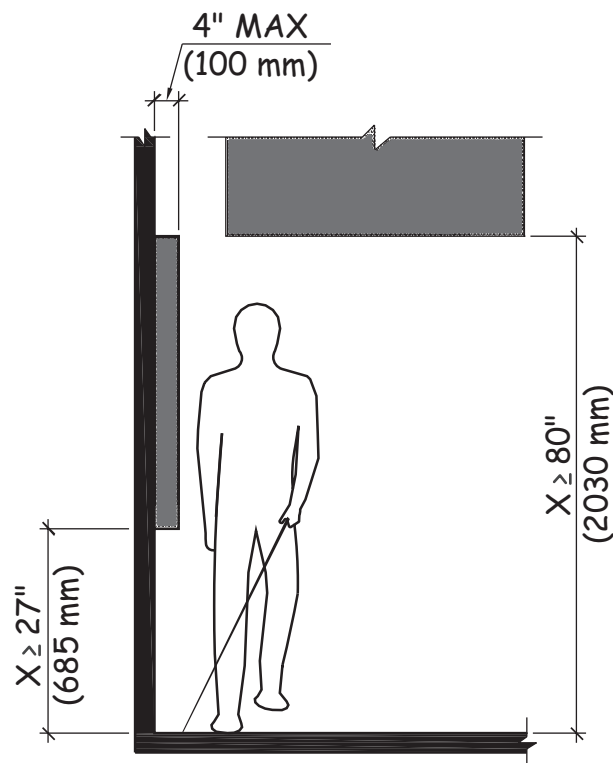


Figure 31—Constructed elements must comply with the technical requirements for protruding objects in section 307.2.

### Dune Crossings [1018.10]

A dune crossing that is part of a beach access route and has a slope that exceeds 1:20 (5 percent) must also provide handrails that comply with section 505 of the ABA Standards and must also provide edge protection. The handrails must be continuous at a height of 34 to 38 inches above the walking surface along both sides of the dune crossing (figure 32). Edge protection in the form of a curb or barrier must prevent the passage of a 2-inch sphere where any portion of the sphere is within 2 inches of the dune crossing surface. The technical requirements for dune crossings do not address the vertical pickets provided in some handrail designs.

When a dune crossing is part of a beach access route, the clear width of the beach access route may be reduced from a minimum of 60 inches to a minimum of 48 inches. Where a removable beach access route is provided as a dune crossing, the beach access route technical provisions for running slope, cross slope, and resting intervals do not apply. When the running slope of a non-removable segment of a dune crossing is steeper than 1:20 (5 percent), the maximum length of the segment is specified in table 6, and a resting interval must be provided at the top and bottom of each segment.

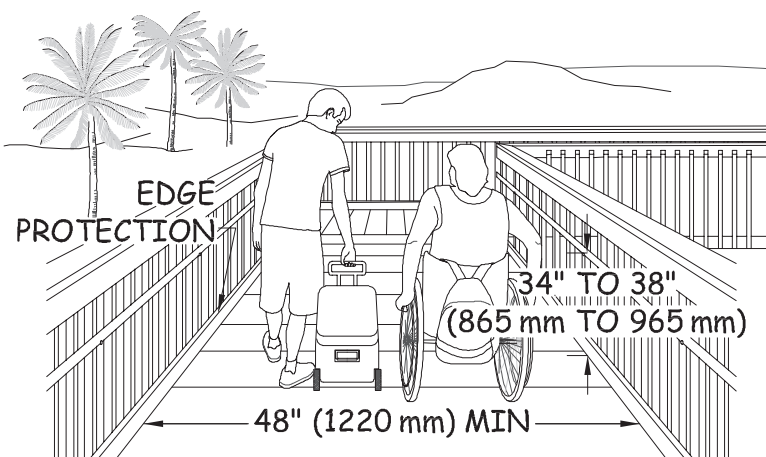


Figure 32—Minimum width and handrail height requirements for dune crossings.

## Appendixes

- **Key Differences Between Routes**
- **Notification Forms**
  - ♦ **Entire Trail Exemption Form [1017.2, Exception 2]**
  - ♦ **Entire Beach Access Route Exemption Form [1018.2, Exception 2]**

## Key Differences Between Routes

Determining the type of route required is often a challenging exercise. The following descriptions and review of the technical requirements for each type of route provides a quick reference to aid with this process.

**Accessible Routes**—An accessible route is a continuous, unobstructed path that connects all accessible elements and spaces of a building or facility. Interior accessible routes may include corridors, floors, ramps, elevators, lifts, and clear floor space at fixtures. Exterior accessible routes may include accessible parking space access aisles, curb ramps, crosswalks at vehicular ways, walks, ramps, and platform lifts.

**Pedestrian Access Routes**—A pedestrian access route, often called a sidewalk, is located in a public right-of-way and typically is parallel to a roadway. Consequently, sidewalk grades (running slopes) must generally be consistent with roadway grades so that they fit into the right-of-way. Sidewalks are designed for pedestrian transportation and are not designed for bicycles or other recreational purposes.

**Pedestrian Trails**—A trail typically is not parallel to a roadway and is designed primarily for recreational purposes. Trails are not necessarily part of an infrastructure connecting elements or facilities, but typically are designed to provide a recreational experience. Trails may also be used by multiple types of users, but most are not designed for bicycles, nor do they have a transportation purpose.

**Outdoor Recreation Access Routes**—An outdoor recreation access route (ORAR) is a continuous, unobstructed path that is intended for pedestrian use and that connects accessible elements, spaces, and facilities within camping and picnic facilities and at viewing areas and trailheads only. ORARs cannot be used at other types of facilities, such as educational campuses, office parks, or theme parks.

**Beach Access Routes**—A beach access route is a continuous, unobstructed path that crosses the surface of the beach to allow pedestrians to play, swim, or participate in other beach-, shoreline-, or water-related activities. A beach access route may be a permanent or removable route. Beach access routes typically coincide with or are located in the same general area as pedestrian access points to the beach. Beach access routes are not required where pedestrian access to the beach is not permitted.

**Shared-Use Paths**—A shared-use path is part of a transportation system in a public right-of-way that provides off-road routes for a variety of users. Even where the primary users may be bicyclists, skaters, or equestrians, shared-use paths typically are designed to serve pedestrians, including people using mobility devices such as manual or motorized wheelchairs. In addition to transportation uses, shared-use paths often provide recreational experiences. They may extend or complement a roadway network. For example, they may supplement on-road bike lanes, shared roadways, bike boulevards, and paved shoulders. Shared-use path design is similar to roadway design but on a smaller scale and for lower speeds. Whether located within a highway right-of-way, provided along a riverbank, or established over natural terrain within an independent right-of-way, shared-use paths differ from sidewalks and trails in that they are designed for a variety of users and serve both recreational and transportation purposes.

The following table highlights the key elements of design for different route types.

Route Characteristics	
Pedestrian Route Type	Key Elements of Design Intent
Accessible route (AR)	Connects accessible elements and spaces of a building or facility on a site
Sidewalk—pedestrian access route (PAR)	Parallel to roadway  Designed for pedestrians (not bicycles)  Sometimes part of the roadway
Trail	Designed for the “recreation experience”  Does not connect elements and spaces on a site  Generally includes a trailhead  Has limited to no transportation function
Outdoor recreation access route (ORAR)	Connects outdoor constructed features and spaces within picnic and camping facilities, viewing areas, and trailheads <b>only</b>
Beach access route (BAR)	Crosses the surface of the beach to the shoreline  Coincides with or is located in the same general area as pedestrian access points to the beach
Shared-use path (SUP)	Intended for multi-use  Bicycle/transportation focus  Machined, layered surface (improved)  Located in either an “independent corridor” or public right-of-way

The following tables highlight the technical requirements for slope, width, and surface for different route types.

Technical Requirements for Slope			
If Running Slope of Segment is Steeper Than	But Running Slope of Segment is Not Steeper Than	Maximum Length of Segment	Cross Slope
Trail			
0:00 (0%)	1:20 (5%)	any length	Concrete, asphalt, boards—1:48 (2%)
1:20 (5%)	1:12 (8.33%)	200 feet	All other surfaces when necessary for drainage —1:20 (5%)
1:12 (8.33%)	1:10 (10%)	30 feet	
1:10 (10%)	1:8 (12%)	10 feet	
ORAR			
0:00 (0%)	1:20 (5%)	any length	Concrete, asphalt, boards—1:48 (2%)
1:20 (5%)	1:12 (8.33%)	50 feet	All other surfaces when necessary for drainage —1:20 (5%)
1:12 (8.33%)	1:10 (10%)	30 feet	
BAR			
0:00 (0%)	1:20 (5%)	any length	Concrete, asphalt, boards—1:48 (2%)
1:20 (5%)	1:12 (8.33%)	50 feet	All other surfaces when necessary for drainage —1:20 (5%)
1:12 (8.33%)	1:10 (10%)	30 feet	

Technical Requirements for Width and Surface		
	Minimum Width	Surface
AR	36 inches	Firm, Stable, Slip Resistant
PAR	48 inches	Firm, Stable, Slip Resistant
Trail	36 inches	Firm and Stable
ORAR	36 inches	Firm and Stable
BAR	60 inches	Firm and Stable
SUP	No requirement	Firm, Stable, Slip Resistant

## Notification Forms

Documentation is required where a condition for exception prohibits full compliance with a specific technical requirement. The documentation must include the reason that full compliance could not be achieved and should be retained with the project records. In addition to the reason for the exception, documentation should include the date the decision was made and the names and positions of the individuals making the decision.

Where extreme or numerous exceptions make it impracticable to provide a newly constructed or altered trail or beach access route that meets the technical requirements, the standards provide an exemption for the entire trail or beach access route (see 1017.1, exception 2 and 1018.1, exception 2). In these rare cases, an explanation of the conditions that resulted in the determination that it was impracticable for the entire trail or beach access route to comply must be recorded and the documentation must be retained with the records for that project. A copy must also be sent to the Access Board (see F201.4.1).

The Access Board has developed sample notification forms with assistance from the accessibility program managers for the Federal land management agencies. These forms can be used to notify the Access Board when an entire trail or beach access route is exempted. The Access Board plans to monitor situations where the exceptions for trails and beach access routes result in exempting an entire trail or beach access route. The notification forms do not require approval or any other action on the part of the Access Board or the Federal agency. The Access Board will use the information provided by the Federal agencies to develop additional guidance on exempting entire trails and beach access routes. Federal agencies are encouraged to seek technical assistance from the Access Board at [outdoor@access-board.gov](mailto:outdoor@access-board.gov) when considering exempting an entire trail or beach access route.

The trail exemption form can be downloaded at [https://www.access-board.gov/images/guidelines\\_standards/Recreation\\_Facilities/Outdoor\\_Developed\\_Areas/trail\\_exemption\\_notice.pdf](https://www.access-board.gov/images/guidelines_standards/Recreation_Facilities/Outdoor_Developed_Areas/trail_exemption_notice.pdf). The beach access route form can be downloaded at [https://www.access-board.gov/images/guidelines\\_standards/Recreation\\_Facilities/Outdoor\\_Developed\\_Areas/beach\\_route\\_exemption\\_notice.pdf](https://www.access-board.gov/images/guidelines_standards/Recreation_Facilities/Outdoor_Developed_Areas/beach_route_exemption_notice.pdf). The forms must be downloaded before they can be filled out.



## ENTIRE TRAIL EXEMPTION FORM (1017.1 EXCEPTION 2)

This form can be used to notify the U.S. Access Board when a Federal agency determines in accordance with 1017.1, Exception 2 that it is impracticable for an entire trail to comply with the technical requirements in 1017.1. Federal agencies are encouraged to seek technical assistance from the Access Board when considering exempting an entire trail.

Name of Trail:

Check all boxes that apply.

☐

FTDS Designed Use of Pedestrian/hiker

Location: (FLMA unit, nearest town, city,  
county, state)

☐

Connects directly to trailhead or accessible trail that complies with all of the technical requirements in 1017 without any exceptions.

☐

New Construction

☐

Alteration

Length of Trail:

(miles, km, feet) (loop, round-trip, one way)

The U.S. Access Board will consider these conditions as a basis for determining in accordance with 1017.1 Exception 2 that it is impracticable for an entire trail to comply with the technical requirements in 1017.

Check the box beside conditions that apply to the trail.

☐

Combination of running slope and cross slope exceeds 40 percent for over 20 feet

☐

Trail obstacle 30 inches high or more runs across the full tread width of the trail

☐

Trail surface is neither firm nor stable for a distance of 45 feet or more

☐

Tread width is less than 12 inches wide for a distance of 20 feet or more

☐

15 percent or more of the trail does not fully comply with the technical requirements in 1017

Any additional conditions that render it impracticable for a trail to comply with the technical requirements in 1017 should be described below under Additional Information.

**Additional Information:**

**Alternatives Considered:**

Name: \_\_\_\_\_

Agency: \_\_\_\_\_

Position: \_\_\_\_\_

Contact Information

Site Name: \_\_\_\_\_

(email, phone): \_\_\_\_\_

Date: \_\_\_\_\_



**U.S. Access Board, 1331 F. Street, NW, Suite 1000, Washington, DC 20004-1111**

**800-872-2253 (v) 202-272-0082 (TTY)**

**[www.access-board.gov](http://www.access-board.gov)**

## ENTIRE BEACH ACCESS ROUTE EXEMPTION FORM (1018.1 EXCEPTION 2)

This form can be used to notify the U.S. Access Board when a Federal agency determines in accordance with 1018.1 Exception 2 that it is impracticable to provide a beach access route complying with the technical requirements in 1018.1. Federal agencies are encouraged to seek technical assistance from the Access Board when considering exempting an entire beach access route.)

