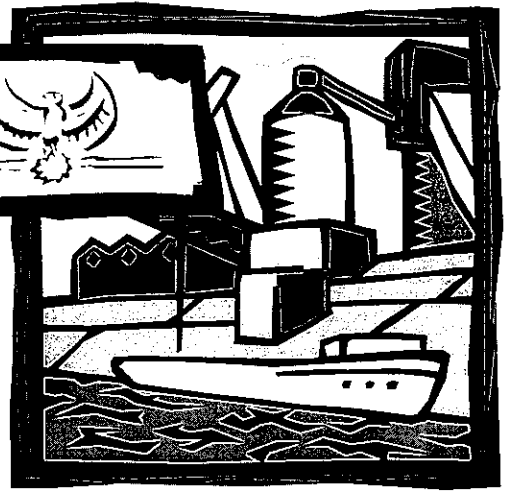
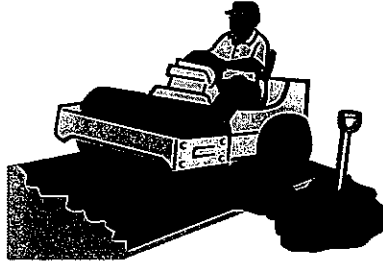
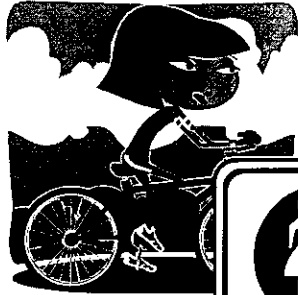
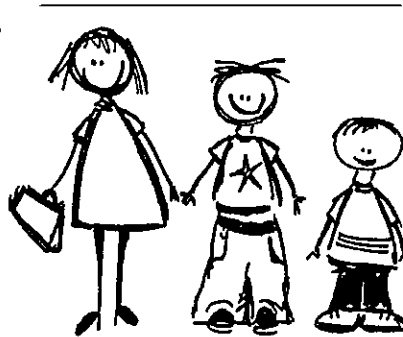
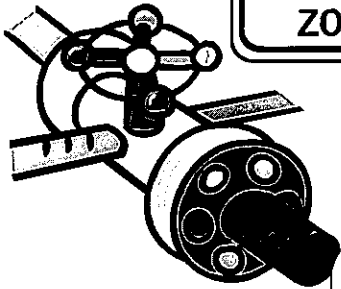


City Council
May 14, 2012
Monday

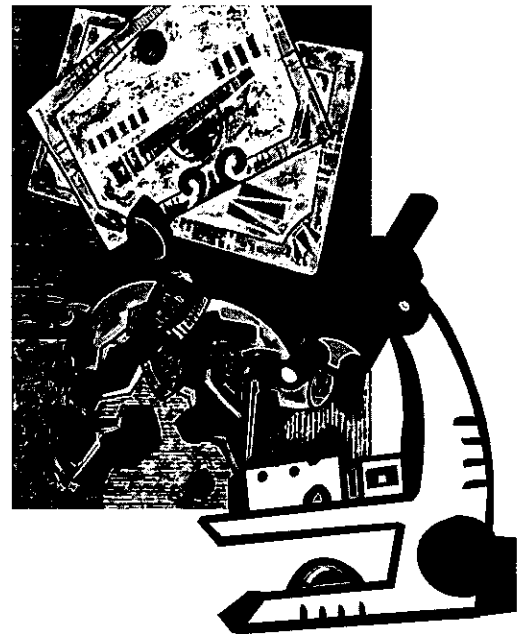


Committee of the Whole
5:00 P.M.
Regular Meeting
6:00 P.M.



Cowles Council Chambers
City Hall
491 E. Pioneer Avenue
Homer, Alaska

Produced and
Distributed by the City Clerk's Office -5/9/2012-rk



May 2012

Monday 14th	CITY COUNCIL Committee of the Whole 5:00 p.m. and Regular Meeting 6:00 p.m.
Tuesday 15th	TRANSPORTATION ADVISORY COMMITTEE Regular Meeting 5:30 p.m.
Wednesday 16th	PLANNING COMMISSION Worksession 5:30 p.m. and Regular Meeting 6:30 p.m. PUBLIC ARTS COMMITTEE Worksession 4:00 p.m. and Regular Meeting 5:00 p.m.
Thursday 17th	LIBRARY LANDSCAPE COMMITTEE Meeting Noon to 1:00 p.m. at Library. PARKS AND RECREATION ADVISORY COMMISSION Regular Meeting 5:30 p.m.
Wednesday 23rd	PORT AND HARBOR ADVISORY COMMISSION Regular Meeting 6:00 p.m.
Thursday 24th	KACHEMAK DRIVE PATH COMMITTEE Meeting 5:30 p.m. PORT AND HARBOR IMPROVEMENT COMMITTEE Meeting 5:30 p.m.
Monday 28th	CITY OFFICES CLOSED in observance of Memorial Day.
Tuesday 29th	CITY COUNCIL Committee of the Whole 5:00 p.m. and Regular Meeting 6:00 p.m.

Regular Meeting Schedule

City Council 2nd and 4th Mondays 6:00 p.m.
Library Advisory Board 1st Tuesday 5:00 p.m.
Economic Development Advisory Commission 2nd Tuesday 6:00 p.m.
Parks and Recreation Advisory Commission 3rd Thursday of the month with exception of December 5:30 p.m.
Planning Commission 1st and 3rd Wednesday 6:30 p.m.
Port and Harbor Advisory Commission 4th Wednesday 5:00 p.m.
Transportation Advisory Committee Quarterly 3rd Tuesday 5:30 p.m.
Public Arts Committee Quarterly 3rd Thursday 5:00 p.m.
Lease Committee Quarterly 2nd Thursday 3:00 p.m.
Permanent Fund Committee Quarterly 2nd Thursday 5:15 p.m.

MAYOR AND CITY COUNCILMEMBERS AND TERMS

JAMES C. HORNADAY, MAYOR – 12
MARY E. (BETH) WYTHER, COUNCILMEMBER – 13
FRANCIE ROBERTS, COUNCILMEMBER – 12
BARBARA HOWARD, COUNCILMEMBER – 14
DAVID LEWIS, COUNCILMEMBER – 14

**BRYAN ZAK, COUNCILMEMBER – 13
BEAUREGARD BURGESS, COUNCILMEMBER – 12**

**City Manager, Walt Wrede
City Attorney, Thomas Klinkner**

**<http://www.cityofhomer-ak.gov/cityclerk> home page access, Clerk's email address is:
clerk@ci.homer.ak.us Clerk's office phone number: direct line 235-3130, other
number 435-3106.**

COWLES COUNCIL CHAMBERS
491 E. PIONEER AVENUE
HOMER, ALASKA
www.cityofhomer-ak.gov



COMMITTEE OF THE WHOLE
5:00 P.M. MONDAY
MAY 14, 2012

MAYOR JAMES C. HORNADAY
COUNCIL MEMBER BETH WYTHE
COUNCIL MEMBER FRANCIE ROBERTS
COUNCIL MEMBER BARBARA HOWARD
COUNCIL MEMBER DAVID LEWIS
COUNCIL MEMBER BRYAN ZAK
COUNCIL MEMBER BEAUREGARD BURGESS
CITY ATTORNEY THOMAS KLINKNER
CITY MANAGER WALT WREDE
CITY CLERK JO JOHNSON

**MEETING NOTICE
COMMITTEE OF THE WHOLE AGENDA**

1. **CALL TO ORDER, 5:00 P.M.**
2. **AGENDA APPROVAL** (Only those matters on the noticed agenda may be considered, pursuant to City Council's Operating Manual, pg. 5)
3. Michelle Drew, Mikunda, Cottrell, & Co., Auditor's Report, 15 minutes.
4. **REGULAR MEETING AGENDA**
5. **COMMENTS OF THE AUDIENCE**
6. **ADJOURNMENT NO LATER THAN 5:50 P.M.**
Next Regular Meeting is Tuesday, May 29, 2012 at 6:00 p.m. and Committee of the Whole 5:00 p.m. All meetings scheduled to be held in the City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

CALL TO ORDER
PLEDGE OF ALLEGIANCE
AGENDA APPROVAL

COWLES COUNCIL CHAMBERS
491 E. PIONEER AVENUE
HOMER, ALASKA
www.cityofhomer-ak.gov



REGULAR MEETING
6:00 P.M. MONDAY
MAY 14, 2012

MAYOR JAMES C. HORNADAY
COUNCIL MEMBER BETH WYTHE
COUNCIL MEMBER FRANCIE ROBERTS
COUNCIL MEMBER BARBARA HOWARD
COUNCIL MEMBER DAVID LEWIS
COUNCIL MEMBER BRYAN ZAK
COUNCIL MEMBER BEAUREGARD BURGESS
CITY ATTORNEY THOMAS KLINKNER
CITY MANAGER WALT WREDE
CITY CLERK JO JOHNSON

REGULAR MEETING AGENDA

Committee of the Whole 5:00 p.m. in Homer City Hall Cowles Council Chambers.

1. CALL TO ORDER, PLEDGE OF ALLEGIANCE

Department Heads may be called upon from time to time to participate via teleconference.

2. AGENDA APPROVAL

(Addition of items to or removing items from the agenda will be by unanimous consent of the Council. HCC 1.24.040.)

3. PUBLIC COMMENTS UPON MATTERS ALREADY ON THE AGENDA

4. RECONSIDERATION

5. CONSENT AGENDA

(Items listed below will be enacted by one motion. If separate discussion is desired on an item, that item may be removed from the Consent Agenda and placed on the Regular Meeting Agenda at the request of a Councilmember.)

- A. Homer City Council unapproved Special and Regular Meeting minutes of April 23, 2012.
City Clerk. Recommend adoption. Page 13

- B. **Memorandum 12-074**, from Mayor, Re: Reappointment of Roberta Highland to the Transportation Advisory Committee and Angie Newby to the Public Arts Committee.
Page 31
Memorandum 12-078 from Transportation Advisory Committee as backup.
Page 33

- C. **Ordinance 12-19**, An Ordinance of the City Council of Homer, Alaska, Accepting and Appropriating a Commercial Vessel Passenger Tax Program Pass Through Grant from the Kenai Peninsula Borough for FY 2011 in the Amount of \$32,687.50, and Authorizing the City Manager to Execute the Appropriate Documents. City Manager. Recommended dates: Introduction May 14, 2012, Public Hearing and Second Reading May 29, 2012.