Name or Location of Beach Access Route:

Check boxes that apply.

☐

New Construction

Length of Beach

Access Route:

(miles, km, feet)

☐

Alteration

Any conditions that render it impracticable to provide a beach access route complying with the technical requirements in 1018 should be described below under Additional Information.

Additional Information:

--

Alternatives Considered:

--

Name: \_\_\_\_\_

Agency: \_\_\_\_\_

Position: \_\_\_\_\_

Contact Information

Site Name: \_\_\_\_\_

(email, phone): \_\_\_\_\_

Date: \_\_\_\_\_



**U.S. Access Board, 1331 F. Street, NW, Suite 1000, Washington, DC 20004-1111**

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## Notes

## Notes

## More Information

Copies of the final rule for federal outdoor developed areas and technical assistance is available from the US Access Board at **[www.access-board.gov](http://www.access-board.gov)** or **by calling 1-800-872-2253 (voice) 1-800-993-2822 (TTY).**



UNITED STATES ACCESS BOARD

1331 F Street, NW, Suite 1000  
Washington, DC 20004-1111

voice 800-872-2253  
tty 800-993-2822

[www.access-board.gov](http://www.access-board.gov)



## ADA ADVISORY BOARD 2023 Calendar

	AGENDA DEADLINE	MEETING	CITY COUNCIL MEETING FOR REPORT*	ANNUAL TOPICS/EVENTS
<b>JANUARY</b>	No Meeting Scheduled			
<b>FEBRUARY</b>	Wednesday 2/1 5:00 p.m.	Thursday 2/09 5:00 p.m.	Monday 2/13 6:00 p.m.	ADA Budget Review *may not be applicable during non-budget years Development of Annual Strategic Plans & Goals
<b>MARCH</b>	No Meeting Scheduled			
<b>APRIL</b>	Wednesday 4/5 5:00 p.m.	Thursday 4/13 5:00 p.m.	Monday 4/24 6:00 p.m.	<ul style="list-style-type: none"> <li>Review first draft Parks, Play Areas &amp; Campgrounds Transition Plan</li> <li>ADA Budget Review FY23-FY25</li> </ul>
<b>MAY</b>	Wednesday 5/3 5:00 p.m.	Thursday 5/11 5:00 p.m.	Monday 5/22 6:00 p.m.	<ul style="list-style-type: none"> <li>Engage in Education and outreach activities for ADA Compliance in Local businesses to promote Accessible Homer</li> <li>Schedule Site Audits for Trails</li> <li>Draft Strategic Plan for 2024</li> </ul>
<b>JUNE</b>	Wednesday 6/7 5:00 p.m.	Thursday 6/8 5:00 p.m.	Monday 6/12 6:00 p.m.	<ul style="list-style-type: none"> <li>2nd Draft Parks, Play Areas &amp; Campgrounds Transition Plan</li> <li>Finalize Strategic Plan</li> </ul>
<b>JULY</b>	Wednesday 7/5 5:00 p.m.	Thursday 7/13 5:00 p.m.	Monday 7/24 6:00 p.m.	<ul style="list-style-type: none"> <li>Term Expiration Notices/Reapplications Sent Out</li> <li>Site Audits for Trails</li> </ul>
<b>AUGUST</b>	Wednesday 8/2 5:00 p.m.	Thursday 8/10 5:00 p.m.	Monday 8/14 6:00 p.m.	<ul style="list-style-type: none"> <li>Reapplications Due</li> <li>Continued Site Visits for City Trail Transition Plan,</li> <li>Review Draft Transition Plan Parks</li> </ul>
<b>SEPTEMBER</b>	No Meeting Scheduled			
<b>OCTOBER</b>	Wednesday 10/4 5:00 p.m.	Thursday 10/12 5:00 p.m.	Monday 10/23 6:00 p.m.	<ul style="list-style-type: none"> <li>Approve Meeting Schedule for Upcoming Year</li> <li>Board Training by City Clerk</li> </ul>
<b>NOVEMBER</b>	Wednesday 11/1 5:00 p.m.	Thursday 11/9 5:00 p.m.	Monday 11/13 6:00 p.m.	<ul style="list-style-type: none"> <li>Review Strategic Plan &amp; Goals for 2024</li> </ul>
<b>DECEMBER</b>	No Meeting Scheduled			

\*The Board's opportunity to give their report to City Council is scheduled for the Council's regular meeting following the Board's regular meeting, under Agenda Item 8 – Announcements/ Presentations/ Borough Report/Commission Reports.



# City of Homer

[www.cityofhomer-ak.gov](http://www.cityofhomer-ak.gov)

## Office of the City Manager

491 East Pioneer Avenue  
Homer, Alaska 99603

[citymanager@cityofhomer-ak.gov](mailto:citymanager@cityofhomer-ak.gov)  
(p) 907-235-8121 x2222  
(f) 907-235-3148

### Memorandum

TO: Mayor Castner and Homer City Council  
FROM: Rob Dumouchel, City Manager  
DATE: April 19, 2023  
SUBJECT: City Manager's Report for April 24, 2023 Council Meeting

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#### 2022 Visioning Update

I have been providing roughly quarterly updates to the Council priorities set in the 2022 visioning work sessions at the Pratt Museum. Accompanying this report is a memo providing updates on all priority items from 2022 and some additional commentary on projects that carried over from 2020.

#### Coast Guard City Update

We have received word that the Commandant of the Coast Guard officially intends to approve our application to become a Coast Guard City. We are waiting for the end of the 30-day Congressional notification period to expire before we can become official. I expect that we should have exciting news to share at the second Council meeting in May.

#### Goodbye to Rick Abboud, City Planner

After 15 years, and upwards of 300 Planning Commission meetings with the City of Homer, Rick is moving on to a new position with York County, South Carolina which is part of the Charlotte metropolitan area. Rick joined the City in 2008 after serving as the planning director for Bethel, AK. He jumped straight into the big job of wrapping up the 2008 Homer Comprehensive Plan and shepherding it through the Planning Commission, City Council, and the Kenai Peninsula Borough Assembly. During Rick's tenure with the City he has worked with staff, Planning Commission, and Council on projects that have touched every section of Title 21. He collaborated to make additions to code which include solutions relating to steep slope development, grading and filling, over slope development, wind energy, telecommunication towers, storm water, creation of the East End Mixed use District, allowances for accessory dwellings, marijuana businesses, flood hazard ordinances and maps, coastal setbacks, and the creation of the medical district. Working with legal counsel, Rick has diligently defended numerous decisions of the Planning Commission over the years – including some that have been sustained in the Alaska Supreme Court and have been used as subject matter in an Alaska Chapter of the American Planning Association Conference session titled "how to do it right". Rick has also been very active with MAPP, Rotary, the Kenai Peninsula Homelessness Coalition, and served a stint as interim-City manager before my arrival in Homer. Being City Planner is a tough job, and Rick has done a good job. I applaud Rick for his efforts here in Homer and wish him the best in York County.

In preparation for Rick's departure, I have Special Projects Coordinator Ryan Foster on deck to serve as interim-City Planner. Ryan has previously served as the Planning Director for the City of Kenai and as Principal Planner for Los Alamos County, New Mexico.

### State Legislature Check-Ins

After visits to Council by both Senator Stevens and Representative Vance, staff conducted follow up Zoom meetings arranged by our lobbyists at J&H Consulting. We largely talked about the Harbor Expansion study, the Municipal Harbor Grants Program, and each elected official gave us their analysis of the ongoing state budget process.



### Residential Fuel Spill

On April 2, 2023 Homer Fire received a call from Enstar Energy who was reporting a smell of fuel in the area of Crittenden and Hidden Way. At about 11:45am Chief Kirko responded to the area and met with an Enstar employee who stated he had been receiving reports from residents in the neighborhood for the past few days of a potential gas leak. Enstar had inspected the homes and utilities in the area and found all systems to be secure and functioning properly. As the Chief investigated the area he detected the scent of diesel in the air, however there was no obvious visual sign of a spill. Chief Kirko was able to track the scent to storm drain and then followed it back to a property on Hidden way. At that time the spill was reported the City's Water & Sewer team, Alaska Department of Environmental Conservation (DEC), and the Coast Guard. At approximately 12:20pm HVFD and the Water & Sewer team responded with crews to place absorbent boom and pads at culverts and storm drains to help mitigate the spread of fuel toward the bay. At approximately 1:00pm Coast Guard, MST2 John Fanelli arrived and conducted a survey and obtained witness statements. Through the statements and the investigation of the site it was determined that the fuel spilled was home heating fuel from a 300-gallon home heating tank. The total amount leaked is estimated at between 75-150 gallons. Chief Kirko met with Jade Gamble of DEC on site and briefed her on our mitigation activities. We also met with the son of the home owner to discuss the cleanup procedures and what the home owner responsibilities are when this happens. At this point, the Fire Department is no longer directly involved keeping this case open and DEC has taken over case management. Moving forward, we're going to try to take this event as an opportunity for public education regarding residential oil storage and spill

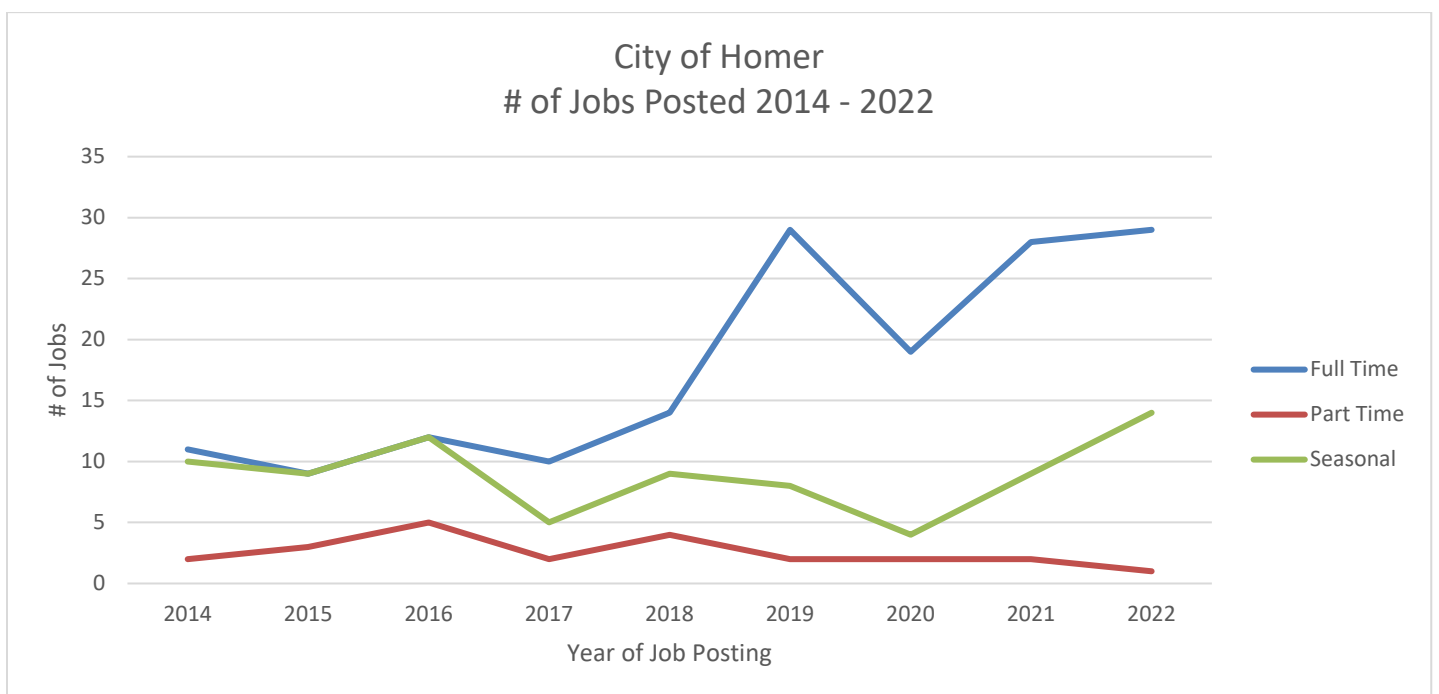
mitigation/response. Chief Kirko and Assistant to the City Manager Bella Vaz will be collaborating on that topic in the coming weeks.

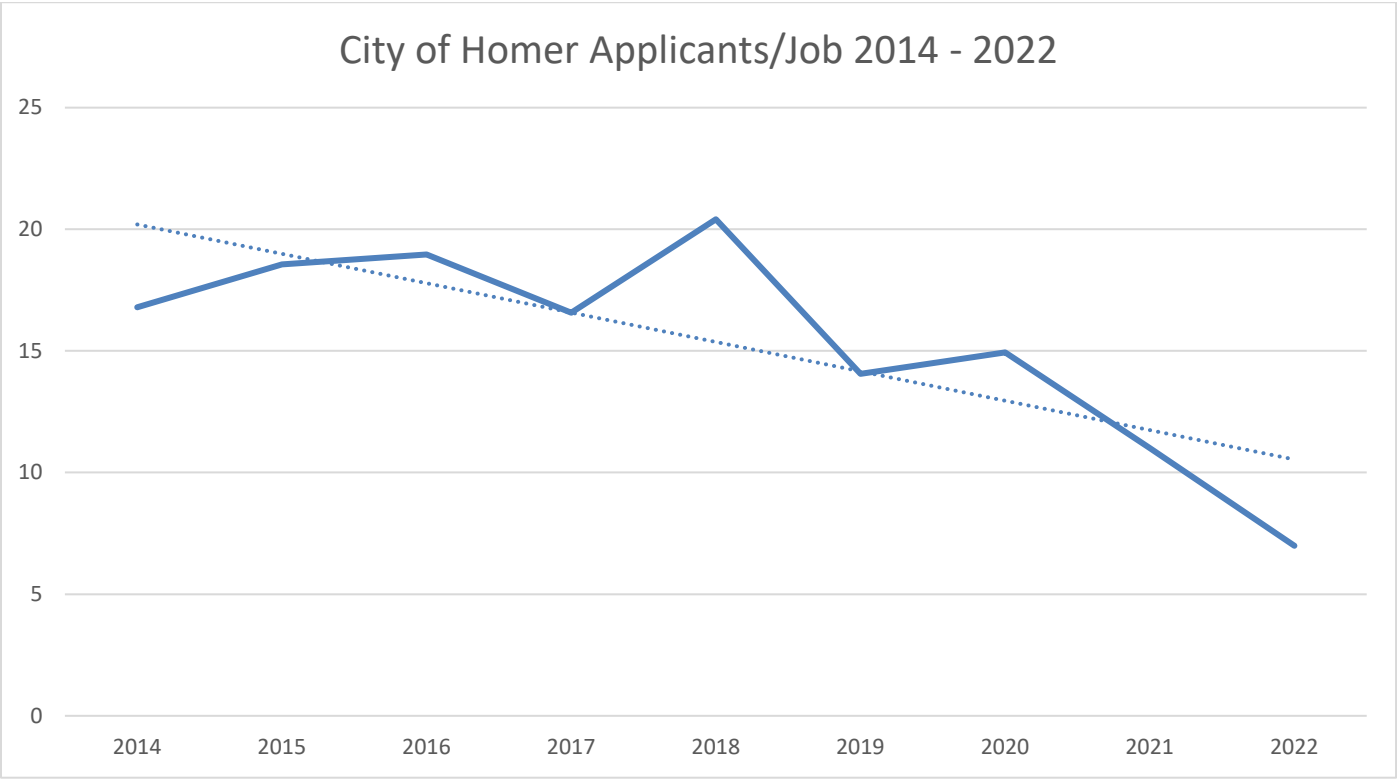
### Guiding Growth Conversation Series

Guiding Growth is a conversation series developed by community members to explore growth opportunities and shared community values. Participating community members are unaffiliated and anyone is invited to help brainstorm, implement, and participate in the discussions. Through civic discourse, good conversation, and relevant resources the group hopes to help guide the vision of how the Homer community develops. The first discussion is on April 29, 4:00 pm at Christian Community Church. Mercedes Harness will present newspaper clippings about how Homer was growing forty years ago--and how many of our concerns and shared values have remained consistent. Tom Kizzia, who edited the Homer News in the mid-1970s, will provide deeper context. Community members will be invited to share their insights and ideas.

### Job Applicant Data

I occasionally field questions from Council, other Cities, and local employers about how successful our recruiting efforts are at this time. The general perception is that the volume of job applicants has decreased and it has been more difficult to hire quality candidates. Wanting to see if reality backs that perception, I pulled some data from our online job application system and saw a declining volume of interest. I then turned it over to Special Projects Coordinator David Parker for a deeper dive. A couple trends have emerged. Since about 2019, we've been posting more job opportunities. We have also seen fewer applicants per listing starting in 2019. As one would anticipate, there are a large number of factors that play into the dynamics that drive interests in municipal positions – the types of jobs available, the pay ranges, the local housing market, how aggressively we market positions, etc. Despite lower numbers, we have had some very successful recruitments which landed talented and experienced new hires. At the same time, some positions have taken many months to fill with a qualified and interested person. If Council approves the proposed funding of a review of Personnel Regulations and Salary Schedules included in the Capital Budget, we will be able to dig deeper into potential internal barriers to recruitment and retention and develop solutions that will assist our ongoing modernization and improvement of human resources practices.







# City of Homer

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## Office of the City Manager

491 East Pioneer Avenue  
Homer, Alaska 99603

[citymanager@cityofhomer-ak.gov](mailto:citymanager@cityofhomer-ak.gov)

(p) 907-235-8121 x2222

(f) 907-235-3148

## Memorandum

TO: Mayor Castner and Homer City Council

FROM: Rob Dumouchel, City Manager

DATE: April 17, 2023

SUBJECT: 2022 Council Visioning Follow Up #4

Staff continues to work diligently to achieve 2022 Council visioning priorities. This is the fourth installment of what are intended to be roughly quarterly updates on project progress.

### 2022 Council Priorities

Fiscal Policy Improvements					
Comprehensive Plan Fast Forward		Zoning Code Modernization		Building Code Adoption and Implementation	
Expansion of Sidewalks and Trails			Water and Sewer Expansion		
Housing Challenges			Emergency Preparedness and Training		
Public Safety Conversations	Volunteer Action Plan	Coast Guard City Designation	Harbor Float Replacements	Recreation Priorities and Planning	Cybersecurity Improvements

- **Fiscal Policy Improvements** – We have developed and introduced the FY24/25 Operating and Capital Budgets and the FY22 Audit is nearly complete. Administration is using the budget as a device to provide a lot of baseline information to Council on different funds and future needs of the City. I see this as the foundation for the fiscal planning objective. The next layer is inventorying and expanding our fiscal policies as they relate to the various funds, our desires for reserve amounts, what funds can be used for, etc. Matched with that is a continued improvement to reporting methods. As financial policy improvements underpin virtually everything we do here at the City, we're trying to take a very comprehensive approach to this process.
- **Comprehensive Plan Fast Forward** – This project is being re-imagined. Council approved Ordinance 23-11 which appropriated \$650,000 for the comprehensive plan and zoning code update. That item was subsequently line item vetoed to reduce the appropriation to \$250,000. This dramatically changes what we are capable of achieving in relation to this Council priority. Standby for what will likely be a summer work session to get Council buy-in for the path forward.

- **Zoning Code Modernization** – This item was split into two pieces, pre- and post-comprehensive plan. With the comprehensive plan project up for re-imaging, the pathway of the zoning code modernization is somewhat unclear. We have had successes with what were intended to be pre-comprehensive plan items which included the West Hill rezone (Ordinance 22-35) and the conditional use permit reform project (Ordinance 22-68(A)). Ordinance 22-68(A) has been challenged in Court and our legal team is addressing that complaint.
- **Building Code Adoption and Implementation** – We submitted an application in late December for the BRIC (Building Resilient Infrastructure and Communities) Grant Program through FEMA as a possible source of funding to get our building program off the ground. The response to this application, not expected until June at the earliest, will have a large influence on how we proceed to the next steps. In the meantime, we're also working on permitting concepts for grading, dirt work, and burning which impact the same general stakeholders.
- **Housing Challenges** – The Special Projects Team had been planning a housing forum of some sorts when we became aware of some other entities within the community looking to do the same thing. We joined forces with South Peninsula Hospital Foundation, Kenai Peninsula Economic Development District, Homer Chamber of Commerce, Choosing Our Roots, and MAPP to host a community conversation facilitated by Denali Daniels. Upwards of 100 people came to the meeting and others completed associated surveys. I expect Council to receive a report from the event soon. More information available here: <https://ddaalaska.com/homer-housing-solutions/>
- **Water and Sewer Expansion** – Public Works has been putting significant effort into the general concept of expanding our utilities for the last two years. Expect more detailed updates in the future as we work to secure funding for significant expansion opportunities. We especially need to have future discussions regarding the financing of water and sewer expansions. We currently have access to frequent loan opportunities through ADEC. Special Projects has been looking at potential updates to the special assessment district process as well.
- **Expansion of Sidewalks and Trails** – The Main Street sidewalk has been completed and the Master Transportation Plan has made significant progress. In November, Council passed Ordinance 22-42(S-3) which creates new specifications for when new streets are required to provide for non-motorized transportation. The proposed FY24/25 Capital Budget has \$1.7M for the construction of a Ben Walters Sidewalk. We have also heard that our State Transportation Alternatives Program (TAP) grant application for the Homer All Ages and Abilities Pedestrian Path (supported by Resolution 23-013) is a big hit with AKDOT and has a high likelihood of success.
- **Emergency Preparedness and Training** – The All Hazards Plan has been completed and accepted by Council. We've used that document to create a monthly feature on a different hazard within the City's newsletter. The tsunami brochure design project with UAF has been completed and printed copied distributed. There remains much work to do related to the Emergency Operations Plan (EOP) and future training opportunity development. I have staff actively looking for funding opportunities related emergency preparedness. Chief Kirko and I attended a Tsunami Operations Workshop in Kodiak in March 2023. We learned a lot of valuable information from the various state and federal agencies present. We're also hoping to leverage those connections into some in person training for staff in the not too distant future.
- **Public Safety Conversations** – The Volunteer Fire Department hosted an open house in October which brought the public inside the fire hall for a discussion about fire and EMS services here in

Homer. The Homer Police Department has had success engaging the public with women's self-defense classes and a talk about social media that **Lt. Browning** has given multiple times here in town, once at the State legislature, and a number of other communities in Alaska.

- **Volunteer Action Plan** – Due to demands related to other projects, the Volunteer Action Plan has been on a bit of a hiatus. I'm looking at having it resume activity shortly with significant help from Economic Development and Community Recreation.
- **Coast Guard City Designation** – The Coast Guard Commandant has indicated her intent to approve our application to Congress. If no objections are filed during the 30-day notification period, our status as a Coast Guard City will become official. If all goes according to plan, this will happen at our second meeting in May.
- **Harbor Float Replacements** – Council passed Ordinance 22-19(A) which made \$56,540 available from the Port Reserves Fund for grant and engineering assistance related to a harbor float replacement project. At the second meeting in April, Council will have an opportunity to pass a resolution supporting the City's submission of a Port Infrastructure Development Grant that would help fund float replacement.
- **Recreation Priorities and Planning** – While this is a bigger conversation than the HERC/Pioneer Gateway Redevelopment Project, much of the general recreation conversation is occurring in parallel to those work sessions/discussions. We did host a presentation by Community Recreation Manager Mike Illg on August 22 that covered the City's recreational relationship with the Kenai Peninsula Borough School District. Further discussions have been part of the FY24/25 operating budget discussion.
- **Cybersecurity Improvements** – In late 2022 we filled an open IT position created by Ordinance 22-20, and in early 2023 we brought on a new lead for IT which gave us a full three person IT team for the first time since the third position was created. The lead IT position's job description was rewritten by the Library Director and I to function as a Chief Technical Officer (CTO). The new team, paired with the institutional knowledge and experience of Network Administrator Tomasz Sulczynski, has been making up a lot of ground on necessary updates and improvements. You'll see in the FY24/25 proposed operating and capital budgets that are increased funds for cybersecurity operations and a number of systems upgrades funded as capital projects.

## 2020 Highlights

- HERC Demolition – demolition of the smaller HERC building hit a snag in permitting related to hazardous materials, specifically PCBs. Council approved a task order to do testing for PCBs, and the results will drive the next step in the process.
- Stormwater Management Plan and Implementation – we have a grant pending to support the portions of the Kachemak "sponge" stormwater plan. Ordinance 23-16(S) accepted a grant and appropriated funds to support the Beluga Slough stormwater treatment system.
- Climate Action Plan – the CAP was intended to be integrated into the Comprehensive Plan for which funding was largely vetoed; a new path forward needs to be developed
- Wayfinding – Plan was adopted by Council, next steps will involve testing designs and installing prototypes around town. FY24/25 Capital Budget includes \$50,000 for implementation.



- Procurement Policy – Has been rolled into 2022 goal of financial policy improvements; looking at finishing the project with a finance term contract holder
- Reserve Funding; Election Code; Council Operating Manual; Funding for Large Vessel Harbor Study – Largely complete

### **2020 Council Initiated Priorities**

HERC Demolition/Community Recreation Center		Stormwater Management Plan and Implementation		
Climate Action Plan		Wayfinding/Streetscape		
Water and Sewer Policy	Reserve Funding (Water and Sewer)	Election Code	City Council Operating Manual	Procurement Policy
Funding for Large Vessel Harbor Study				



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(f) 907-235-3148

### Memorandum

TO: Mayor Castner and Homer City Council  
FROM: Rob Dumouchel, City Manager  
DATE: May 4, 2023  
SUBJECT: City Manager's Report for May 8, 2023 Council Meeting

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#### **Homer to be Awarded Large Wastewater System of the Year!**

The Alaska Water Waste Management Association (AWWMA) selected the City of Homer to be their large Wastewater System of the Year. The AWWMA, founded in 1960, advances the technology of Alaska's water and wastewater industry through outreach, training and grassroots activism supporting Alaska's water resources and public health. The award for large Wastewater system of the Year recognizes the performance of a waste water system serving 2,500 or more individuals. Members of the Public Works team will receive the award at the upcoming AWWMA conference held in Anchorage on May 10th.