Page 37

6. **VISITORS**

- A. Michelle Drew, Mikunda, Cottrell, & Co., Auditor's Report, 10 minutes.

7. **ANNOUNCEMENTS/PRESENTATIONS/BOROUGH REPORT/COMMISSION REPORTS**

- A. **Mayor's Proclamation** – Peace Officers Memorial Day May 15 and National Police Week May 13 – 19, 2012

Page 53

- B. **Mayor's Proclamation** - National Public Works Week – May 20 – 26, 2012

Page 55

- C. **Mayor's Proclamation** - National Women's Health Week - May 13 – 19, 2012

Page 57

- D. **Mayor's Proclamation** – Bike to Work Week - May 14 – 18, 2012

Page 59

- E. Borough Report

- F. Commissions/Board Reports:

1. Library Advisory Board
2. Homer Advisory Planning Commission
3. Economic Development Advisory Commission
4. Parks and Recreation Advisory Commission
5. Port and Harbor Advisory Commission

- G. Letter of thanks from Hospice of Homer

Page 61

8. PUBLIC HEARING(S)

A. Port and Harbor Revenue Bond Project

B. Resolution 12-037, A Resolution of the City Council of Homer, Alaska, Amending the City of Homer Fee Schedule for Port and Harbor Fees for the Purpose of Repaying a Revenue Bond and Contributing to the Port and Harbor Enterprise Reserves. City Clerk/Port and Harbor Advisory Commission. Page 65

C. Resolution 12-038, A Resolution of the City Council of Homer, Alaska, Amending the Port of Homer Terminal Tariff No. 600 for the Purpose of Repaying a Revenue Bond and Contributing to the Port and Harbor Enterprise Reserves. City Clerk/Port and Harbor Advisory Commission. Page 77

D. Ordinance 12-17(S), An Ordinance of the City Council of Homer, Alaska, Appropriating \$30,000 from the Port and Harbor Reserves to Make Emergency Repairs and/or Replace Damaged Float Piles in the Homer Harbor and Authorizing the City Manager to Execute a Change Order to the Existing Contract with Pacific Pile and Marine to Conduct the Harbor Float Piling Repair and Replacement Work in an Amount Not to Exceed \$30,000. City Manager. Page 85

Memorandums 12-068 and 12-077 from Port and Harbor Director as backup.

Page 91

E. Ordinance 12-18, An Ordinance of the City Council of Homer, Alaska, Amending the 2012 Operating Budget to Allow for the Completion of the 2012 Streets Repaving Project Utilizing Homer Accelerated Roads and Trails Program (HART) Funds in an Amount Not to Exceed \$900,000. City Manager/Public Works Director. Introduction April 23, 2012, Public Hearing and Second Reading May 14, 2012.

Page 95

Memorandum 12-065 from Public Works Director as backup.

Page 99

9. ORDINANCE(S)

A. Ordinance 12-20, An Ordinance of the City Council of Homer, Alaska, Amending the FY 2012 Operating Budget by Appropriating and Transferring \$3,000 from the General Fund Reserve (Fund Balance) to the Public Arts Committee for the Baycrest Overlook Improvement Gateways Project. City Manager. Recommended dates: Introduction May 14, 2012, Public Hearing and Second Reading May 29, 2012. Page 105

Memorandums 12-070 and 12-071 from Public Arts Committee as backup.

Page 109

10. CITY MANAGER’S REPORT

A. City Manager’s Report Page 119

B. Bid Report Page 121

11. CITY ATTORNEY REPORT

12. COMMITTEE REPORT

A. Public Arts Committee

B. Transportation Advisory Committee

C. Permanent Fund Committee

D. Lease Committee

E. Port and Harbor Improvement Committee

F. Employee Committee Report

13. PENDING BUSINESS

A. **Resolution 11-090**, A Resolution of the City Council of Homer, Alaska, Supporting the Concept and Construction of Non-Motorized Pathways to Increase the Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission. Page 123

Resolution 11-090(S), A Resolution of the City Council of Homer, Alaska, Supporting ~~The Concept~~ And Construction of a Non-Motorized Pathways to Increase ~~The Safety~~ for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the **Homer** City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission. Page 125

14. **NEW BUSINESS**

15. **RESOLUTIONS**

- A. **Resolution 12-039**, A Resolution of the City Council of Homer, Alaska, Accepting the 2011 Comprehensive Annual Financial Report with Audit and Financial Statements and Acknowledging the Management Letter Submitted by the City's Independent Auditor, Mikunda, Cottrell, & Co. and Authorizing the City Manager to Execute the Financial Report. City Manager/Finance. Page 287
- B. **Resolution 12-040**, A Resolution of the City Council of Homer, Alaska, Extending the City of Homer Prisoner Meal Service Contract for One Year to the Firm of Caribou Family Restaurant of Homer, Alaska, and Authorizing the City Manager to Execute the Appropriate Documents. City Clerk. Page 289
- Memorandum 12-075 from Police Chief as backup. Page 291
- C. **Resolution 12-041**, A Resolution of the City Council of Homer, Alaska, Designating Community Economic Development Strategy (CEDS) Priorities and Requesting that the City Economic Development Advisory Commission Review These Priorities and Provide Recommendations Regarding Implementation Strategies Including Timetables, Responsible Parties, and Funding. Wythe. Page 295
- D. **Resolution 12-042**, A Resolution of the City Council of Homer, Alaska, Requesting That the Kenai Peninsula Borough Transfer its Share of State Fisheries Business Tax Proceeds that are Collected in the City of Homer to the City of Homer to Assist It with Maintenance, Repair, and Replacement of Critical Port and Harbor Infrastructure. Howard, Lewis, Wythe. Page 297
- E. **Resolution 12-043**, A Resolution of the City Council of Homer, Alaska, Accepting and Approving Recommendations Submitted by the Port and Harbor Improvement Committee Regarding Capital Improvements in the Harbor and the Funding Thereof and Authorizing the City Manager to Prepare the Documents Necessary for Grant Funding, a Revenue Bond Sale, and the Fee Adjustments Necessary to Service the Bonds. Howard, Lewis, Wythe. Page 299
- Memorandum 12-076 from Port and Harbor Improvement Committee as backup. Page 301
- Memorandum 12-079 from Port and Harbor Advisory Commission as backup. Page 305
- F. **Resolution 12-044**, A Resolution of the City Council of Homer, Alaska, Expressing Its Intent to Initiate a Broad and Inclusive Community Review Process on the Merits of Public Financing for Construction of a Natural Gas Distribution System Within the City and Setting a Workshop Date for June 4, 2012 to Begin the Process. Burgess.

- G. **Resolution 12-045**, A Resolution of the City Council of Homer, Alaska, Awarding the Wastewater Treatment Plant Boiler Replacement Project to the Firm of Orca Specialties, Inc. of Homer, Alaska, in the Amount of \$17,437.50 and Authorizing the City Manager to Execute the Appropriate Documents. City Clerk/Public Works Director.

Page 321

Memorandum 12-081 from Public Works Director as backup.

Page 323

- H. **Resolution 12-046**, A Resolution of the City Council of Homer, Alaska, Authorizing the City Manager to Enter Into a New Short Term Lease with the Homer Boys and Girls Club and to Extend the Club's Occupancy of the Lower Level of the Homer Education and Recreation Complex Building (HERC) Until September 1, 2012. Burgess/Howard.

Page 327

- 16. **COMMENTS OF THE AUDIENCE**
- 17. **COMMENTS OF THE CITY ATTORNEY**
- 18. **COMMENTS OF THE CITY CLERK**
- 19. **COMMENTS OF THE CITY MANAGER**
- 20. **COMMENTS OF THE MAYOR**
- 21. **COMMENTS OF THE CITY COUNCIL**

22. **ADJOURNMENT**

Next Regular Meeting is Tuesday, May 29, 2012 at 6:00 p.m. and Committee of the Whole 5:00 p.m. All meetings scheduled to be held in the City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

**PUBLIC COMMENTS UPON MATTERS
ALREADY ON THE AGENDA**



RECONSIDERATION

CONSENT AGENDA

Session 12-12 a Special Meeting of the Homer City Council was called to order on April 23, 2012 at 4:00 p.m. by Mayor James C. Hornaday at the Homer City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska, and opened with the Pledge of Allegiance.

PRESENT: COUNCILMEMBERS: BURGESS, HOWARD, LEWIS, ROBERTS,
WYTHE

ABSENT: COUNCILMEMBERS: ZAK (excused)

STAFF: CITY MANAGER WREDE
CITY CLERK JOHNSON

Councilmember Zak has requested excusal.

Mayor Hornaday ruled Councilmember Zak's absence as excused. There was no objection from the Council.

AGENDA APPROVAL (Only those matters on the noticed agenda may be considered, pursuant to City Council's Operating Manual, pg. 5)

The agenda was approved by consensus of the Council.

NEW BUSINESS

A. **Memorandum 12-062**, From City Clerk, Re: Request for Executive Session Pursuant to AS 44.62.310 (a-c)(1-3) Matters, the Immediate Knowledge of Which Would Clearly Have an Adverse Effect Upon the Finances of the Public Entity (Settlement Negotiations with Nicholas Kazan).

Mayor Hornaday called for a motion to approve the recommendations of Memorandum 12-062 to meet in Executive Session to discuss Settlement Negotiations with Nicholas Kazan.

WYTHE/ROBERTS - SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Mayor Hornaday called for a motion to adjourn to Executive Session.

WYTHE/ROBERTS – SO MOVED.

There was no discussion.

VOTE: YES. ROBERTS, WYTHE, HOWARD, BURGESS, HOWARD

Motion carried.

Council adjourned to Executive Session at 4:03 p.m. and reconvened the meeting at 4:20 p.m.

Mayor Pro Tempore Wythe stated Council met with the City Manager and he has been authorized to reimburse Mr. Kazan for the cost of survey fees to facilitate easement access.

COMMENTS OF THE AUDIENCE

There were no comments of the audience.

ADJOURNMENT

There being no further business to come before the Council, Mayor Hornaday adjourned the meeting at 4:21 p.m. The next Regular Meeting is Monday, May 14, 2012 at 6:00 p.m. and Committee of the Whole 5:00 p.m. A Worksession is scheduled for April 30, 2012 from 5:00 to 7:00 p.m. All meetings scheduled to be held in the City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

JO JOHNSON, CMC, CITY CLERK

Approved: _____

Session 12-13 a Regular Meeting of the Homer City Council was called to order on April 23, 2012 at 6:06 p.m. by Mayor James C. Hornaday at the Homer City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska, and opened with the Pledge of Allegiance.