#### **Pioneer Avenue Banners**

Local artist Lucas Elliot has been working with Corvus Design and the Economic Development Commission to design new Pioneer Avenue street banners. The final design is complete, and staff expects banners to be up by July 1st.





### **AML Meeting in August**

The City of Homer is hosting this year's Alaska Municipal League summer conference. AML staff have already made one scouting trip to Homer, and will be making a second one in May to work with City staff on planning and logistics.

### **AMMA and AMLJIA Updates**

In May I will be taking over as the president of the Alaska Municipal Management Association (AMMA).

On April 27<sup>th</sup>, I attended a meeting of the AMLJIA Board of Trustees. Market conditions are not favorable right now, but we'll see some protection from our existing three-year agreement. I am expecting that Homer will see a rate increase capped at approximately 10% over last year (which is what we budgeted for in my draft FY24/25 operating budget proposal).

### **Invasive Species Mitigation**

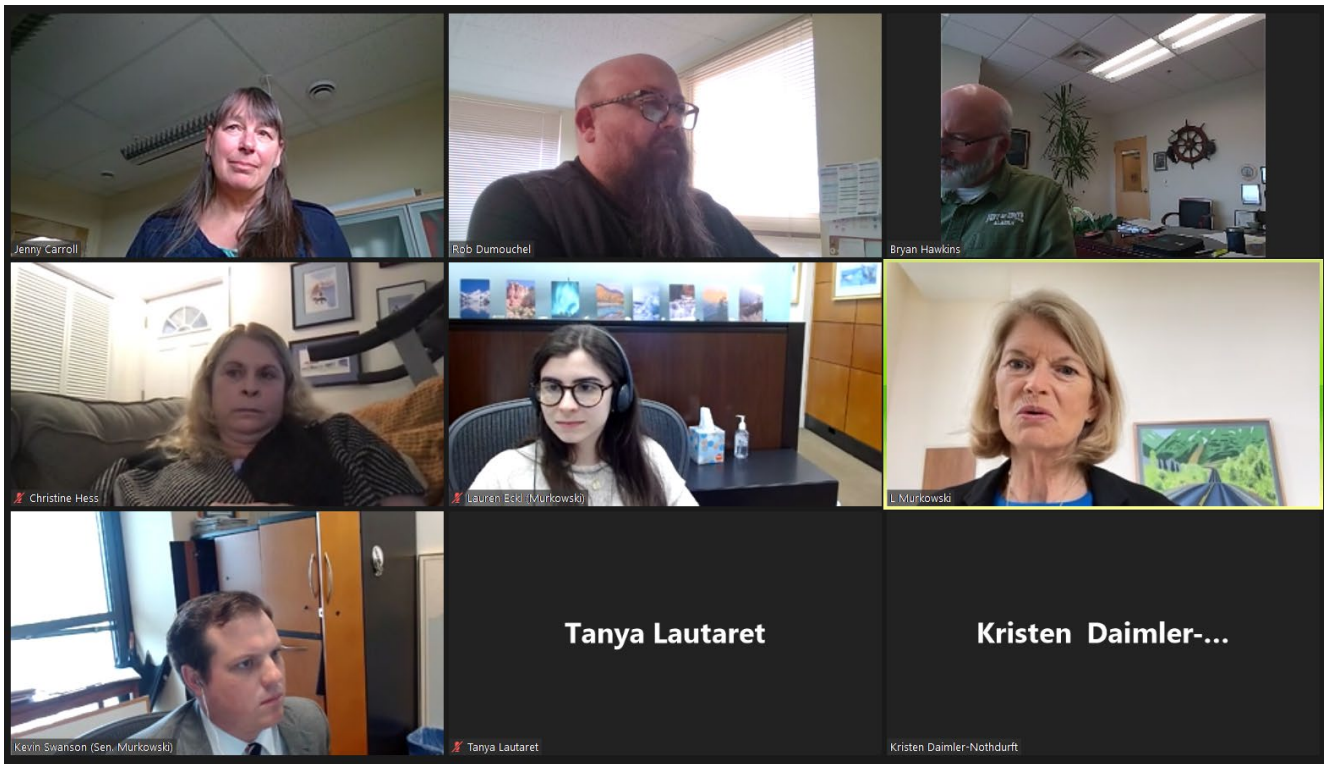
Update from Public Works Director Keiser:

*The City is again partnering with the Homer Soil & Water Conservation District (HSWCD) to work with local gravel/sand vendors to get their pits recertified as "weed-free". This program is part of the City's Invasive Species Management Program. Trained HSWCD staff visit the pits, inspect them for the presence of invasive weeds, assist with mitigation, and help the pit owners understand how to keep their pits weed-free. To further promote best practices, City specifications require that gravel/sand used on public works projects be from certified weed-free sources. Three local pit owners are participating in the program.*

*On other fun and exciting news related to the City's partnership with the HSWCD – there will be a "chicken tractor" coming to town soon! The HSWCD received a grant to research whether a portable coop of hens is a viable means of controlling infestations of invasive weeds. The HSWCD will build and maintain the "chicken tractor" and Public Works will provide the control sites as well as some retired laying hens. We might even find out why the chicken crossed the road!*

### Check-In with Senator Murkowski's Office

Special Projects Coordinator Jenny Carroll, Port Director Hawkins, and I met with Senator Murkowski's office to discuss various Homer topics with a federal nexus. We covered a lot of ground with staff, and Senator Murkowski was able to join us later on in the meeting for a quick harbor expansion study update.



### Harbor Matching Grant Program is Funded in Senate Budget Draft

The City of Homer does not have a project queued up this year for the State of Alaska's Harbor Matching Grant Program, however, we have been strong supporters of the program for many years. Mayor, Council, and staff have all lobbied for the program to be fully funded in this year's State budget at different points throughout the year. At this time, the program has found proposed funding from the Senate which is a great step forward. It still needs to make it to, and through, the Governor, but at least it's in a draft. There is a good chance Homer will be looking to use the program in the next cycle to support upcoming port projects.

### Homer Represented at Alaska Trails Conference

In April, Parks & Trails Planner Matt Steffy traveled to the annual Alaska Trails Conference in Anchorage. He delivered a presentation on "Trails as Transportation: How to get things done together," and outlined the work being done in the City of Homer. This included showcasing the highly successful relationship with the Homer Drawdown organization, as well an overview of some internal discussions for future developments. The session was very well attended with admiration expressed from other communities who have been trying to accomplish similar goals. The primary focus was community collaboration and cohesiveness. There was also a presentation by Cameale Johnson from the Homer Trails Alliance, as well as a discussion on mapping from Seldovia's Bretwood Higman (Hig) who regularly participates in Homer area trails projects.

### Economic Development, Parks, and Port Transitions

A number of transitions were put in motion for this summer season with various tasks moving between departments and divisions to attempt to build more sustainable and successful public-facing programs for parks, campgrounds, and public restrooms. To that end, Parks, Economic Development, and Public Works

staff have been meeting to coordinate summer projects such as Hornaday Park repairs, NOMAR wall repainting, and other projects at various stages of completion. The Port is beginning to welcome its first campers, and long-time seasonal all-star Chad Felice has recently stepped in as the temporary Parks Maintenance Coordinator. If the position is approved in the budget, we will be recruiting for the full-time version of that position later this summer. Collaboration will continue as we figure out new ways to work together and with the community to take care of Homer's parks.

### **Harbor Monthly Check-In**

In response to Resolution 23-37, staff has been working on developing the first report in response to that item. I am expecting it to show up in the supplemental packet on Monday.

#### **Enclosures:**

1. May Employee Anniversaries
2. Grant Updates
3. HERC Updates
4. Update from SBDC





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491 East Pioneer Avenue  
Homer, Alaska 99603

[citymanager@cityofhomer-ak.gov](mailto:citymanager@cityofhomer-ak.gov)

(p) 907-235-8121 x2222

(f) 907-235-3148

## Memorandum

TO: MAYOR CASTNER AND CITY COUNCIL  
FROM: Andrea Browning  
DATE: May 22, 2023  
SUBJECT: May Employee Anniversaries

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I would like to take the time to thank the following employees for the dedication, commitment and service they have provided the City and taxpayers of Homer over the years.

<b>Mark Whaley</b>	<b>Port</b>	<b>18</b>	<b>Years</b>
<b>Pike Ainsworth</b>	<b>Port</b>	<b>15</b>	<b>Years</b>
<b>Mike Gilbert</b>	<b>Public Works</b>	<b>9</b>	<b>Years</b>
<b>Jackie McDonough</b>	<b>Library</b>	<b>9</b>	<b>Years</b>
<b>Sean McGrorty</b>	<b>Port</b>	<b>7</b>	<b>Years</b>
<b>Ralph Skorski</b>	<b>Public Works</b>	<b>5</b>	<b>Years</b>
<b>Taylor Crowder</b>	<b>Police</b>	<b>2</b>	<b>Years</b>
<b>Susan Jeffres</b>	<b>Library</b>	<b>2</b>	<b>Years</b>
<b>Sean Love</b>	<b>Public Works</b>	<b>2</b>	<b>Years</b>
<b>Tim Roberts</b>	<b>Public Works</b>	<b>1</b>	<b>Year</b>
<b>Devan Wilson</b>	<b>Police</b>	<b>1</b>	<b>Year</b>



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## Administration

491 East Pioneer Avenue  
Homer, Alaska 99603

(p) 907-235-8121 x2222

(f) 907-235-3148

## Memorandum

TO: Mayor and City Council  
FROM: Jenny Carroll, Special Projects and Communications Coordinator  
THROUGH: Rob Dumouchel, City Manager  
DATE: May 4, 2023  
SUBJECT: Update on Federal and State grant applications

This is an informational memo to provide an update on Federal and State grant applications submitted or in process in support of City of Homer projects. New actions/information since the April 4, 2023 are highlighted in yellow on the attached chart.

Highlights include:

- Staff submitted four FY24 Congressionally Designated Spending (CDS) requests to Senator Murkowski and Representative Peltola. We learned that they have all been recommended to the Energy and Water Development Appropriations Subcommittee, clearing the first hurdle in the selection process.

Senator Murkowski forwarded our top three requests:

1. Drinking Water Resiliency Project, a combination of the Raw Water Transmission Main (RWTM) project, the A-Frame Water Transmission Line and the 250,000 gallon A-Frame water tank.
2. Kachemak Drive Peatland Water Quality Improvement Project & Erosion Mitigation (part of the Slope Stability & Erosion Mitigation Program)
3. Beluga Slough Lift Station

Representative Peltola forwarded our fourth priority request:

4. Homer Spit Coastal Erosion Mitigation

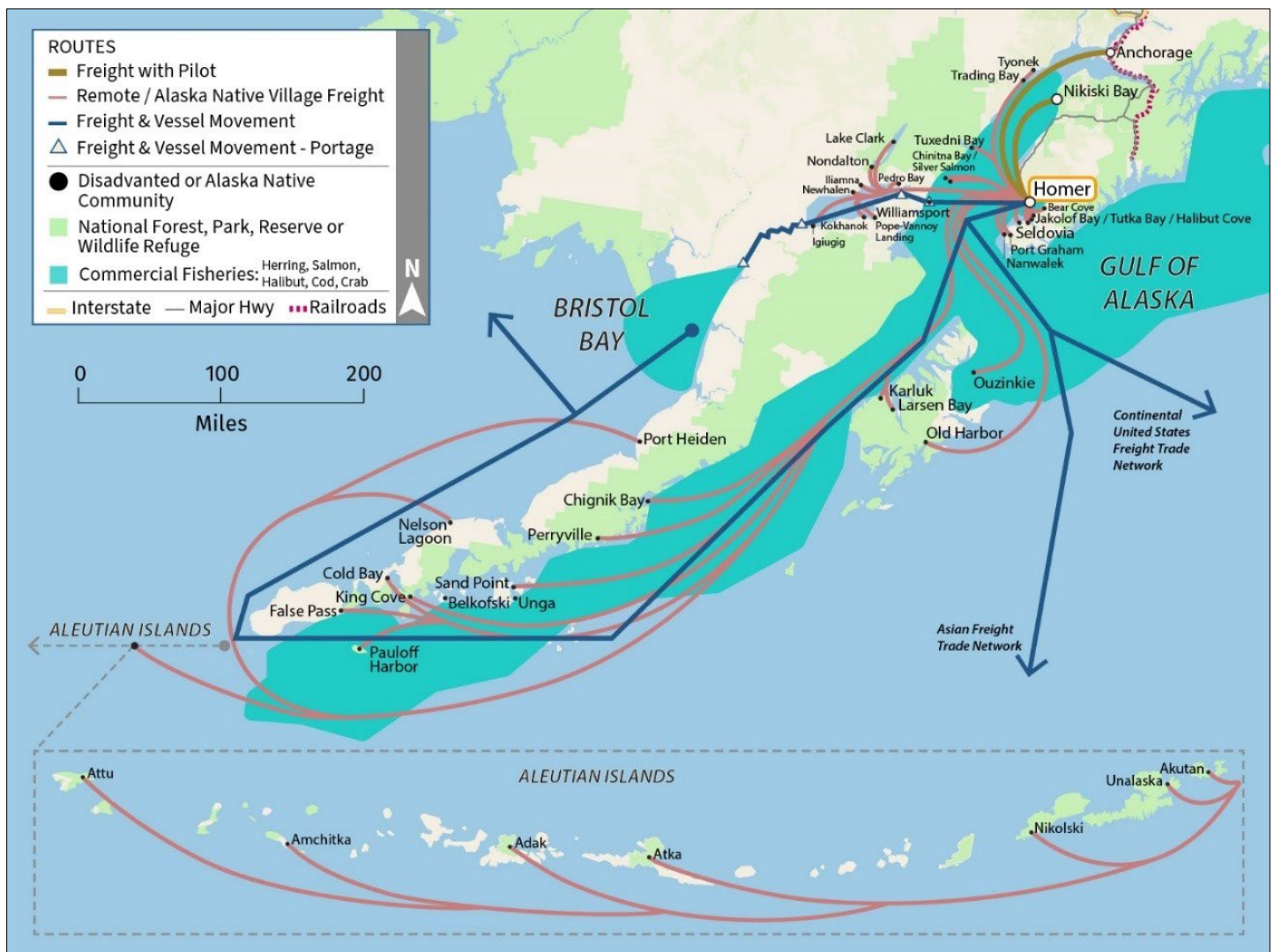
Two other hurdles to clear include approval by the subcommittee and Congress' Federal Budget deliberations.