PRESENT: COUNCILMEMBERS: BURGESS, HOWARD, LEWIS, ROBERTS,
WYTHE, ZAK (telephonic)

STAFF: CITY MANAGER WREDE
CITY CLERK JOHNSON
CITY PLANNER ABOUD
PORT AND HARBOR DIRECTOR HAWKINS

Council conducted a Special Meeting from 4:00 p.m. to 4:21 p.m., adjourning to Executive Session to discuss Settlement Negotiations with Nicholas Kazan. From 5:00 p.m. to 5:58 p.m. Council met as a Committee of the Whole to hear from Lobbyist Linda Anderson and discuss Regular Meeting Agenda items.

Councilmember Zak has requested telephonic participation.

Mayor Hornaday called for a motion to approve Councilmember Zak's participation by telephone.

LEWIS/BURGESS – SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION.

Motion carried.

Department Heads may be called upon from time to time to participate via teleconference.

AGENDA APPROVAL

(Addition of items to or removing items from the agenda will be by unanimous consent of the Council. HCC 1.24.040.)

The agenda was approved by consensus of the Council with the following changes: **RECONSIDERATION** - Ordinance 12-01(S)(A), Regulation of Signs. Written public comment; **COMMITTEE REPORT** - Memorandum 12-070, from Public Arts Committee, Re: Baycrest Overlook Improvement Gateways Project and Memorandum 12-071, from Public Arts Committee, Re: Request for Support for Funding Contribution for the Baycrest Overlook Improvement Gateways Project.

PUBLIC COMMENTS UPON MATTERS ALREADY ON THE AGENDA

Susannah Webster, Homer resident and daycare provider, commented there is a need for child care providers for infants and toddlers.

Pete Wedin, city resident, supports Resolution 12-034. Halibut is fully allocated and longliners were reduced by 50% since quotas were issued. The charters are fighting to protect the two halibut limit; subsistence fishermen are fighting to find fish close to home.

Lisa Aslin, Homer resident and preschool owner, supports early childhood education.

Monte Davis, city resident and Executive Director of Homer Chamber of Commerce, expressed support for Ordinance 12-01(S)(A) as it stands. Mom and pop operations are the heart of the community and have limited financial means to get the message out. There is no record of an accident or safety incident as a result of sandwich boards.

Ingrid Harrald, city resident, supports Ordinance 12-01(S)(A), agreeing with Mr. Davis. Safety issues include no sidewalk on Main Street and lack of snow removal on Poopdeck Trail.

Jackie Dentz, city resident and restaurant owner, supports Ordinance 12-01(S)(A). If Council is not going to represent the business owners they cannot afford to continue doing business to an unfriendly Council.

Sue Post, Homer resident and business owner, expressed opposition to sandwich boards. Her business has been successful for many years without the sandwich boards.

Jerri Martin, Homer resident and Spit business owner, supports Ordinance 12-01(S)(A) as adopted. She uses sandwich boards to advertise available charter trips to late night strollers.

Al Waddell, Homer resident and business owner, told Council to leave the sign ordinance alone and let people try and do business. Homer has the highest rate of sales tax in the state. He is selling his business and equipment due to high property taxes.

Karin Marks, city resident and business owner, supports Ordinance 12-01(S)(A) as adopted.

Holly VanPelt, city resident and owner of a vacation rental, supports Ordinance 12(S)(A) as adopted.

Adrienne Sweeney, city resident and business owner, supports Ordinance 12-01(S)(A). Signs work and the city needs to trust the small businesses to do the right thing.

Fred Kaatz, Homer resident and business owner, supports Ordinance 12-01(S)(A). If you don't tell, you don't sell. It is critical for him to have a sandwich board to let people know he is there.

Nelton Palma, city resident and business owner, supports Ordinance 12-01(S)(A) as adopted. He has heard lots of comments from people that would like less restrictions.

Beth Cumming, city resident, supports Resolution 11-090(S) to construct a non motorized path along Kachemak Drive.

WYTHE/LEWIS - MOVED TO SUSPEND THE RULES SO WE CAN MEET WITH REPRESENTATIVE SEATON AT THIS TIME.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

VISITORS

A. **Representative Paul Seaton, Legislative Update**

Representative Seaton reported he is staying busy in Juneau with the special legislative session. Passed by the Senate and House are a number of road projects including Anchor Point to Baycrest, Greer Road, and Kachemak City roads. The Homer High School track, Homer gas line, Fishing Lagoon improvements, Diamond Ridge fire station, Pratt Museum design, mariculture projects, I-pod project with the schools, and the Kachemak Bay Campus are all in the budget to be forwarded to the Governor.

Mayor Hornaday commended Representative Seaton for his good work, noting the harbor improvements were turned into statewide transportation projects.

Representative Seaton reported the Seldovia harbor is slated for funding since it had matching funds. In the future he will look for more cruise ship projects along with other possibilities for the harbor.

PUBLIC COMMENTS UPON MATTERS ALREADY ON THE AGENDA (continued)

Larry Slone, city resident, advocated for sign boards as they create a festive environment. They tell people that you have something interesting and for a limited time.

Dan Coe, Homer resident and business owner, has painted numerous sandwich board signs. He encouraged Council to pay attention to the small businesses and let the cash flow to businesses.

RECONSIDERATION

A. **Reconsideration issued by Councilmember Roberts:**

Ordinance 12-01(S)(A), An Ordinance of the City Council of Homer, Alaska, Amending Homer City Code 21.60.040, Definitions; Homer City Code 21.60.060, Signs Allowed on Private Property With and Without Permits; Homer City Code 21.60.070, Permits Required; Homer City Code 21.60.080 Design, Construction, and Maintenance; Homer City Code 21.60.090, Signs in the Public Right-Of-Way; Homer City Code 21.60.095, Electoral Signs; Homer City Code 21.60.100, Signs Exempt from Regulation Under this Chapter; Homer City Code 21.60.110 Signs Prohibited Under this Chapter; Homer City

Code 21.60.130, Temporary Signs-Private Property; Homer City Code 21.60.150, Time of Compliance-Nonconforming Signs and Signs Without Permits; and Homer City Code 21.60.170, Enforcement and Remedies; and Repealing Homer City Code 21.60.120, General Permit Procedures; Homer City Code 21.60.140, Temporary Signs-Public Rights-Of-Way; and Homer City Code 21.60.160, Violations; Regarding the Regulation of Signs. Planning/City Attorney.

ROBERTS/WYTHE - MOTION FOR THE RECONSIDERATION OF ORDINANCE 12-01(S)(A).

Councilmember Roberts moved for the reconsideration as there was a substantial change at the last minute and not all public testimony had been heard. It is important to honor the Planning Commission who Council sent the ordinance to twice.

Councilmember Zak thanked the business owners who communicated an important message. We have to listen to the small business owners.

VOTE: YES. ROBERTS

VOTE: NO. WYTHE, ZAK, HOWARD, BURGESS, LEWIS

Motion failed.

CONSENT AGENDA

(Items listed below will be enacted by one motion. If separate discussion is desired on an item, that item may be removed from the Consent Agenda and placed on the Regular Meeting Agenda at the request of a Councilmember.)

- A. Homer City Council unapproved Special and Regular Meeting minutes of April 9, 2012. City Clerk. Recommend adoption.
- B. **Memorandum 12-063**, from Mayor, Re: Reappointment of Trina Fellows and Michele Miller to the Public Arts Committee.
- C. **Resolution 12-033**, A Resolution of the City Council of Homer, Alaska, Authorizing Lease Negotiations for Lot 88-3 on the Homer Spit Between the City and William Sullivan, dba Dockside II, and Finding that Awarding a New Lease to the Current Lessee Without a Competitive Bidding Process is in the Public Interest. City Manager/Lease Committee.

Memorandum 12-064 from Lease Committee as backup.

Mayor Hornaday called for a motion to approve the recommendations of the consent agenda as read.

HOWARD/LEWIS - SO MOVED.

Asked by Councilmember Lewis if the lease was at fair market value, City Manager Wrede answered it is at the appraised value.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

VISITORS (continued)

B. Linda Anderson & Yuri Morgan, Anderson Group, Lobbyists

Linda Anderson, City Lobbyist, provided an overview of the legislative session. She is hopeful about the sessions and dialogue on behalf of Homer. Revenue sharing funds were increased \$5M more than last year. As the State makes more money they share revenue dollars with municipalities. Although they would have liked to see support from the Governor's budget going in, Homer did not receive any projects. She encouraged City Council to use an aggressive strategy to get funding at the beginning of the Governor's budget process in October. It is always easier if the Governor puts the money in first before the session begins in Juneau. The fact that Homer received \$6.5M in cruise ship money last year was touted as a reason that Homer did not get port and harbor money this year. Mrs. Anderson suggested large port and road projects be addressed early in the process as it may take a few years to receive funding. The gas line funds are hugely important to Homer. The entire state knows and hopefully so does the Governor. She encouraged a few councilmembers to meet with the Governor after the special session ends to let him know the effort and personal financial contributions. When the capital budget is transmitted from the Legislative body to the Governor he has 30 days to decide which projects remain within the budget. She encouraged the communication with the Governor the first part of June.

C. Jenny Martin, Families First, Week of the Young Child

Lolita Brache and Bonnie Betley, Co-Chairs of Families First, provided information on local organizations working together to enhance the community's well being. Early education services are vital for children in their first five years of life. The number of words a child learns by age three is a predictor as to how they will do in school. Children that start behind have a tendency to stay behind during their academic careers. Children in stressful or violent homes will have developmental delays, social/emotional problems, medical issues, and mental health problems. Preventative work can make a big difference. Families First promotes good health, good nutrition, supportive bonds with caregivers. High quality childcare, preschool opportunities, support to families, and stimulating environments are important. It also increases the economics and makes for a stronger community.

For over three years Families First has worked to improve early education services for children birth to 8 years and their families. They have collaborated with MAPP on overlapping goals. For the safety and maintenance at Baycrest Park, Families First installed a new slide, boulders, and a Porta-Potty. They have promoted indoor play spaces including the community rec toddler play group and support for HoPP both financially and with volunteers. Families First has acquired a new swing at Bayview Park for children with disabilities. Recently they presented to the

Economic Development Advisory Commission to develop short and long term strategies to enhance early education as developed in the Comprehensive Economic Development Strategy (CEDS). People are encouraged to attend the activities for the Week of the Young Child, www.pop411.org. The Strengthening Families Communities Café discussion will be held April 28th from 11:00 a.m. to 2:00 p.m. at Kachemak Kids and the Christian Community Church. Charlie Johansen will be providing information on the five protective factors for all families and communities and gathering feedback.

Councilmember Howard congratulated Bonnie Betley on her Women of Distinction award. Any woman that can get a Porta -Potty at a public park deserves an award.

- D. **Megan Murphy & Esther Hammerschlag**, Mobilizing for Action through Planning & Partnerships (MAPP), Community Health Improvement Project and Homer Prevention Project, 10 minutes.

Megan Murphy and Esther Hammerschlag made a presentation of the two related projects: MAPP and the Homer Prevention Project. Both programs will provide economic and physical health to the community based on a national public health framework. Data collection began in November 2008 and community visioning meeting followed. Priority themes and workgroups were identified. The object is to provide a proactive, resilient, innovative community.

The Homer Prevention Project is a five step process:

1. Data collection & needs assessment
2. Build community capacity to implement prevention efforts
3. Strategic planning
4. Implementation of environmental prevent strategies
5. Evaluation of project

An advisory workgroup is being formed with the first meet April 27th from 8:00 to 10:00 a.m. at the college.

ANNOUNCEMENTS/PRESENTATIONS/BOROUGH REPORTS

REPORT/COMMISSION

- A. **Mayor's Proclamation** – Week of the Young Child - April 21 - 28, 2012

Mayor Hornaday read the proclamation and presented it to Jenny Martin.

- B. **Mayor's Proclamation** – Municipal Clerks Week - April 29 - May 5, 2012

Mayor Hornaday read the proclamation and presented it to City Clerk Jo Johnson.

- C. Borough Report

- D. Commissions/Board Reports:

1. Library Advisory Board
2. Homer Advisory Planning Commission
3. Economic Development Advisory Commission
4. Parks and Recreation Advisory Commission

Bumppo Bremicker and Dave Brann, Parks and Recreation Advisory Commissioners, reported on Resolution 11-090. The commission worked on the resolution and sent it to Council who referred it to the Planning Commission. Backup for the substitute resolution included a proposed funding plan and preliminary engineering plan for the Kachemak Drive path. The trail options were simplified with a trail off the road that is built in utility easements. Funding would be available with Homer Accelerated Roads and Trails (HART) funds; \$100,000 to \$120,000 goes in the trail portion of the HART fund each year. They suggest using those funds. It may take ten years of funding, but it is a priority project connecting East Road past the Spit. It is a big safety issue for people riding bikes. The path would be built on utility easements, not in the DOT right-of-way. With a lot of volunteers working hard it could save a lot of money. The Kachemak Drive Path Committee has worked hard on the project, along with Kevin Walker, a retired state road engineer, who did the mapping.

Also suggested was the sale of a city lot on Kachemak Drive, KPB parcel number 179-100-01 assessed at \$38,100. It could be sold and the funds utilized for the project. Other funds may come from The Homer Foundation, Rasmussen Foundation, Alaska Trails Initiative Grant, private donations, fundraisers, a STIP project, and a series of corporate grants.

The context of the path has already been approved by City Council in the Non Motorized Transportation and Trails Plan (NMTTP) and the Capital Improvement Plan. The committee is looking for support to start the process. They are not asking the City to obligate anything more than what is in the HART fund. The trail will be an 8 ft. wide compacted gravel path, not a paved path. In the future it could be paved if that was thought to be the best option.

Much of the proposed trail information was not included with the council packet; therefore, Council was not properly informed on the trail.

The trail would be ADA accessible with one area requiring a 3 to 5 degree slope. All plans for the trail are in the NMTTP. Commissioner Brann relies on a trail design manual for the state of Minnesota. The committee has been working on bike signage on Kachemak Drive, to provide four signs with one on each end and two in the middle. They are hopeful they can get sign posts up by June. New commissioner Lars Bell is welcomed to the commission. They still have one opening on the commission. Additionally they are working on the Karen Hornaday Master Plan that includes trails around the park, a bridge connection, the campground, and the day use area. The Kachemak Bay Rotary Club through the Adopt-a-Park Program painted the bathrooms at Ben Walters Park.

5. Port and Harbor Advisory Commission

PUBLIC HEARING(S)

None.

ORDINANCE(S)

A. If reconsidered:

Ordinance 12-01(S)(A), An Ordinance of the City Council of Homer, Alaska, Amending Homer City Code 21.60.040, Definitions; Homer City Code 21.60.060, Signs Allowed on Private Property With and Without Permits; Homer City Code 21.60.070, Permits Required; Homer City Code 21.60.080 Design, Construction, and Maintenance; Homer City Code 21.60.090, Signs in the Public Right-Of-Way; Homer City Code 21.60.095, Electoral Signs; Homer City Code 21.60.100, Signs Exempt from Regulation Under this Chapter; Homer City Code 21.60.110 Signs Prohibited Under this Chapter; Homer City Code 21.60.130, Temporary Signs-Private Property; Homer City Code 21.60.150, Time of Compliance-Nonconforming Signs and Signs Without Permits; and Homer City Code 21.60.170, Enforcement and Remedies; and Repealing Homer City Code 21.60.120, General Permit Procedures; Homer City Code 21.60.140, Temporary Signs-Public Rights-Of-Way; and Homer City Code 21.60.160, Violations; Regarding the Regulation of Signs. Planning/City Attorney.

Ordinance 12-01(S)(A) failed reconsideration under RECONSIDERATION.

B. **Emergency Ordinance 12-17**, An Emergency Ordinance of the City Council of Homer, Alaska, Appropriating \$30,000 from the Port and Harbor Reserves to Make Emergency Repairs and/or Replace Damaged Float Piles in the Homer Harbor and Authorizing the City Manager to Execute a Change Order to the Existing Contract with Pacific Pile and Marine to Conduct the Harbor Float Piling Repair and Replacement Work in an Amount Not to Exceed \$30,000. City Manager.

Memorandum 12-068 from Port and Harbor Director as backup.

Mayor Hornaday called for a motion for the adoption of Ordinance 12-17 for first and final reading.

WYTHE/LEWIS – SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Mayor Hornaday called for a motion to introduce Ordinance 12-17 as a regular permanent ordinance and schedule a public hearing and second reading for May 14.

WYTHE/LEWIS - SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- C. **Ordinance 12-18**, An Ordinance of the City Council of Homer, Alaska, Amending the 2012 Operating Budget to Allow for the Completion of the 2012 Streets Repaving Project Utilizing Homer Accelerated Roads and Trails Program (HART) Funds in an Amount Not to Exceed \$900,000. City Manager/Public Works Director. Recommended dates: Introduction April 23, 2012, Public Hearing and Second Reading May 14, 2012.

Memorandum 12-065 from Public Works Director as backup.

Mayor Hornaday called for a motion for the adoption of Ordinance 12-18 for introduction and first reading by reading of title only.

WYTHE/ROBERTS - SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

CITY MANAGER'S REPORT

- A. City Manager's Report

NATURAL GAS UPDATE

This Legislative capital budget this year once again contains funding for construction of a natural gas transmission line from Anchor Point to Homer. As some of you know, it was a wild and bumpy ride getting there, to say the least. The Legislative grant funding will come to the City of Homer and the amount is \$8.15 Million. Combined with the \$2.5 Million local match, (raised through the \$1.00 per mcf tariff increase), this will be enough to construct the project.

We are optimistic that the Governor will not veto this line item again. I say that for several reasons. First, he has stated in a letter to the Mayor, in a press interview, and in other conversations, that he was pleased with Homer's willingness to contribute to the cost of the transmission line and its progress in planning for the distribution system. Second, he has stated that he is unlikely to veto many projects, if any this year, because the Legislature stayed within the agreed upon budget caps. So, hold on Homer. It looks like getting natural gas to the area is

much closer than it ever has been before. If the Governor does not veto this item, construction work could start as early as this fall.

Now is the time to really focus on whether the City Council wants to participate and provide financing for the local distribution system. Anchor Point and Kachemak City have already decided to do so. The Borough has established a 200 plus lot LID in Anchor Point and there is discussion about more.

We have talked about this issue in workshops and last year a special committee discussed the topic but did not make specific recommendations. Council has adopted several ordinances that put the City into position to accept gas utility line construction and to finance construction if it chooses. The next big decision, and it is becoming increasingly urgent, is if and how the Council will provide financial assistance to its businesses and residences so that they can hook up quickly. If the Council decides to move ahead with an LID or other financing strategy, the goal would be to have it approved and in place by this fall so that cost estimates will be locked in place and Enstar can make construction plans for the 2013 construction season.

Attached is a copy of a memorandum that I prepared for the workshop we had back in February. Since that time, there have been a number of new developments. They include:

- Enstar has provided cost estimates for three separate build-out scenarios. One is for the “core area”, one for the entire City limits, and one for the core area plus a loop that includes West Hill Road, Skyline Drive, and East Hill Road.
- The City Attorney has determined that lots which front the main transmission line being financed by the State cannot be included in an LID. This is important because there are many such lots and the street main construction costs will be borne by the other properties in the LID. This will drive up the cost per lot from prior estimates.
- The Legislature adopted SB 25. This bill allows AIDEA to provide direct loans for gas pipeline construction and it may provide an alternative source of low interest loans for the distribution system, potentially saving lots of money in bond closing costs and interest payments.
- The Economic Development Commission recently adopted a motion recommending that the Council finance construction of the distribution system using an LID and that the LID encompass the entire town.

Suggested Course of Action:

Step 1. Workshop: The first thing the Council must decide is whether it wants to provide financing for construction of the distribution system. If the answer is yes, then the next question is how. For example, it could simply make LIDs available to any area that wants to petition for them. Or, it could initiate an LID on its own that encompassed the core area or the entire town (or some other variation). There is a lot of information that would need to be considered in making this decision (maps, cost estimates, amortization schedules, etc.). That is why I suggest a workshop first, so that the Council can have as much information available to it as possible

before making decisions. This workshop should be widely advertised and perhaps a public hearing might be added to the regular meeting agenda.

Step 2. Initiate an LID under HCC 17.04.040. There are advantages to the Council initiating an LID rather than waiting for individual neighborhoods to get organized. If the Council adopted a resolution initiating an LID under HCC 17.04.040, it would set in motion a whole chain of events including community meetings, development of a specific improvement plan, public hearings, more votes by the Council and so on. The ordinance would be followed from that point on.

I look forward to discussing this issue during the Manager's Report and perhaps during the Committee of the Whole, if there is time.