The RWTM portion of the CDS Drinking Water Resiliency project is looking likely to be covered 100% by a FEMA Disaster Mitigation grant. My understanding is that the award request has been approved by FEMA and is now before the AK Congressional Delegation for approval. The RWTM project was included in the CDS Drinking Water Resiliency Project to show some potential funding momentum (\$2.18M) from another source.

- City staff collaborated very successfully with HDR to submit a solid grant application to the FY2023 Port Infrastructure Development Program (PIDP) at the end of April. Funds were requested to replace Float Systems 4 & 1 in the Homer Port & Harbor. We understand that the PIDP, which emphasizes supporting freight movement, attracts a large and competitive applicant pool.

Through our work on the application with HDR, we were able to quantify the Harbor's many marine transportation and freight connections, which looks different than port facilities handling containers but is quite compelling.

At least 47 non-road connected communities, unincorporated areas and remote work sites located across Kachemak Bay, in Cook Inlet and Western Alaska receive freight or transit through the Homer Port and Harbor, making Homer a marine transportation hub for a geographic area of roughly 130,000 square miles.



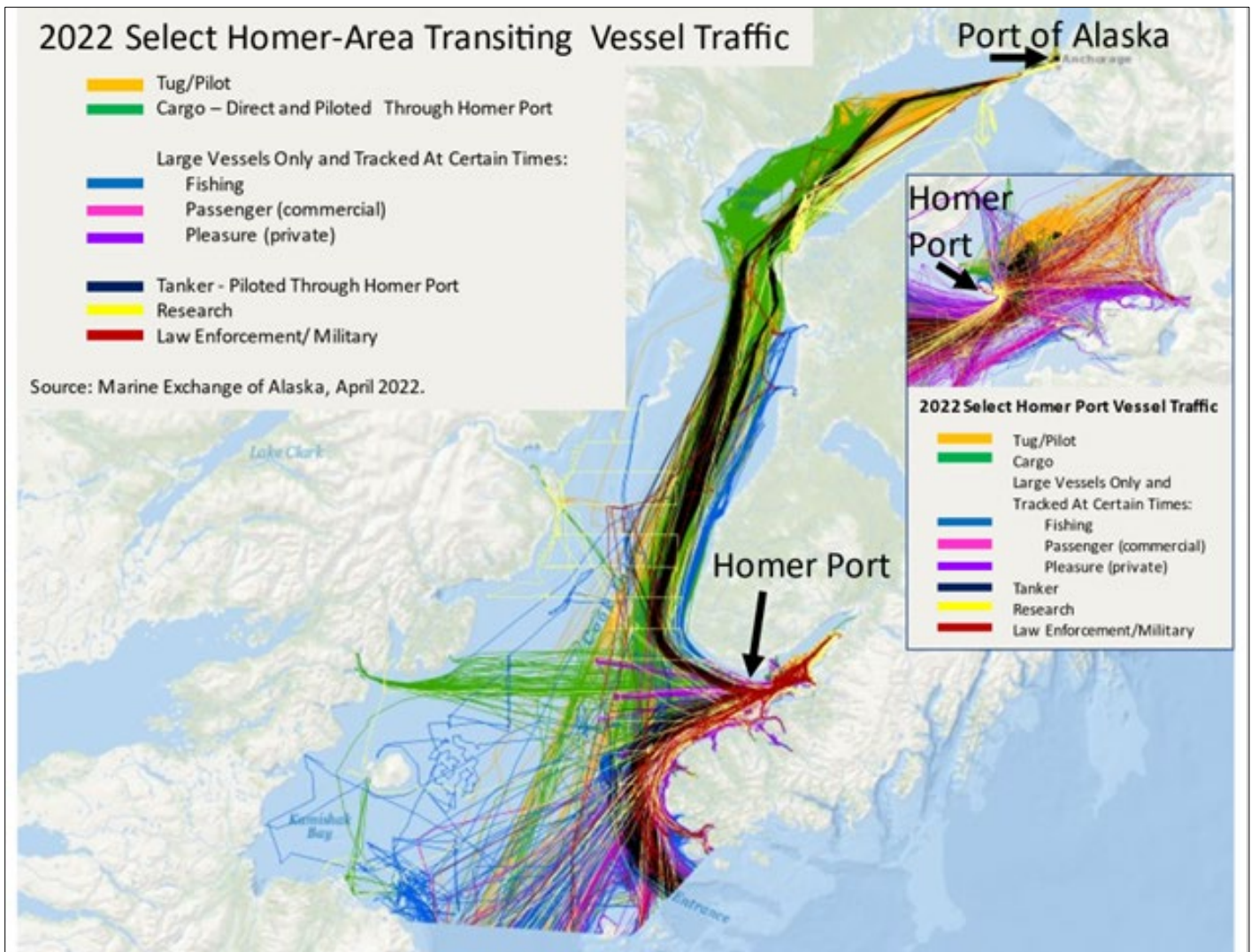
Ninety-two vessels in Homer's Port & Harbor provide freight shipping; the cargo consists primarily of essential goods and building materials – the supplies needed for homes and businesses to operate.

Two vessels provide 480 piloted cargo and tanker trips annually. This is a critical service for all cargo and tanker vessels traveling into the Port of Alaska, through which 90% of Alaska's cargo is handled.

Deliveries from the Homer Port also support commercial fisheries. As an example, in 2022, 4M tons of cargo, 300,000 gallons of fuel, and 350 fishing vessels originated in Homer's Port for delivery to Bristol Bay via Williamsport and Iliamna Portage Road.



The Marine Exchange of Alaska tracks data on select vessel movements. The diagram below shows vessel movements through and supported by the Homer Port and illustrates the Homer Port & Harbor high use and statewide connections.



- At the last City Council meeting, Coowe Walker, the Reserve Manager for the Kachemak Bay National Estuarine Research Reserve (KBNERR), announced that NOAA awarded a \$1.28 million Habitat Protection and Restoration grant in support of the City's innovative Kachemak Sponge Green Infrastructure Stormwater Management Project. I wanted to share the link to a press release NOAA published about the Alaska projects that received funding: at [www.noaa.gov/news-release/noaa-bil-investments2023-alaska](https://www.noaa.gov/news-release/noaa-bil-investments2023-alaska).

FY23

Project Supported	TPC	Grant Fund	Grant Funds Req	Match/Local Cost	Status	Notes
<b>Slope Stability- Erosion Mitigation Program</b>						
Kachemak Drive Peatland Water Quality Improvement (Kahcemak Sponge)	\$4,388,791	NOAA Habitat Conservation KBNERSS led/City partner	\$ 1,188,275.00 (land acquisition)	\$ 418,000.00 (land acquisition)	Submitted Awarded 4/1/2023	Awaiting entering into sub-grant agreement with UAA. Grant is for Kachemak Drive peatlands acquisition
		Congressionally Designated Spending Request #1 of 4	\$ 1,890,000.00 (water works)	\$ 892,516.00 (water works)	Submitted 3/17/2023 4/20 Forwarded to Approp Subcommittee by Murkowski	To Sen Murkowski & Represenative Peltola City costs eligible for funds from ADEC Clean Water Revolving Loan Fund & HART funds
		State of AK CAPSIS	\$ 2,744,994.00		Submitted 2/14/2023	State capital funding very unlikely in the State's FY24 budget
Beluga Slough Stormwater Treatment System	\$ 690,000.00	Alaska Clean Water Actions	\$ 153,307.00	\$ 107,182.00	Submitted 11/9/2022 Awarded 3/1/2023	Grant agreement Ord before City Council City costs in-kind + elibigle for funds from ADEC Clean Water Revolving Loan Fund
		State of AK CAPSIS	\$ 502,604.00		Submitted 2/14/2023	State capital funding very unlikely in the State's FY24 budget
<b>Transportation</b>						
Transportation Planning	\$ 960,000.00	Safe Streets For All KPB Applic/City partner	\$ 960,000.00	\$ 23,000.00 (in-kind)	Award announced	Application approved Reso 22-063
Non-Motorized Trans Network (REACH Project)	\$ 1,500,000.00	Fed RAISE Planning Grant	\$ 1,500,000.00	\$ -	Submitted 2/28/2023 Awaiting decision	HDR application support Reso 22-087 approved Project support Reso 23-012 approved Start date July '23/End date June '25 RAISE Constr Grant possible Feb '25
Homer All-Ages & Abilities Pedestrian Pathway	\$ 3,900,000.00	State of AK Transportation Alternatives  (TAP)	\$ 3,432,000.00	\$ 468,000.00	Submitted 2/15/2023 Awaiting DOT cost estimate	HAPP sections in PW's 5-year road plan Project support Reso 23-011 approved Public Eval Board Review mid-May Award announce May '23; Perf period end date June '25
Main Street Rehab/ Sidewalk South	\$ 4,200,000.00	State of AK Community Transportation Program (CTP)	\$ 3,696,000.00	\$ 504,000.00	Submitted 2/15/2023 Awaiting DOT cost estimate	Main St in 7-10 year horizon in PW Road Plan Project support Reso 23-011 approved Public Eval Board Review mid-May Award announce May '23; Perf period end date June '25  (\$500,000 set aside in HART for grant matching)
<b>Homer Float Systems</b>						
Float Systems 4 & 1 Replace	\$ 59,289,547.00	Fed Port Infrastructure Development Projects (PIDP)	\$47,135,190	\$ 12,154,357.00 (20.5% match)	Submitted 4/28/2023	HDR application support Ord 23-12 aprproved Project support Reso to Council 4/24/2023 Could apply for PIDP '24 if '23 unsuccessful Revenue bond or TIFIA loan for match
		Municipal Harbor Grant	\$ 5,000,000.00	\$ 5,000,000.00	Grant for construction only due in August 2023	If funded, leverages State match to lower City match
		Denali Commission Grant			Application not begun Due date 4/14/24	Can be used as non-Federal Match May not be competitive - \$1M max award Could apply in '24 for Design/Engineering
<b>Building Code Development</b>	\$ 587,500.00	State BRIC Sub-Grant (Building Resilient Infrastructure & Communities)	\$ 470,000.00	\$ 117,500.00 (covered by State of Alaska)	Submitted 12/21/2022 Awaiting decision	Council Approved application Reso 22-086 As a sub-applic State covers local match
<b>Parks &amp; Recreation</b>						
Multi-Use Community Center	\$ 350,000.00	State of AK CAPSIS	\$ 350,000.00 (planning)	-	Submitted 2/14/2023 Awaiting decision	State capital funding very unlikely in the State's FY24 budget
Bayview Park Renovations	\$ 139,230.00	State of AK Healthy & Equitable Community Round 2	\$ 74,919.00	\$ 52,314.00 Addl redistribution funds requested	Submitted 4/24/2023 Awaiting decision	Council Approved Resolution 23-031 \$12,000 donation from Kach Bay Rotary Possilble Approp req from HART if no addn HEC funds
<b>Utilities - Infrastructure Resilience</b>						
Drinking Water Resiliency (A-Frame Water Transmission & 250,000 gallon water tank)	\$ 2,852,253.00	Congressionally Designated Spending Request #2 of 4	\$ 1,931,090.00	\$ 921,163.00	Submitted 3/17/2023 4/20 Forwarded to Approp Subcommittee by Murkowski	To Sen Murkowski & Represenative Peltola City costs eligible for funds from ADEC Clean Water Revolving Loan Fund & HAWSP
A-Frame Water Transmission	\$ 804,029.00	State of AK CAPSIS	\$ 643,274.00	\$ 160,818.00	Submitted 3/14/2023	State capital funding very unlikely in the State's FY24 budget
Beluga Lift Station	\$ 2,937,353.00	Congressionally Designated Spending Request #3 of 4	\$ 2,000,000.00	\$ 937,353.00	Submitted 3/17/2023 4/20 Forwarded to Approp Subcommittee by Murkowski	To Sen Murkowski & Represenative Peltola City costs eligible for funds from ADEC Clean Water Revolving Loan Fund & HAWSP
Spit Road Erosion Mitigation (USACE General Investigation)	\$ 3,000,000.00	Congressionally Designated Spending Request #4 of 4	\$ 1,500,000.00	-	Submitted 3/17/2023 4/20 Forwarded to Approp Subcommittee by Peltola	To Sen Murkowski & Represenative Peltola Match funds potential from State PROTECT grant (unknown)
Raw Water Transmission Main		FEMA Disaster Mitigation	\$ 1,988,650.00	-	Submitted 1/32/2020	4/11/23: Responded to latest FEMA request for information 5/2/23: Award submitted to Congressional Deleg. for approval



# HERC

## May 2023 Updates

### Comprehensive Plan

#### Chapter 6: Public Services & Facilities

*Goal 1: Provide and improve city-operated facilities and services to meet the current needs of the community, anticipate growth, conserve energy, and keep pace with future demands.*

#### Goal 1: Demolition of HERC 2



##### Progress

Test samples were taken May 1 and 2.