City Manager Wrede referenced the many projects that include completion of the Spit trail, staging areas for cruise ship passengers, and restrooms. Now is the time for Council to begin a serious dialogue about the gas line. The process will be a workshop, community forums, and ways to get information to the public. We need Enstar at the workshop with a build out of the project. Council needs to make a decision if the City will help participate in financing the build out. Kachemak City is currently in the objection/comment period of their LID. They have spoken to the bond bank and the amount of the money they wish to borrow is not enough on their own. The Homer and Kachemak City gas line projects could be combined. It would be good for us to have a plan in effect by early fall. Enstar would then know and could start the project next summer.

Councilmember Roberts asked if Enstar's cost estimates on the three different ways to provide natural gas to Homer would be provided at the worksession. The first worksession could be scheduled for Monday, June 4th.

Asked by Councilmember Lewis what was going to be done with the dead trees along Pioneer Avenue, City Manager Wrede answered he did not have a good sense of the damage the moose have done. There is a budget for landscaping along Pioneer Avenue generally used for flowers that could replace trees.

B. Bid Report

CITY ATTORNEY REPORT

COMMITTEE REPORT

A. Public Arts Committee

1. Memorandum 12-070 from Public Arts Committee, Re: Baycrest Overlook Improvement Gateways Project.

2. Memorandum 12-071 from Public Arts Committee, Re: Request for Support for Funding Contribution for the Baycrest Overlook Improvement Gateways Project.

- B. Transportation Advisory Committee
- C. Permanent Fund Committee
- D. Lease Committee
- E. Port and Harbor Improvement Committee

Port and Harbor Improvement Committee Chair Barbara Howard reported the committee has been very busy. On April 19th they had a public hearing and had seven speakers. On Wednesday, April 25th Councilmember Wythe, Harbormaster Hawkins, and City Manager Wrede will be on KBBI's Coffee Table at 9:00 a.m. That evening the Port and Harbor Advisory Commission has a public hearing at 5:00 p.m. There is a question and answer fact sheet available. A presentation to City Council is scheduled for May 14th.

- F. Employee Committee Report

PENDING BUSINESS

- A. **Memorandum 12-056**, from Mayor, Re: Appointments of Ken Castner, Bob Howard, Sharon Minsch, Lloyd Moore, Terry Yager, and Councilmember Mary E. (Beth) Wythe to the Water and Sewer Rate Task Force.

Memorandum 12-069 from City Clerk as backup.

ROBERTS/WYTHE - MOVED FOR APPROVAL OF MEMORANDUM 12-056.

Councilmember Burgess referenced the attorney's opinion of Lloyd Moore's appointment to the task force. Conflicts of interest are outlined in the ethics code. Mr. Moore holds a controlling interest in a company that exceeds the monetary limits. If Council decides to represent large water users, we should also represent residents who are not users of water and sewer. Their tax dollars subsidize the water/sewer system when budgetary adjustments are made to supplement the shortfalls of water and sewer.

Councilmember Wythe personally does not have access to water/sewer and will represent that group. It feels discriminatory to exclude Lloyd because he is a party of two water haulers. The task force is not going to set the rates; they will send a recommendation to Council. To exclude him is discriminating against water haulers. The water delivery service is one of the major users of the system. Commercial and residential users will be represented.

WYTHE/ROBERTS - MOVED TO AMEND THE APPOINTMENT LIST TO INCLUDE COUNCILMEMBER HOWARD AS THE SECOND COUNCILMEMBER SEAT OF THE COUNCIL.

There was no discussion.

VOTE: (amendment) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Six people applied to the task force within the time limit, two applied after the deadline and one was told he could not apply since the deadline had passed. Councilmember Wythe applied timely and Council has appointed Councilmember Howard to fill the other council seat.

VOTE: (main motion as amended) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- B. **Resolution 11-090**, A Resolution of the City Council of Homer, Alaska, Supporting the Concept and Construction of Non-Motorized Pathways to Increase the Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

Resolution 11-090(S), A Resolution of the City Council of Homer, Alaska, Supporting ~~The Concept And~~ Construction of a Non-Motorized Pathways to Increase ~~The~~ Safety for Motorized and Non-Motorized Users Along Kachemak Drive Located Within the Homer City Limits, from the Base of the Homer Spit to East End Road. Lewis/Zak/Parks and Parks and Recreation Advisory Commission.

Motion on the floor from September 12, 2011 – Motion for the adoption of Resolution 11-090 by reading of title only. Council referred Resolution 11-090 to Planning and Zoning due to issues of land use and utility right-of-ways. Some easements in place for sewer and water include limitations. The proposed trail is a land use issue rather than recreation.

Mayor Hornaday called for a motion to substitute Resolution 11-090(S) for Resolution 11-090.

WYTHE/LEWIS - SO MOVED.

WYTHE/ROBERTS - MOVED TO POSTPONE TO THE NEXT MEETING.

Reference information pertaining to the support of the trail was requested for the next packet. It should include page numbers from the following:

- Homer Non Motorized Transportation and Trails Plan
- Homer Area Transportation Plan
- Climate Action Plan
- Homer Accelerated Roads and Trails Policy Manual
- Capital Improvement Plan
- Planning Commission's recommendation

VOTE: (postponement) YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

NEW BUSINESS

RESOLUTIONS

- A. **Resolution 12-034**, A Resolution of the City Council of Homer, Alaska, Urging the North Pacific Fishery Management Council to Adopt Measures that Reduce the Halibut Prohibited Species Catch in the Gulf of Alaska Groundfish Fisheries. Lewis.

Mayor Hornaday called for a motion for the adoption of Resolution 12-034 by reading of title only.

LEWIS/BURGESS – SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- B. **Resolution 12-035**, A Resolution of the City Council of Homer, Alaska, Approving a New Five Year Lease at the Homer Airport Terminal for Hertz / Pioneer Car Rentals Inc. and Authorizing the City Manager to Execute the Appropriate Documents. City Manager.

Memorandum 12-066 from City Manager as backup.

Mayor Hornaday called for a motion for the adoption of Resolution 12-035 by reading of title only.

WYTHE/LEWIS - SO MOVED.

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

- C. **Resolution 12-036**, A Resolution of the City Council of Homer, Alaska, Approving a New Short Term Lease (Six Months) for Peninsula Scrap and Salvage on a Portion of Lot 12, Homer Spit Subdivision No. 5 and Authorizing the City Manager to Execute the Appropriate Documents. City Manager.

Memorandum 12-067 from City Manager as backup.

Mayor Hornaday called for a motion for the adoption of Resolution 12-036 by reading of title only.

WYTHE/LEWIS – SO MOVED

There was no discussion.

VOTE: YES. NON OBJECTION. UNANIMOUS CONSENT.

Motion carried.

COMMENTS OF THE AUDIENCE

Dave Brann, city resident, invited the public to the Water Trails public meeting at Islands and Ocean Visitors Center Wednesday, April 25 from 5:30 to 7:00 p.m. Thursday they will have a meeting in Seldovia.

Don McNamara, city resident, commented it seems like City Council wants to keep the Special Service District (SSD) intact after the CORPS are done. Council never wanted to be involved in maintenance of the seawall; they just wanted to build the wall. The City needs to wash its hands and quit spending the taxpayers' money.

COMMENTS OF THE CITY ATTORNEY

City Attorney Klinkner was not present.

COMMENTS OF THE CITY CLERK

City Clerk Johnson had no comment.

COMMENTS OF THE CITY MANAGER

City Manager Wrede responded to Don McNamara's comments. He doesn't think the City Council wants to keep the SSD going regardless of what the Corps does. If the Corps rules the way the Council is hoping it will, there may not be any need for a SSD. He doesn't have the authority to let McNamara's out of the district; Council will need to make that decision. As to the sign code, the Planning Commission worked a lot on the ordinance and accomplished a lot of positive things. Most of it was overlooked due to the discussion on the sandwich boards. The commission liberalized the amount of signage on the boardwalks. The signage was based on a per lot basis; it is now based on a per business basis.

COMMENTS OF THE MAYOR

Mayor Hornaday appreciates the good work of the Planning Commission. He welcomed sweet springtime with the eagles building a nest in a different tree across from the post office. The swans and the cranes are back and he even saw eagles making love on Beluga Slough.

COMMENTS OF THE CITY COUNCIL

Councilmember Wythe thanked Francie for bringing the sign ordinance back for reconsideration. She recognizes her concerns of feeling there was inadequate input after a substantial change. Even though the reconsideration of the ordinance was not approved we received more input tonight with different things. She thanked Sue Post for her comments of not supporting the sandwich board signs. She thanked the guests for talking to us about the early childhood development, and the reminder of how many infants need childcare. She thanked the Clerks for providing support to the Council, commissions, committees, and the community.

Councilmember Lewis echoed Beth's comments.

Councilmember Zak thanked Council for allowing him to participate by phone. There was a lot of good testimony.

Councilmembers Roberts and Howard had no comment.

Councilmember Burgess thanked the Clerks for their efforts in informing the public. He reiterated the socio and economic impact of investing in our young children. It is a stitch in time saves nine sort of thing. He supports the halibut bycatch limits being lowered, a big issue for local people. He thanked the local businesses and Chamber for showing up to speak about the sign ordinance. We can be a pro-business city council and be pro Homer at the same time. He would like to open the dialogue about the gas line and distribution. The City Council needs to take the lead in facilitating the discussion.

ADJOURNMENT

There being no further business to come before the Council, Mayor Hornaday adjourned the meeting at 8:24 p.m. The next Regular Meeting is Monday, May 14, 2012 at 6:00 p.m. and Committee of the Whole 5:00 p.m. A Worksession is scheduled for Monday, April 30, 2012 from 5:00 p.m. to 7:00 p.m. All meetings scheduled to be held in the City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

JO JOHNSON, CMC, CITY CLERK

Approved: _____



Office of the Mayor
James C. Hornaday
Homer City Hall
491 E. Pioneer Avenue
Homer, Alaska 99603-7624

Phone 907-235-8121 x2229
Fax 907-235-3143

MEMORANDUM 12-074

TO: HOMER CITY COUNCIL

FROM: JAMES C. HORNADAY, MAYOR

DATE: MAY 7, 2012

SUBJECT: REAPPOINTMENT OF ROBERTA HIGHLAND TO THE TRANSPORTATION ADVISORY COMMITTEE AND ANGIE NEWBY TO THE PUBLIC ARTS COMMITTEE.

Roberta Highland is reappointed to the Transportation Advisory Committee. Her appointment will expire April 1, 2015.

Angie Newby is reappointed to the Public Arts Committee. Her appointment will expire April 28, 2014.

RECOMMENDATION:

Confirm the reappointments of Roberta Highland to the Transportation Advisory Committee and reappointment of Angie Newby to the Public Arts Committee.