##### Next Steps

Test results are expected by mid-June. Staff will provide information to Council in the June/July time frame based on the results.

#### Goal 2: Business Plan



##### Progress

Working with existing user groups to quantify demand and which programs are expected to grow.

##### Next Steps

Continue collecting information to inform business plan creation over the next year.

#### Goal 3: Right Size a Replacement Building



##### Progress

Staff held a conversation with representatives from the largest Community Recreation programs. Participants spoke about space needs for their activity (basketball, pickle ball, volley ball, karate, etc.) and talked about the user trends. Are programs growing? Overall, the consensus was yes, more people would participate if space was available.

##### Next Steps

Staff and Council will have a work session in August. Council has set an upper limit on the building cost and asked for a range of less expensive alternatives. Additionally, staff has been asked to plan ahead for a building that only meets today's needs, but the needs 20 years in the future. Staff and Council can begin to determine if a straight replacement of the HERC gym and multipurpose room is enough, or if a double gym and a multipurpose room are the goal.

Information from the FY 24/25 budgets will be informative as to how much money the general fund can provide to pay increased costs for supporting a new/larger facility. It may be that a new bond measure needs to be structured like the police station, where a portion of the ballot measure includes a permanent sales tax to support the new facility. Or perhaps pursuing a new tax.

#### Goal 4: Budget - Construction and Operations and Maintenance



##### Progress

None.

##### Next Steps

The August work session will have an update on the estimated bond payoff for the police station, and rough numbers on building operations and maintenance costs. This will provide context as to when a new bond measure vote might be appropriate and necessary new revenue for operations.





April 27, 2023

City of Homer  
491 E. Pioneer Ave  
Homer, AK 99603

Dear Mayor Castner, City Council, and City Staff,

This letter serves as our quarterly report for the period January 1 to March 31, 2023. The Homer Business Advisor, Robert Green, had a strong start to the year, logging the most hours of advising in a quarter ever in Homer. In the decade prior to Robert Green's hire in June 2020, the Alaska SBDC logged an average of 84.7 hours per quarter with clients in Homer. Robert has averaged 154.3 hours per quarter since joining the SBDC, with a steady increase each year, indicating local demand has been growing. Despite the busiest quarter the SBDC has recorded in Homer, Robert maintained a 100% satisfaction rating on client surveys, a testament to the quality of advising he provides entrepreneurs in the Homer community. Here is a summary of deliverables to the Homer community during the quarter:

Client Hours: 238.5

Total Clients: 55

New Businesses Started or Bought: 2

Jobs Supported: 189

Capital Infusion: \$25,000

Client Surveys: 100% positive

This year, quarterly reports will feature lists of the top advising topics and top industries obtaining technical assistance from the Alaska SBDC in Homer. Assistance to start-ups and entrepreneurs seeking loans to grow their business were the top topics this quarter, while tour operators, gearing up for tourist season, and food services, from full-service restaurants to food trucks, were the top industries.

#### Topics

1. Start-up Assistance: 76.7 hrs (32%)
2. Financing/Capital: 44.6 hrs (19%)
3. Financial Planning: 37.5 hrs (16%)
4. Business Planning: 13.0 hrs (5%)
5. General Management: 11.4 hrs (5%)

#### Industries

1. Tour Operators: 55.3 hrs (23%)
2. Food Services: 31.6 hrs (13%)
3. Rentals: 23.2 hrs (10%)
4. Retailers: 20.7 hrs (9%)
5. Educational Services: 18.5 hrs (8%)

We would like to thank the City of Homer for their support of the Homer Business Advisor position. We greatly appreciate the knowledge, experience, and consistency Robert Green brings to our team. Please do not hesitate to contact us if you have any questions.

Sincerely,

DocuSigned by:

*Jon Bittner*  
F3E1FE8A6ADF4BD...

Jon Bittner

Executive Director  
Alaska SBDC

VOL. II - ISSUE VIII | MAY 2023

# CITY OF HOMER

Monthly Newsletter from the Office of the City Manager



## Greetings from City Hall

As the snow melts and the days grow longer, our community is buzzing with activity and excitement. The City is excited to bring you the latest news and updates from our vibrant community.



## Kachemak Sponge Green Infrastructure Stormwater Management Project

At a recent City Council meeting, Coowe Walker, Reserve Manager for the Kachemak Bay National Estuarine Research Reserve (KBNERR), announced some exciting news: the National Oceanic and Atmospheric Agency (NOAA) awarded a \$1.28 million Habitat Protection and Restoration grant in support of the City's innovative Kachemak Sponge Green Infrastructure Stormwater Management Project. Read the NOAA press release at [www.noaa.gov/news-release/noaa-bil-investments-2023-alaska](http://www.noaa.gov/news-release/noaa-bil-investments-2023-alaska).

*Kachemak Sponge* is one of several green infrastructure projects planned by the City that utilizes natural wetlands to help mitigate problems associated with stormwater runoff. Currently, drainage ditches and culverts discharge stormwater directly into Kachemak Bay, which contributes to bluff erosion and introduces turbidity and nonpoint source pollutants into Kachemak Bay. The *Kachemak Sponge* project will convey this stormwater to retention/filtration structures first to remove sediment and contaminants, and then on to peatlands. Peatlands work naturally to absorb stormwater and slow discharge rates into Kachemak Bay. Peatland vegetation also has the ability to further remediate hazardous substances through long-term sequestration and elemental pollution removal.

The system is based on successful wetland treatment projects in Finland that have been proven to work wonders in cold climates. NOAA funds will acquire the peatlands necessary for this project. The City thanks KBNERR for their valued partnership in the project. For more information on this project, visit <https://bit.ly/3AKAKpl>.

## WHAT'S INSIDE?

- Library Events
- Community Corner
- Community Recreation
- Fire Department
- City Manager's Office
- City Clerk's Office
- May is Bike Month!
- Port & Harbor
- Public Safety Corner
- Meet the Staff
- Municipal Art Collection
- Stay Connected with City Council
- Join Our Team

Discover something new today and see the latest City project updates information! Learn about ways community members can get involved at City Hall and in the Homer community.

## Follow us on Facebook

- City Hall: @cityofhomerak
- Parks & Recreation: @homerparksandrec
- Homer Public Library: @homerpubliclibrary
- Homer Police: @homerpolice
- Fire Department: @HomerVolFireDept

Subscribe to the Monthly Newsletter:

[www.cityofhomer-ak.gov/citymanager/monthly-email-newsletter](http://www.cityofhomer-ak.gov/citymanager/monthly-email-newsletter)

*"Where the Land Ends and the Sea Begins"*

# LIBRARY EVENTS

## Virtual Author Talks

Zoom in and listen to your favorite authors talk about their latest books. For a complete list of Upcoming Speakers, go to <https://libraryc.org/homerlibrary/upcoming>

**May 3:** *Raising Anti-Racist Children* by Britt Hawthorne, 9 a.m.

**May 10:** *Not Funny* by Jena Fiedman, 4 p.m.

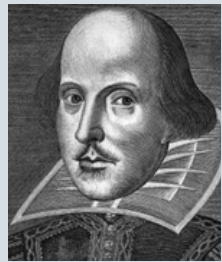
**May 20:** *Sadie* by Courtney Summers, 10 a.m.

## Spring Book and Plant Sale

### May 5-6

On Friday, May 5, join Friends of the Homer Library for a **members-only sale!** Browse through hundreds of books and dozens of plants the evening before the sale goes public. Want to become a member of the Friends of the Library and get first pick? Memberships start at \$15. You can join as a member at the door or [sign up today!](#)

On Saturday, May 6, all are welcome! The sale will continue all day, from 10 a.m. to 6 p.m.



## Second Sunday Shakespeare

### May 14: *Henry VIII*

Friends of the Homer Library, in partnership with Pier One Theatre and Kachemak Bay Campus, invite you to participate in an online reading of *Henry VIII* by William Shakespeare.

Find more information on the City's website: [www.cityofhomer-ak.gov/library/second-sunday-shakespeare-2](http://www.cityofhomer-ak.gov/library/second-sunday-shakespeare-2)

## Lunch with a Council Member

### May 9 - Shelly Erickson

Noon - 1 p.m., Library Meeting Room

Join the conversation with Council Member **Shelly Erickson**. Bring your questions and topics for discussion and enjoy a virtual lunch.

[Virtual Meeting Information](#)



Check out the complete line-up of library programs



**Homer Public Library**  
500 Hazel Street - 907-235-3180  
[circ@ci.homer.ak.us](mailto:circ@ci.homer.ak.us)  
[www.cityofhomer-ak.gov/library](http://www.cityofhomer-ak.gov/library)

# Community Corner

What's happening around the city of Homer

## 31st Annual Kachemak Bay Shorebird Festival

### May 3-7

The Kachemak Bay Shorebird Festival is right around the corner! Bird watchers from all over the world flock to Homer to experience the annual migration of birds along the miles of shoreline and intertidal habitats found here. Homer is a prime location to catch a glimpse of the birds and take part in the educational programs led by the Center for Alaskan Coastal Studies, the Pratt Museum and Park and more.

Online registration is open. Go to [www.kachemakshorebird.org](http://www.kachemakshorebird.org) for more information.



**Sign up! Form a team!**  
**Step Up for Your Health!**



**7th Annual Free Community Walking Challenge**

**SAVE THE DATE! ORGANIZE YOUR TEAM!**

**REGISTRATION OPENS APRIL 10!**

**May 1st - 28th, 2023**

**WALK TO WIN: WEEKLY DRAWINGS FOR HOMER BUCKS!**

**End-of-challenge CASH PRIZE DRAWINGS - \$100 - \$200 - \$300**  
**\$500, \$300 or \$200 to donate to your team's charity of choice!**

**Custom "2023 Homer Steps Up! Winners" T-shirts for your team!**

**HOMERSTEPSUP.COM**





# Community Recreation



## 30th Annual Safe and Healthy Kids Fair

The 30th Annual Safe and Healthy Kids Fair is happening on for Saturday, May 6 from 11 a.m. - 2 p.m. in the parking lot of the Homer High School. This is a free, family-friendly event for children of all ages to focus on safety, health, wellness, and fun! It offers more than 25 booths and stations with hands-on activities and demonstrations. All attendees are invited to enter their names into a drawing to win one of four brand-new bikes of various sizes for differing age ranges.

Kids are invited to bring their bikes and helmets for a free bike inspection and enjoy riding the course of the bike rodeo. Helmets are required and are available for purchase on-site at a discounted rate.

Other attractions include visits from Beary the Be Well Bear, the infamous teddy bear clinic, stilt walkers, BOB the bookmobile, loads of giveaways, free car seat checks, a children's circus performance, resources from youth-serving agencies, and more!

Food will be available for purchase, and all proceeds from food and helmet sales help support the operational expenses of the fair. The Safe and Healthy Kids Fair is organized by a committee representing local organizations whose missions include facilitating healthy outcomes and injury prevention for youth. The motto reflected at the annual fair is **"Be safe, have fun, do good, and pass it on!"**

For more information or to get involved, visit the Safe and Healthy Kids Fair Facebook page, email [safeandhealthykidsfair@gmail.com](mailto:safeandhealthykidsfair@gmail.com).



Contact the Safe & Healthy Kids Fair Committee for more info: [safeandhealthykidsfair@gmail.com](mailto:safeandhealthykidsfair@gmail.com)  
KPBSD posts flyers a community service. Such posting does not constitute an endorsement for or against the materials or viewpoints expressed in them.

## Volleyball Tournament

City of Homer Community Recreation hosted an all-day Adult Co-Ed Volleyball Tournament on Sunday, April 2. A total of eight teams, including one from the Central Peninsula, played 28 games with a crowd of over a hundred fans cheering them on.

Congratulations to **TEAM PENINSULA** for winning the inaugural volleyball Tournament. **TEAM CAROLINE** took second place and **TEAM ANNA** placed third.

To see the entire Community Recreation program list, check out [www.cityofhomer-ak.gov/com-rec](http://www.cityofhomer-ak.gov/com-rec).



## Inlet Winds Band Performance

The City of Homer Community Recreation **Inlet Winds Band** held an amazing performance at the Mariner Theater on April 22. An estimated 250 people were in attendance. There are 38 band members including seven high school students who have been practicing once a week for two hours for the past 10 weeks to learn an array of some complex yet entertaining songs. The band was led by volunteer Band Directors Eric and Mary Simondsen. Stay tuned for future band and concert opportunities in the fall!



## Fire Department



### Residential Fuel Spill

In April, Homer Volunteer Fire Department responded to a call from Enstar Energy regarding a fuel smell in Crittenden and Hidden Way area. Fire Chief Mark Kirko tracked the scent to a property on Hidden Way and reported the spill to the City's Water and Sewer team, the Alaska Department of Environmental Conservation, and the Coast Guard. The spill was determined to be home heating fuel from a 300-gallon tank, with an estimated leakage of 75-150 gallons. HVFD and the City's Water and Sewer team responded with crews to place absorbent boom and pads at culverts and storm drains to help mitigate the spread of fuel toward the bay. DEC has taken over case management and HVFD is no longer involved.

To prevent future spills, the City encourages property owners to take a look at their heating oil fuel tanks to make sure they're in good shape. Even if you have converted to natural gas, you may still have a heating oil fuel tank, so be sure to check out old systems for safety and security!

Check out the [Steps to Close a Heating Oil Tank](#) for some advice.

### Steps to Close a Heating Oil Tank

#### Notify the Oil Supplier

1

If you are converting to gas or electric heat or if you are replacing a tank, call your oil distributor and ask them to discontinue the oil service to your tank.

2

#### Remove Product

Remove all petroleum product from the tank and line before pulling the tank. Your oil supplier may pump out the reusable oil and credit your account. The fuel supplier should have an explosion-proof or air-driven pump or a vacuum truck to remove the liquid in the tank.

3

#### Disconnect all Piping and Drain

All piping should be disconnected, drained, and capped (if it cannot be removed). This includes the tank fill line.

4

#### Tank Cleaning

Oil sludge or residue should be removed and disposed of properly.

5

#### Tank Removal

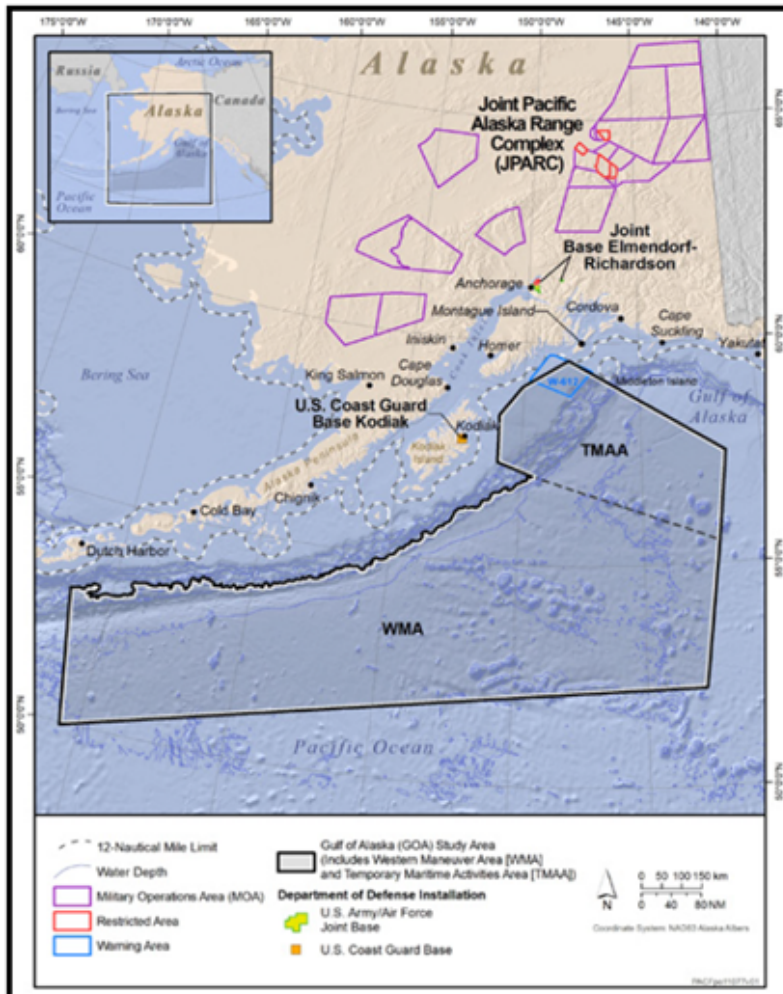
Once the tank is cleaned, it may be removed from the property and disposed properly.



## Northern Edge Exercise

In a virtual meeting on April 20, 2023, Mayor Castner and City staff met with representatives from the Navy to discuss the upcoming Northern Edge Exercise, which will start in early to mid-May. The Northern Edge Exercise is the military's largest joint training exercise in Alaska. Maritime training activities conducted during Northern Edge occur in the Gulf of Alaska within a designated Temporary Maritime Activities Area (TMAA), which is located south of Prince William Sound and east of Kodiak Island, and the Western Maneuver Area (WMA), which is located south of Kodiak Island.

This meeting was part of the coastal community outreach that the Navy is coordinating in anticipation of the exercises which take place every other year and typically last up to 21 days. During the meeting with City staff, John Moser, Environmental Planner for the Navy, discussed the Environmental Impact Statement (EIS) for Northern Edge and the mitigation measures in place. The prominent changes since the last exercise in 2021 include an enlarged training area where vessels and aircraft can operate called the Western Maneuver Area (WMA), and a new Continental Shelf and Slope Mitigation Area within the Temporary Maritime Activities Area (TMAA). While in the past a majority of activities occurred in the TMAA, the Navy now prohibits the use of explosives during training in the Continental Shelf and Slope Mitigation Area to protect marine species and biologically important habitat.



## LOCAL HAZARD MITIGATION PLAN

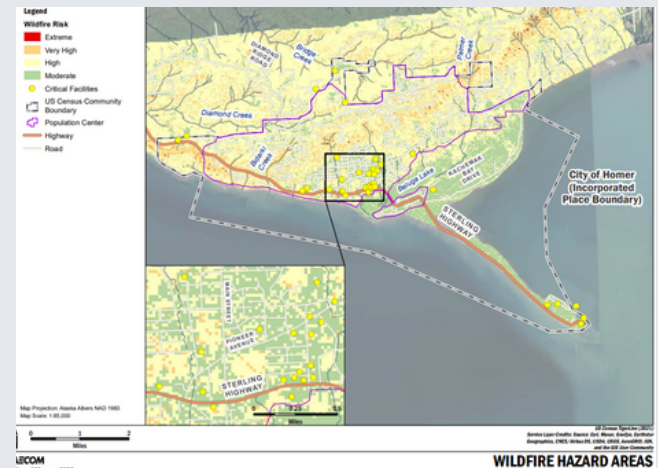
### WILDFIRE

A wildfire—also known as a wildland fire—is a fire in an area of combustible vegetation occurring in rural areas. Wildfires can be caused by human activities (e.g., unattended burns, campfires, or off-road vehicles without spark-arresting mufflers); or by natural events (e.g., lightning, drought, or infestation). Wildfires can be classified as forest, urban, tundra, interface or intermix fires, and prescribed burns.

Most of the Homer area has moderate or high wildland fuel risk, with some areas of very high risk. The Homer Spit, tidal flats, and low-lying areas around Beluga Lake are at moderate risk; the areas of very high risk are primarily along the bluffs.

Creating and maintaining defensible space around your home is crucial. For more information on defensible space, review the [AK Firewise Pocket Guide](#).

If you would like to read more about wildfires and their potential hazard of them to Homer, please refer to Table 3-10 of the [Local Hazard Mitigation Plan](#).



### Burn Bans & Permits

As we enter the dry spring season before the green-up, the risk of fire becomes greater. To ensure your safety and the safety of those around you, it is important to take the necessary precautions before burning. Make sure to check if there are any burn bans in effect and obtain any required permits before proceeding. Check out [www.cityofhomer-ak.gov/fire/burn-permits](http://www.cityofhomer-ak.gov/fire/burn-permits) for more information.

# City Clerk's Office

## Celebrating the 54th Annual Professional Municipal Clerks Week

April 29 – May 6, 2023, is the 54th Annual Professional Municipal Clerks Week. Initiated in 1969 by the International Institute of Municipal Clerks and endorsed by all of its members throughout the United States, Canada, and 15 other countries, the week is a time of celebration and reflection on the importance of the Clerk's office. Although it is one of the oldest positions in local government, few people realize the vital services Municipal and Deputy Clerks perform for their community. They are the local officials who administer democratic processes and ensure transparency to the public, which includes keeping the official records of the city, conducting local elections, and facilitating all legislative actions. They act as compliance officers for federal, state, and local statutes, provide parliamentary support to City Council and the commissions/boards, manage public inquiries and relationships, arrange for ceremonial and official functions, and may even serve as financial officers or chief administrative officers. For more information on the Homer City Clerk's Office including our Code of Ethics, staffing history, and the history of the Clerk profession, see the complete Clerk's Office Current and Historical Information Packet online at [www.cityofhomer-ak.gov/cityclerk](http://www.cityofhomer-ak.gov/cityclerk).



Rachel and the Library Advisory Board. Red Asslin Martin, Marcia Kuszmaul, David Berry, Douglas Bailey, Kathy Carsow, Rachel Tussey, Kate Finn, Michael McKinney, and Bristol Johnson.

## May is Bike Month in Homer!

With beautiful scenery and ample opportunities for outdoor recreation, it's no wonder biking is a popular mode of transportation in our community. But biking isn't just a fun way to get around – it's also an important part of our City's [Climate Action Plan](#). By choosing non-motorized transportation options like biking, we can reduce our carbon footprint and work towards a more sustainable future.

Throughout the month of May, Homer Drawdown and Homer Cycling Club will be hosting a variety of biking activities and events. Whether you're an experienced cyclist or just getting started, there will be something for everyone to enjoy. Check out the [Bike Month flyer](#) for more details. From a safety class to a film festival and bike rodeo, there are plenty of ways to get involved and show your support for biking in the community.

Community participation is key to achieving the community's climate goals, and that's why community members are encouraged to get involved in bike month activities. By coming together as a community to support non-motorized transportation options like biking, we can make a real difference in reducing our impact on the environment and building a more sustainable future for generations to come. **So grab your bike, join the fun, and let's make May 2023 a Bike Month to remember!**

**HOMER DRAWDOWN and HOMER CYCLING CLUB presents**

# BIKE MONTH

MAY 2023  
[www.HomerDrawdown.info](http://www.HomerDrawdown.info)

**BIKE / WALK TO WORK / SCHOOL WEEK : MAY 15 - 19**

**ENERGIZER BOOTH at WKFL PARK**  
coffee snacks bacon  
Mon, May 15th, 7:30 - 9:30 am  
smoothy bike  
Fri, May 19th, 4:30 - 6:30 pm

**SAFE AND HEALTHY KIDS FAIR**  
bike rodeo  
Sat, May 6th, 11:00 - Noon  
High School Parking Lot

**KBBI COFFEE TABLE**  
listen and call in  
Wed, May 10th, 9:00 - 10:00 am

**FILMED BY BIKE**  
film festival  
Thurs, May 18th, 6:00 pm  
Homer Theater  
\$10

**BY BICYCLE: COMMUTING HOMER**  
Sat, May 20th, 10:00 - Noon  
KPC campus

**free safety class**

**PLAY BIKE BINGO!**  
Fill out a card and turn it in to the Friday energizer booth for a chance to win prizes!  
[www.HomerDrawdown.info](http://www.HomerDrawdown.info)





## Harbor Expansion Study Update

On March 29, 2023, the City and the U.S. Army Corps of Engineers (USACE) signed a Federal Cost Share Agreement to begin a three-year feasibility study of expanding Homer's harbor. Planning activities with the USACE will kick off with a three-day design charrette, May 17 through 19.

The USACE conducts design charrettes in the very early stage of a general investigation study to identify project requirements, gather functional information from customers and stakeholders, assess the project site, and allow for public engagement with the USACE design team. The full schedule of public events is below and online at [www.cityofhomer.ak.gov/port/homer-harbor-expansion](http://www.cityofhomer.ak.gov/port/homer-harbor-expansion). Participation can be in-person or virtual. A link will be provided later at [www.homerharborexansion.com](http://www.homerharborexansion.com).

The City is also working with HDR as an owner representative to assist the City in a successful coordination effort between USACE and the greater Homer community. HDR's planning activities with the City began on Friday, April 28 at a joint work session with the Homer City Council and Port & Harbor Advisory Commission where they began drafting a Charter Document that defines a shared vision, collective goals and objectives, and success factors for the City's role in the Homer Harbor Expansion.

Stay up to date on the Homer Harbor Expansion study at the website: [www.homerharborexansion.com](http://www.homerharborexansion.com).

Mark Your Calendar!



May 17, 18 and 19



Islands & Oceans Visitor Center



[www.homerharborexansion.com](http://www.homerharborexansion.com)



Full schedule below

## Homer Harbor Expansion Design Charrette

Public Work Session Schedule

Wednesday, May 17

12:30 to 4:30 p.m.

First Public Work Session

*(Open to the public with comment at end of work session)*

- Introduction
- USACE Process
- General Navigation Feature vs. Local Services Facility
- Harbor Expansion History

Thursday, May 18

1:30 to 5 p.m.

Second Public Work Session

*(Open to the public with full public engagement)*

- Developing potential measures and alternatives to be considered in scoping phase

Friday, May 19

9 a.m. to 12:30 p.m.