Fiscal Note: N/A

Office of the City Clerk

Jo Johnson, CMC, City Clerk
Melissa Jacobsen, CMC, Deputy City Clerk II
Renee Krause, CMC, Deputy City Clerk I



491 E. Pioneer Avenue
Homer, Alaska 99603
(907) 235-3130
(907) 235-8121
ext: 2224, 2226, or 2227
Fax: (907) 235-3143
Email: clerk@ci.homer.ak.us

MEMORANDUM12-078

TO: Mayor Hornaday and Homer City Council

FROM: Melissa Jacobsen, CMC, Deputy City Clerk (W)

DATE: May 8, 2012

SUBJECT: Planning Commission recommendation to appoint Commissioner Roberta Highland to serve on the Transportation Advisory Committee

The following action was taken at the May 2, 2012 meeting of the Homer Advisory Planning Commission:

ERICKSON/VENUTI SO MOVED TO RECOMMEND REAPPOINTMENT OF COMMISSIONER HIGHLAND TO THE TRANSPORTATION ADVISORY COMMISSION.

There was no discussion.

VOTE: NON OBJECTION: UNANIMOUS CONSENT

Motion carried.

Renee Krause

Subject: FW: URGENT PLEASE SEND ME AN EMAIL

MAY 07 2012 AM 11:47 *Re*

From: angienewby@alaska.com [<mailto:angienewby@alaska.com>]
Sent: Monday, May 07, 2012 10:52 AM
To: Renee Krause
Subject: Re: URGENT PLEASE SEND ME AN EMAIL

Dear Mayor and Council:

I am very interested in serving one more term on the Public Art Committee. I would like to see our Gateway projects through to completion.

Sent from my BlackBerry® smartphone powered by ACS!

From: Renee Krause <RKrause@ci.homer.ak.us>
Date: Mon, 07 May 2012 10:41:50 -0800
To: Angie Newby <angienewby@alaska.com>
Subject: URGENT PLEASE SEND ME AN EMAIL

We just realized your term on the PAC expired April 28th. Can you please send me an email requesting to be reappointed. This will be on the May 14th council agenda for approval.

The next PAC meeting is Wednesday May 16th at 4:00 p.m. worksession and regular meeting at 5:00 p.m.

Renee Krause, CMC
Deputy City Clerk I
City of Homer
491 E. Pioneer Avenue
Homer, Alaska 99603
Ph. 907-235-8121 ext. 2224
Fax. 907- 235-3143
rkrause@ci.homer.ak.us

"Successful is the person who has lived well, laughed often and loved much, who has gained the respect of children, who leaves the world better than they found it, who has never lacked appreciation for the earth's beauty, who never fails to look for the best in others or give the best of themselves."

The information contained in this message is proprietary and/or confidential. If you are not the intended recipient, please: (i) delete the message and all copies; (ii) do not disclose, distribute or use the message in any manner; and (iii) notify the sender immediately. In addition, please be aware that any message is subject to archiving and review by persons other than the intended recipient. Thank you.

ORDINANCE REFERENCE SHEET
2012 ORDINANCE
ORDINANCE 12-19

An Ordinance of the City Council of Homer, Alaska, Accepting and Appropriating a Commercial Vessel Passenger Tax Program Pass Through Grant from the Kenai Peninsula Borough for FY 2011 in the Amount of \$32,687.50, and Authorizing the City Manager to Execute the Appropriate Documents.

Sponsor: City Manager

1. City Council Regular Meeting May 14, 2012 Introduction
 - a. KPB Ordinance 2011-19-79
 - c. KPB Grant Agreement

1 CITY OF HOMER
2 HOMER, ALASKA

3 City Manager

4 ORDINANCE 12-19

5
6 AN ORDINANCE OF THE CITY COUNCIL OF HOMER,
7 ALASKA, ACCEPTING AND APPROPRIATING A
8 COMMERCIAL VESSEL PASSENGER TAX PROGRAM PASS
9 THROUGH GRANT FROM THE KENAI PENINSULA
10 BOROUGH FOR FY 2011 IN THE AMOUNT OF \$32,687.50,
11 AND AUTHORIZING THE CITY MANAGER TO EXECUTE
12 THE APPROPRIATE DOCUMENTS.

13
14 WHEREAS, The State of Alaska collects revenues under the Commercial Vessel
15 Passenger Tax Program and remits proceeds to boroughs which contain ports where cruise ships
16 land; and

17
18 WHEREAS, The Kenai Peninsula Borough has received such funds for FY 2011 and
19 decided to pass those revenues through to Homer and Seward; the communities where cruise
20 ships land and utilize local infrastructure and services; and

21
22 WHEREAS, The Borough Assembly adopted KPB Ordinance 2011-19-79 which
23 appropriated funds and authorized a pass through grant to the City of Homer in the amount of
24 \$32,687.50.

25
26 NOW, THEREFORE, THE CITY OF HOMER ORDAINS:

27
28 Section 1. The Homer City Council hereby accepts and appropriates a Commercial
29 Vessel Passenger Tax Program pass through grant from the Kenai Peninsula Borough for FY
30 2011 in the amount of \$32,687.50 as follows:

31
32 Appropriation:

<u>Account</u>	<u>Description</u>	<u>Amount</u>
460-927	KPB CVPT Pass Through Grant FY 11	\$32,687.50

33
34
35
36 Section 2. The City Manager is authorized to execute the appropriate documents.

37
38 Section 3. This ordinance is a budget ordinance only, is not permanent, and shall not be
39 codified.

41 ENACTED BY THE CITY COUNCIL OF HOMER, ALASKA, this _____ day of
42 _____, 2012.

43
44 CITY OF HOMER

45
46 _____
47 JAMES C. HORNADAY, MAYOR

48 ATTEST:

49
50 _____
51 JO JOHNSON, CMC, CITY CLERK

52
53 YES:

54 NO:

55 ABSTAIN:

56 ABSENT:

57
58 First Reading:

59 Public Hearing:

60 Second Reading:

61 Effective Date:

62
63 Reviewed and approved as to form:

64
65
66 _____
67 Walt E. Wrede, City Manager

68
69 _____
70 Thomas F. Klinkner, City Attorney

Date: _____

Date: _____

Introduced by: Mayor
Date: 03/13/12
Hearing: 04/17/12
Action: Enacted
Vote: 9 Yes, 0 No, 0 Absent

**KENAI PENINSULA BOROUGH
ORDINANCE 2011-19-79**

**AN ORDINANCE APPROPRIATING COMMERCIAL PASSENGER VESSEL TAX
PROCEEDS RECEIVED FROM THE STATE OF ALASKA IN THE AMOUNT OF
\$357,552.50 AND ALLOCATING \$324,865 TO THE CITY OF SEWARD AND
\$32,687.50 TO THE CITY OF HOMER**

WHEREAS, in the August 22, 2006 State of Alaska election, Alaska voters approved the initiative in Ballot Measure 2, enacting AS 43.52.200 – 43.52.295, which imposed a tax on travel aboard certain cruise ships travelling in Alaska waters; and

WHEREAS, the Alaska legislature has authorized the sharing of Commercial Passenger Vessel (CPV) excise tax collections with eligible ports of call in the State; and

WHEREAS, CPV excise tax collections are derived from taxes imposed on cruise ship passengers; and

WHEREAS, the City of Seward and City of Homer are eligible ports of call for receiving CPV funds; and

WHEREAS, when the eligible ports of call are cities located in a borough then the cities and the borough each receive \$2.50 for each passenger; and

WHEREAS, the total amount received by the borough from the State of Alaska for the 2011 calendar CPV programs was \$357,552.50; and

WHEREAS, the City of Seward and City of Homer have requested that funds received by the borough that are derived from cruise ship passengers in each city be appropriated to each city for port improvement projects; and

WHEREAS, the intended use of these funds by the cities of Seward and Homer will comply with AS 43.52.230(b), federal legislation and court rulings that permit the use of these funds for port facilities, harbor infrastructure, and other services provided to the commercial passenger vessels and the passengers on board those vessels;

NOW, THEREFORE, BE IT ORDAINED BY THE ASSEMBLY OF THE KENAI PENINSULA BOROUGH:

SECTION 1. That the mayor is authorized to execute grant agreements and any other documents deemed necessary to expend the funds and to fulfill the intents and purposes of this ordinance.

SECTION 2. That \$357,552.50 received from the State of Alaska in FY2012 for the commercial passenger vessel excise tax collection program be appropriated from the miscellaneous grant fund balance to account 271.94910.12CPV.43011 contract services, for payment to the City of Seward in the amount of \$324,865.00 and to the City of Homer in the amount of \$32,687.50 to be used for port improvement projects.

SECTION 3. This ordinance shall become effective immediately upon enactment.

ENACTED BY THE ASSEMBLY OF THE KENAI PENINSULA BOROUGH THIS 17TH DAY OF APRIL, 2012.

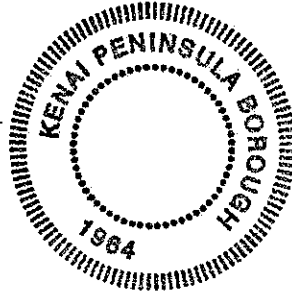


Gary Knopp, Assembly President

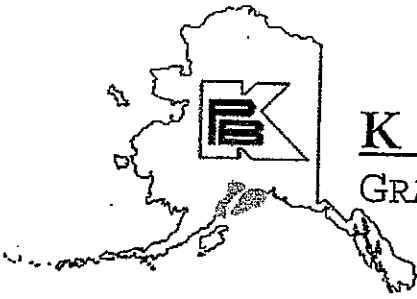
ATTEST:



Johni Blankenship, Borough Clerk



Yes: Haggerty, Johnson, McClure, Murphy, Pierce, Smalley, Smith, Tauriainen, Knopp
No: None
Absent: None



KENAI PENINSULA BOROUGH GRANT AGREEMENT

Total Grant Funds
\$32,687.50

Authorizing Ordinance
O2011-19-79

KPB Account Number
271.94910.12CPV.43011

Total Grant Award
\$32,687.50

Project Title
Commercial Passenger Vessel Tax Program

Funding Source
State

Grantee		Borough Contact Person	
Name	DUNS# 040171563	Name	
City of Homer	EIN# 92-0030963	Brenda Ahlberg, Community & Fiscal Projects Manager	
Mailing Address		Mailing Address	
491 E. Pioneer Avenue		144 N. Binkley Street	
City/State/Zip		City/State/Zip	
Homer AK 99603		Soldotna, AK 99669	
Contact Person		Email	
Walt Wrede, City Manager		bahlberg@borough.kenai.ak.us	
Phone	Fax	Phone	Fax
235-8121	235-3148	714-2153	714-2377

The Kenai Peninsula Borough (*hereinafter* "Borough") and City of Homer (*hereinafter* "Grantee") agree as set forth herein.

Section I: The Borough shall pay Grantee for the performance of the project work under the terms outlined in this agreement. The amount of the payment is based upon project expenses incurred, which are authorized under this agreement. In no event shall the payment exceed \$32,687.50.

Section II: The Grantee shall perform all of the work required by this agreement.

Section III: The work to be performed under this agreement begins 04/17/2012 and shall be completed no later than 06/30/2014.

Section IV: the agreement consists of this page and the following attachments:

- A. Scope of Work and Use of Funds
 - Payment Method and Reporting Requirements
 - Standard Provisions
 - B. Financial/Progress Report
 - C. Signature Authorization Form
- Certificate(s) of Insurance – *provided by Grantee*

Section V: The Borough Clerk shall control the original, including any attachments filed.

Grantee	Borough
Signature: <u>Walt Wrede</u>	Signature: _____
Authorized Signatory Name and Title Walt Wrede, City Manager	Authorized Signatory Name and Title Mike Navarre, Mayor
Date: <u>5/3/12</u>	Date: _____
	Attest: Johni Blankenship, Borough Clerk

Approved as to form and legal sufficiency:

Holly Montague, Deputy Borough Attorney

FY12 KPB Grant Agreement

City of Homer - Commercial Passenger Vessel Tax Program CY2011 Funds

Scope of Work and Use of Funds

Project Description: The purpose of this Borough Grant is to provide funding through the State of Alaska Commercial Passenger Vessel Tax Program, *hereinafter* referred to as CPVT, to the City of Homer, *hereinafter* referred to as "grantee." The Grantee will use the funds for port and harbor projects impacted by cruise ship landings as governed by the Alaska Statutes (see "Source of Funds, page 2).

Project Management

1. The project will be managed by the grantee.
2. The City Manager, or such other person(s) as indicated on Attachment C, shall be designated as the representative to receive or make all communications regarding the performance or administration of this agreement. The Borough Community & Fiscal Projects Manager, or such other person(s) as designated by the Borough Mayor, is hereby designated as the representative of the Borough to receive or make all communications, payment requests, and reports regarding the performance or administration of this agreement, and who approves payment under this agreement. The parties may change their representative upon written notification to the other party.

Key Contact Information

Brenda Ahlberg, Community & Fiscal Projects
714-2153 • bahlberg@borough.kenai.ak.us Kenai Peninsula Borough
144 N. Binkley St.
Soldotna AK 99669

Project Approval

The Assembly approved and appropriated CPVT funds from calendar year 2011 in the amount of \$32,687.50 to the grantee through KPB Ordinance 2011-19-79. Grantee shall cite KPB account number **271.94910.12CPV.43011** in correspondence and reporting forms.

Grant funds, or any earnings there from, may be spent only for the purposes of the grant project as described above. Any monies used for purposes not authorized by this agreement shall be refunded to the Borough immediately after such expenditures, with or without demand by the Borough. Request to amend the budget shall be submitted to the Community & Fiscal Projects Manager no less than ninety (90) days prior to performance period end date.

Project Performance Period

Period: The performance period shall be effective for the period commencing on April 17, 2012 and ending on June 30, 2014. The grantee must liquidate all obligations not later than June 30, 2014. The Borough shall have no obligation for payment of services rendered by the grantee which are not performed within this specified period.

Request to Extend: Any determination to extend the project performance period is solely at the discretion of the Borough. A request to amend the performance period should be submitted not later than ninety (90) days prior to the established end date.

Funding

1. Source of Funds:
 - a. This project is being funded in full through the State of Alaska Department of Commerce, Community & Economic Development, "Commercial Passenger Vessel Tax Program" (CPVT) as governed by AS 43.52.200 - 43.52.295.
 - b. All unexpended grant funds as of the end of the grant period must be returned to the Borough. Funds will be considered obligated if they have actually been expended or encumbered prior to the end date of the performance period.
2. Condition for Receipt of Funds: The grantee shall comply with AS 43.52.230 (b), federal legislation and court rulings that permit the use of these funds for port facilities, harbor infrastructure, and other services provided to the commercial passenger vessels and the passengers on board those vessels.
3. Funds, or earnings there from, shall not be expended for the purposes of lobbying activities before the Borough Assembly, the Alaska State Legislature or U.S. Congress.

Payment Terms and Reporting Requirements

1. Payments:
 - a. The Grantee will be compensated for up to and not exceeded \$32,687.50 as determined by the State of Alaska for projects identified in the City of Homer's request letter dated February 28, 2012 and approved by Kenai Peninsula Borough Ordinance 2011-19-79.
 - b. Under no circumstances will funds be released to the Grantee unless all required reporting is current.
2. Reporting Requirements:
 - a. The Borough's approval of the proposed expenditures shall not be construed to mean that the Borough is liable in any manner whatsoever if it is determined by an agency or court of competent jurisdiction that the city's project does not satisfy the statutory criteria.
 - b. Upon completion of the projects, but no later than thirty days thereafter the agreement end date, the grantee shall provide a detailed written report to the Borough administration and assembly, explaining the use of these funds and their compliance with statutory requirements.

- c. The Grantee agrees to refund any such amounts, including principal, interest, costs, fees, fines, or other charges, if it is determined by a court of competent jurisdiction that the grantee's expenditure of the funds does not meet the statutory criteria.
- d. This agreement is not to be construed as entitlement to city receiving the future allocation of the Borough's CPVT.
- e. Failure to meet the reporting requirements set forth in this agreement may result in the Borough withholding future allocations of the borough's grant of CPVT funds.

Mail OR Email Narrative and Fiscal Reports to:

Brenda Ahlberg, Community & Fiscal Projects Manager
Kenai Peninsula Borough
144 N. Binkley St., Soldotna AK 99669
(907) 714-2153 • bahlberg@borough.kenai.ak.us

Standards for Financial Management

Financial Management System:

1. The grantee will maintain an accounting system and a set of accounting records that at a minimum, allows for the identification of individual projects by source of revenue and expenditures related to this project.
2. All costs will be supported by source documentation. Grantee shall retain all contracts, invoices, materials, payrolls, personnel records, conditions of employment, and other data relating to matters covered by this agreement for a period of three years after the completion date of the agreement, or until final resolution of any audit findings, claims, or litigation related to the grant.
3. The grantee's accounting records will be the basis for generating financial reports which must reflect accurate and complete data. In addition, financial records must be properly closed out at the end of the project period and all reports submitted in a timely manner.

Procurement Standards

The Grantee agrees to use a competitive process when making procurements for goods and services. These standards include but are not limited to the following:

1. Grantee may use its own procurement policies provided that they adhere to the applicable standards;
2. Grantee shall maintain a code of conduct which shall govern the performance of its officers, employees or agents in contracting with or expending grant funds; and
3. All procurement transactions shall be conducted in a manner so as to provide for maximum open and free competition.

Audits and Monitoring

Audits:

1. Provisions of 2 AAC 45.010 shall apply to a Borough Grantee receiving a grant of \$100,000 or more from the Borough. An audit in compliance with 2 AAC 45.010 shall be

required on the performance of the grant conditions. Such audit report shall be due to the Borough no later than 30 days after the audit is completed, or 6 months after the expiration of the grant, whichever is sooner. A copy of a federal single audit report may be submitted to fulfill the requirements of this section.

2. An annual audited financial statement, certified by a Certified Public Accountant, shall be submitted by a Borough Grantee receiving a grant less than \$100,000 from the Borough. Such financial statement shall be due no later than 6 months from the termination of this agreement.
3. Grantees receiving less than \$100,000 from the Borough and receiving payments on a reimbursable basis are exempt from the audit requirements.

Monitoring:

1. The grantee may receive an on-site review from the Borough, or its authorized representatives, in addition to reviews from State of Alaska personnel representing the AK Department of Commerce, Community & Economic Development. Monitoring staff may review project and financial activity relating to the terms of this agreement. Upon request, the Borough shall be given full and complete access to all information related to the performance period of this agreement to ensure compliance with the project activities and consistently applied costs.
2. The grantee shall provide the Borough and its authorized representatives all technical staff, assistance and information needed to enable the Borough or the State personnel to perform its monitoring function. This assistance from the grantee includes, but is not limited to, information about the grantee's project operation, accounting and data-base systems.

Program and Financial Deficiencies:

1. Through audits, reviews, monitoring or other means, the Borough may find the grantee to have program or financial deficiencies in the performance of the agreement. Such deficiencies may include, but are not limited to, the areas of accounting, financial controls, budgeting, and/or project compliance issues. If deficiencies are found, the Borough may require the grantee to take corrective action and to submit a written corrective action plan to address identified deficiencies. All corrective action plans must be accepted by the Borough or its authorized representatives. Any corrective action must be satisfactorily completed within thirty days from the date of written notification.
2. The Borough, in its sole discretion, may require the grantee to submit periodic written verification that measures have been taken to implement the corrective action. If the grantee fails to demonstrate its compliance with the approved corrective action plan within the time constraints set by the Borough, the Borough may, at its option, exercise its rights to terminate the agreement. The Borough may exercise any of the other rights and remedies available to it at law or in equity.

Standard Provisions

1. **No Maintenance or Further Funding Responsibility:** By signing this agreement, grantee certifies that it will not ask the Borough to operate or maintain its program, except as may be otherwise agreed to in writing signed by both parties. Grantee understands, acknowledges and agrees that the Borough shall not be responsible for any services, programs, maintenance, operations, or further funding to grantee, or actions related thereto, and has not,

and will not assume any such responsibility, all of such to be the sole and exclusive responsibility of grantee.