Third Public Work Session

*(Open to the public with comment at end of work session)*

- Compare measures and alternatives that will be considered in scoping phase

2 to 3 p.m.

USACE Charette Briefing

*(Open to the public with comment at end of work session)*

- Provide summary of work completed during charrette
- Provide USACE next steps in Feasibility Study

# Port & Harbor



## Float Rehabilitation Project Update

Harbor maintenance staff are working on refurbishing the transient floats on the end of floats K through Q. These floats were built in 1986 and are six years past their engineered life expectancy. Most floats in that area are still in remarkably good condition despite heavy use in Alaskan weather, but the T-Head floats are twisted and damaged from several factors including the age of the floats. Additional strain is caused by rafting multiple vessels on the end floats. High wakes caused by fast incoming vessels traveling to the Load and Launch Ramp add even greater force loads to the floats, more than they were engineered for. With assistance from Bellingham Marine, the City is rehabilitating these floats to relieve the wracking damage and restore a safe walking surface.



## Fishing Lagoon Maintenance

The Fishing Lagoon is undergoing maintenance in order to keep the lagoon from filling in with sediment and gravel. Materials are collected in the narrow part of the lagoon, as shown in the photo. If not cleared out, the materials will flow into the lagoon and fill in the lobes, making the depth too shallow for the salmon fry pens.

A local contractor, Dibble Creek, will be walking a large excavator down the entrance of the lagoon to clean out all the materials that they can reach with a 60' boom. The materials will be loaded into trucks and moved offsite to the contractor's materials storage yard on the Spit. They will also clear out the materials that have accumulated in the entrance and clean behind the East groin. It is recommended that the East groin is kept cleared out enough so that the material is not allowed to migrate around and down the entrance into the lagoon.

The Fishing Lagoon does not have a fund to support it, so this time around, City Council approved to trade the dredged materials in exchange for the contractor's services. Dibble Creek is a local contractor with experience completing this type of work for the City. The proposed work is properly permitted and covered under the U.S. Army Corps of Engineers permit currently in place and held by Public Works. This project will preserve the great fishing that the lagoon has to offer.





## Parking Fee Structure Update

The City of Homer Port & Harbor department has announced an update to its parking fee structure. The update includes a change to one parking area and an increase in fees in some areas.

Starting Memorial Day, May 29, 2023, the last section of angled parking spaces on the west side of Homer Spit Road will be limited to 3-hour parking and remain free or at no charge. These spaces were previously designated as 7-day free parking.

Additionally, parking fees will increase from \$5 to \$10 per day in certain high-demand areas. These areas include the spaces at the top of Ramps 1 through 4. The fee increase is necessary to keep up with the rising cost of maintenance and operations and to ensure that the parking facilities continue to meet the needs of the community. All other Spit parking lots continue to offer free parking for up to seven consecutive 24-hour days. After seven days, vehicles must be removed from City property for at least 24 hours before returning, purchase a long-term parking pass, or will be subject to citation.

Future expansion of paid parking to most of the areas on the west side of the Spit is under consideration. The City of Homer is working with the State Department of Transportation on implementing paid parking in the public right-of-way. The potential expansion of paid parking is being explored as a way to manage parking demand and support the sustainability of Port & Harbor maintenance operations for high-quality transportation and public spaces.

Future expansion of paid parking to most of the areas on the west side of the Spit is under consideration. The City of Homer is working with the State Department of Transportation on implementing paid parking in the public right-of-way. The potential expansion of paid parking is being explored as a way to manage parking demand and support sustainability of Port & Harbor maintenance operations for high quality transportation and public spaces.

"We understand that these changes may be difficult for some of our customers, but we believe that they are necessary to provide a better parking experience for everyone," said Bryan Hawkins, Harbormaster and Port & Harbor Director. "By implementing paid parking in high-demand areas, we can reduce congestion and ensure that spaces are available for those who need them. And by increasing fees in certain areas, we can ensure that our parking facilities are well-maintained and continue to meet the needs of our community."

The City of Homer is committed to providing safe and affordable parking for all community members and visitors. For more information about the fee structure update or to provide feedback, please visit [www.cityofhomer-ak.gov/port/port-homer-parking](http://www.cityofhomer-ak.gov/port/port-homer-parking).

### City of Homer Port & Harbor Facilities Vehicle Parking





# Public Safety Corner



In this section, we aim to keep readers informed about the latest developments in public safety in the community. Whether it's news about crime prevention, emergency preparedness, or updates on local law enforcement activities, we've got you covered. Our goal is to promote a safe and secure environment for all community members and visitors of Homer, and we believe that staying informed is a crucial part of achieving that. Read on to learn more about what's happening in public safety in Homer.

## Construction Season Safety

As the 2023 construction season kicks off, it's important for all of us to be aware of the hazards and challenges that come with roadwork.

According to [workzonesafety.org](https://www.workzonesafety.org), in 2022, 857 people were killed in 774 fatal work zone crashes, with 117 of those fatalities being work zone workers. It is crucial for drivers to moderate their speed, stay focused, watch for construction signs, and pay attention to flaggers. At the same time, workers are encouraged to follow important safety guidelines that help prevent worker injury and death. By minimizing distractions in work zones, both drivers and workers can prevent tragedies from occurring.

To help you prepare for construction season, we encourage you to stay up-to-date on construction traffic impacts and delays by checking [511.alaska.gov](https://511.alaska.gov).



In Homer, we value safety above all else. Let's all do our part to ensure that this construction season is a safe one for everyone involved.



## SMOKE ALARMS SAVE LIVES!

Working smoke alarms can cut the risk of death from home fires in half.

That's why we're rallying volunteers, fire departments and partners to *Sound the Alarm*.

Together, we're installing **FREE** smoke alarms in your community. Request a smoke alarm installation today!

Homer / Ninilchik / Anchor Point Communities

May 11-13, 2023

Thank You, Southcentral Territory Sponsors:  
CHUGACH ALASKA & THE HOMER FOUNDATION

[SoundTheAlarm.org/alaska](https://SoundTheAlarm.org/alaska)  
907-201-2047  
[bridget.vivoda@redcross.org](mailto:bridget.vivoda@redcross.org)



American Red Cross



*Matt Steffy and Donna Aderhold  
brushing out the Lee Ave Trail.*

## Red Cross Home Fire Campaign

The Red Cross Home Fire Campaign could help save lives in local communities. From May 11 to 13, they will be providing free smoke alarm installations to homes that do not have them. The Red Cross Home Fire Campaign and is a great opportunity for those who may not have the means to purchase a smoke alarm themselves. To sign up for this free service, visit the Red Cross' website and fill out the form at [www.redcross.org/local/alaska/about-us/our-work/home-fire-campaign.html](https://www.redcross.org/local/alaska/about-us/our-work/home-fire-campaign.html). This is an excellent opportunity for the community to come together and ensure the safety of their homes and families.

Don't miss your chance to sign up for a free smoke alarm installation!



# Welcome

## ABOARD

Let's give a warm Homer welcome to the newest recruits! We are happy to have you as part of our team and wish you the best in your new ventures.

- **David Garcia**, Building Custodian
- **Michael de la Torre**, Public Safety Dispatcher

## April Anniversaries

We would like to recognize City staff members with anniversaries this month. Thank you for the dedication, commitment and service you've provided the City and taxpayers of Homer over the years. You all are an integral part of what makes the City of Homer a great place to work and the community.

Joe Young	Public Works	16 Years
Rick Pitta	Police	14 Years
Erica Hollis	Port	11 Years
Jean Hughes	Public Works	9 Years
Jaclyn Arndt	Fire	8 Years
Dave Berry	Library	4 Years
Ricky Borland	Port	4 Years
Aaron Yeaton	Public Works	2 Years
Evangelina Campuzano	Police	2 Years
Will Kern	Public Works	2 Years
Joe Kahles	Fire	1 Year

## Departures & Transitions

The City thanks **Rachel Tussey** for her service to the City and wishes her well on her next adventure! Rachel started her career with the City as a part-time administrative assistant in the Clerk's office. She also has served as an Administrative Secretary at Port and Harbor and finally as Deputy City Clerk in the Clerk's Office. Rachel also was the Sister City Coordinator with the City's Sister City Teshio, Japan. Rachel is an efficient clerk who had an eye for detail, strong technical writing skills, and helped get some modern touches to the workflows. She was an asset to the team and will be missed!



## Meet the Staff



This section is to highlight staff, their achievements, promotions, retirements, departures and new hires to the City's workforce.

After 15 years, and upwards of 300 Planning Commission meetings with the City of Homer, **Rick Abboud** is moving on to a new position with York County, South Carolina which is part of the Charlotte metropolitan area. Rick joined the City in 2008 after serving as the planning director for Bethel, AK. He jumped straight into the big job of wrapping up the 2008 Homer Comprehensive Plan and shepherding it through the



Planning Commission, City Council, and the Kenai Peninsula Borough Assembly. During Rick's tenure with the City he has worked with staff, Planning Commission, and Council on projects that have touched every section of Title 21. He collaborated to make additions to code which include solutions relating to steep slope development, grading and filling, over slope development, wind energy, telecommunication towers, storm water, creation of the East End Mixed use District, allowances for accessory dwellings, marijuana businesses, flood hazard ordinances and maps, coastal setbacks, and the creation of the medical district. Rick has also been very active with MAPP, Rotary, the Kenai Peninsula Homelessness Coalition, and served a stint as interim-City manager.

After working for Public Works for over 9 years, **Paul Raymond**, lead equipment operator, is retiring. To complement his enormous skill as an equipment operator, Paul is also a barbecue master. We wish him a happy retirement and his skill both on the job and at the grill will be deeply missed.



**Mike Gilbert** will be promoted to lead equipment operator after the retirement of Paul Raymond. Mike has worked at Public Works as an equipment operator for 10 years and the promotion to lead operator is well earned. We welcome the wealth of skill and experience that Mike brings to the table and wish him the best in his new position.

# City of Homer Roster

Rob Dumouchel, City Manager  
Melissa Jacobsen, MMC, City Clerk/Deputy Director of Administration  
Andrea Browning, Personnel Director  
Rick Abboud, City Planner  
Mark Robl, Chief of Police  
Mark Kirko, Fire Chief  
Bryan Hawkins, Port Director/Harbormaster  
Elizabeth Walton, Finance Director  
Jan Keiser, Public Works Director/City Engineer  
Dave Berry, Library Director  
Mike Illg, Community Recreation Manager  
Julie Engebretsen, Economic Development Manager  
Bill Jirsa, Chief Technology Officer

**Mayor** - Ken Castner (2024)

## City Council

Donna Aderhold (2024)  
Jason Davis (2025)  
Shelley Erickson (2024)  
Storm P. Hansen-Cavasos (2025)  
Rachel Lord (2023)  
Caroline Venuti (2023)

## Commissions & Boards

ADA Advisory Board  
Economic Development Advisory Commission  
Library Advisory Board  
Parks, Art, Recreation and Culture Advisory Commission  
Planning Commission  
Port and Harbor Advisory Commission

# MUNICIPAL ART COLLECTION

Learn more about the municipal art collection at:

[www.cityofhomer-ak.gov/  
prac/city-homer-municipal-art-collection](http://www.cityofhomer-ak.gov/prac/city-homer-municipal-art-collection)



**Homer harbor**, Ed Tussey



**Three Cranes**, artist unknown

## STAY CONNECTED TO CITY COUNCIL

Go to [cityofhomer-ak.gov/cityclerk/stay-connected-city-council](http://cityofhomer-ak.gov/cityclerk/stay-connected-city-council) to find instructions on how to listen, provide testimony, and participate in the meetings via Zoom.

## UPCOMING MEETINGS

May	
3	5:30 p.m. Planning Commission Work Session - Canceled 6:30 p.m. Planning Commission Regular Meeting
8	5 p.m. City Council Committee of the Whole 6 p.m. City Council Regular Meeting
9	6 p.m. Economic Development Advisory Commission
11	5 p.m. ADA Advisory Board Regular Meeting
16	5:30 p.m. Library Advisory Board Regular Meeting
17	5:30 p.m. Planning Commission Work Session 6:30 p.m. Planning Commission Regular Meeting Various USACE Homer Harbor Expansion Charrette (May 17-19)
18	5:30 p.m. Parks, Art, Recreation & Culture Advisory Commission
22	5 p.m. City Council Committee of the Whole 6 p.m. City Council Regular Meeting
24	6 p.m. Port & Harbor Advisory Commission Regular Meeting

## JOIN OUR TEAM

The City of Homer has current Job Openings.  
Sign up for Job Alerts or Apply Online at:  
[cityofhomerak.applicantpro.com/jobs](http://cityofhomerak.applicantpro.com/jobs)

## CURRENT JOB LISTINGS

- Building Custodian, Full-Time
- Deputy City Clerk I, Full Time
- Firefighter/EMT-Basic (EMT-I) or Firefighter/EMT-Advanced (EMT II or III), Full-Time
- Parks Technician, Seasonal
- Utilities Laborer, Seasonal
- Harbor Assistant I, Seasonal
- Treatment Maintenance Technician I or II



## City of Homer

491 E. Pioneer Avenue, Homer, Alaska 99603  
907-235-8121

[www.cityofhomer-ak.gov](http://www.cityofhomer-ak.gov)