2. **Defense and Indemnification:** The Grantee shall indemnify, defend, save and hold the Borough, its elected and appointed officers, agents and employees, harmless from any and all claims, demands, suits, or liability of any nature, kind or character including costs, expenses, and attorneys fees resulting from grantee or grantee's officers, agents, employees, partners, attorneys and suppliers' performance or failure to perform this agreement in any way whatsoever. This defense and indemnification responsibility includes claims alleging acts or omissions by the Borough or its agents, which are said to have contributed to the losses, failure, violations, or damage. However, grantee shall not be responsible for any damages or claim arising from the sole negligence or willful misconduct of the Borough, its agents, or employees. Grantee shall also not be required to defend or indemnify the Borough for damage or loss that has been found to be attributed to an independent party directly responsible to the Borough under separate written contract. If any portion of this clause is voided by law or court of competent jurisdiction, the remainder of the clause shall remain in full force and effect.
3. **Insurance:** Grantee shall purchase at its own expense and maintain in force at all times during the term of this agreement Commercial General Liability and Automobile Insurance. Such policies are to include bodily injury, personal injury, and property damage with respect to the property and the activities conducted by grantee in which coverage shall not be less than \$1,000,000 per occurrence or such higher coverage as specified by the Borough. The policy shall name the Borough as an additional insured. Borough approval shall be required for the amount of any deductible or self-insured retention.

Additionally, grantee shall purchase and maintain at its own expense worker's compensation and employers liability insurance for all employees per Alaska State Statutes who are performing work under this agreement.

Proof of Insurance: *Grantee shall deliver to the Borough certificates of insurance along with grantee's signature on this agreement.* The certificates shall indicate the Borough as an additional insured. This insurance shall be primary and exclusive of any other insurance by the Borough. Failure to provide the certificate of insurance required by this section, or a lapse in coverage, is a material breach of the terms of this agreement entitling the Borough to terminate this agreement.

4. **Relationship of Parties:** Nothing herein contained shall be deemed or construed by the parties hereto, nor by any third party, as creating the relationship of principal and agent, or of partnership, or of joint venture between the parties hereto, it being understood and agreed that neither method of computation of payment nor any other provision contained herein, nor any acts of the parties hereto, shall be deemed to create any relationship between the parties hereto other than the relationship of Borough and independent grantee of funds.
5. **No Exclusive Service; No Property Interest:** The grantee understands, acknowledges and agrees that all supplies, materials and equipment purchased with the grant funds shall be and shall remain the property of the grantee, subject to all applicable State statutes and Federal regulations.

6. **Termination.** The Borough may terminate this agreement, by written notice, when it is in the best interest of the Borough. In the event that grantee does not perform the tasks as required in this agreement, or does not submit any required reports for verification of performance, the Borough may exercise its option to terminate this agreement.
7. **Permits, Taxes and Adherence to Local, State, and Federal Laws:** Signing of this agreement does not, in any manner, excuse grantee from complying with any other law, Alaska state statute or regulation, or Borough ordinance or regulation. Grantee must in all cases adhere to all local, state and federal laws and regulations that pertain to public funds, to the services performed pursuant to this agreement, and related to wages, taxes, social security, workers compensation, nondiscrimination, licenses, permits, and registration requirements. Grantee shall pay all taxes pertaining to its performance under this agreement, and shall acquire and maintain in good standing all permits, licenses and other entitlements necessary to the performance under this agreement.
8. **Jurisdiction; Choice of Law:** Any civil action arising from this agreement shall be brought in the superior court for the Third Judicial District of the State of Alaska at Kenai. The law of the State of Alaska shall govern the rights and obligations of the parties.
9. **Non-Waiver:** The failure of the Borough at any time to enforce a provision of this agreement shall in no way constitute a waiver of the provisions, nor in any way affect the validity of this agreement or any part thereof, or the right of the Borough thereafter to enforce each and every protection hereof. No conditions or provisions of this grant agreement can be waived unless approved by the Borough in writing. Waiver by the Borough of any non-compliance by grantee, or excusing or extending performance, shall not be considered a waiver of any other rights of the Borough or a waiver of the right to terminate in the event of future breaches.
10. **No Third-Party Beneficiary:** This agreement is intended solely for the benefit of each party hereto. Nothing contained herein shall be construed or deemed to confer any benefit or right upon any third party.
11. **Environmental Requirements:** The grantee must comply with all environmental standards, to include those prescribed under State of Alaska and Federal statutes and Executive Orders.
12. **Entire Agreement:** This agreement represents the entire and integrated agreement between the Borough and grantee, and supersedes all prior, inconsistent negotiations, representations or agreements, whether written or oral. This agreement may be amended only by written instrument signed by both the Borough and grantee.



KENAI PENINSULA BOROUGH

144 North Binkley Street • Soldotna, Alaska 99669-7599
PHONE: (907) 714-2153 • **FAX:** (907) 714-2377
EMAIL: bahlberg@borough.kenai.ak.us

FROM: City of Homer
KPB ACCOUNT: 271.94910.12CPV.43011

Contract Amount: \$32,687.50
Ending: 06/30/2014

Financial / Progress Report

Submit Report To:

Brenda Ahlberg
 Community & Fiscal Projects Manager
 Kenai Peninsula Borough
 144 N. Binkley St., Soldotna, AK 99669

Project Name: Commercial Passenger Vessel Tax Program

Date:
Report No.:
Quarter From:
To:

FINANCIAL REPORT:

Cost Category	Authorized Budget	Expenditures from Last Report	Expenditures This Period	Total Expenditures to Date	Balance of Funds
	\$ -				
Personnel		-	-		\$ -
Travel		-	-	-	\$ -
Contractual	32,687.50	-	-	-	\$ 32,687.50
Supplies		-	-	-	\$ -
Other		-	-	-	\$ -
		-	-	-	\$ -
TOTALS	\$ 32,687.50	-	\$ -	\$ -	\$ 32,687.50

Advance funding	\$ 32,687.50
Expenditures applied to advance	-
Remaining Advance Balance	\$ 32,687.50

PROGRESS REPORT: Describe activities that have occurred during this reporting period. Describe any challenges you may have experienced, any foreseen problems, and/or any special requests. Attach additional pages.

Grantee Certification: I certify that the above information is true and correct, and that expenditures have been made for the purpose of, and in accordance with, applicable grant agreement terms and conditions.

Signature: _____ Date: _____

Printed Name and Title: _____

Signature Authorization Form

Grant Program: Commercial Passenger Vessel Tax Program - State Pass Thru Funding

Agreement Number: 271.94910.12CPV.43011

Effective Date: 04/17/2012

Name of Grantee

City of Homer

DUNS# and Tax ID#

DUNS # 040171563 TAX ID# 92-0030963

Point of Contact Information

Program Manager, Chief Financial Officer, and Signatory Official must be three (3) different individuals.

Primary and Alternate Signatories: Grant Award/Amendments and Grant Reports

Program Manager Name
Individual managing the project

Brian Hawkins

PM Address

491 E. Pioneer Ave., Homer, AK 99603

PM Telephone

907-235-3160

PM Fax

907-235-3152

PM Email

bhawkins@ci.homer.ak.us

Chief Financial Officer Name
Authorized to certify financial expenditures and records

Regina Mauras

Jo Earls

CFO Address

491 E. Pioneer Ave. Homer, AK 99603

CFO Telephone

907-235-8121

907-435-3110

CFO Fax

907-235-3140

CFO Email

rmauras@ci.homer.ak.us

jearls@ci.homer.ak.us

Signatory Official Name
City Manager

Walt Wrede

Mark Robl

Signatory Official Address

491 E. Pioneer Ave., Homer AK 99603

4060 Heath St., Homer, AK 99603

Signatory Official Telephone

907-235-8121

907-235-3150

Signatory Official Fax

907-235-3140

907-235-3151

Signatory Official Email

wwrede@ci.homer.ak.us

mrobl@ci.homer.ak.us

Signatures required by each of the above-named individuals.

Project Manager

Signature and Date

Chief Financial Officer

Signature and Date

Signatory Official

Signature and Date

Walt Wrede 5/3/12

VISITORS

**ANNOUNCEMENTS
PRESENTATIONS
BOROUGH REPORT
COMMISSION REPORTS**

**CITY OF HOMER
HOMER, ALASKA**

MAYOR'S PROCLAMATION

**PEACE OFFICERS MEMORIAL DAY – MAY 15, 2012
NATIONAL POLICE WEEK – MAY 13 - 19, 2012**

WHEREAS, The members of the Homer Police Department and the Alaska State Troopers stationed in the Homer area play an essential role in safeguarding the rights and freedoms of the Homer area residents, our citizens; and

WHEREAS, It is important that all citizens know and understand the duties, responsibilities, hazards, and sacrifices of their Police Officers and State Troopers; and

WHEREAS, It is also important that members of our Homer Police Department and Alaska State Troopers recognize their duty to serve the people by safeguarding life and property, by protecting against violence and disorder, by protecting the innocent against deception, and protecting the weak against oppression; and

WHEREAS, The men and women of the Homer Police Department and State Troopers unceasingly provide a vital public service and lay their lives on the line for the safety of the Homer area citizens; and

WHEREAS, The Congress and President of the United States have designated May 15 of each year as Peace Officers Memorial Day and the week in which May 15 falls is National Police Week.

NOW, THEREFORE, I, JAMES C. HORNADAY, Mayor of the City of Homer, Alaska, on behalf of the Homer City Council and the Citizens and patriotic, civic and education organizations of our community, do hereby proclaim May 15, 2012 as:

PEACE OFFICERS MEMORIAL DAY


and call upon all citizens of Homer to observe the week of May 13 – 19, 2012 as National Police Officer Week in honor of those law enforcement officers, who through their courageous deeds have made the ultimate sacrifice in service to their community or have become disabled in the performance of duty, and let us recognize and pay respect to the survivors of our fallen heroes.

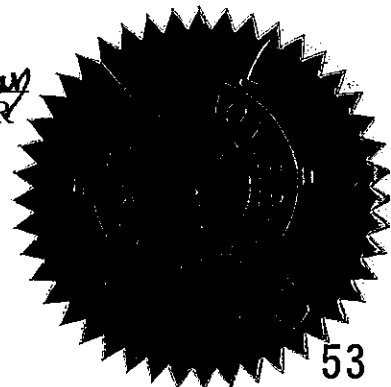
IN WITNESS WHEREOF, I have hereunto set my hand and cause to be affixed the official seal of the City of Homer, Alaska, on this 14th day of May, 2012.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

ATTEST:


J. JOHNSON, CMC, CITY CLERK



**CITY OF HOMER
HOMER, ALASKA**

MAYOR'S PROCLAMATION

**NATIONAL PUBLIC WORKS WEEK
May 20 - 26, 2012**

WHEREAS, Public works infrastructure, facilities, and services are of vital importance to the health, safety, and well-being of the people of the United States; and

WHEREAS, Those facilities and services could not be provided without the dedicated efforts of public works staff, who represent State and local governments throughout the United States; and

WHEREAS, Those individuals design, build, operate, and maintain the transportation systems, water infrastructure, sewage and refuse disposal systems, public buildings, and other structures and facilities that are vital to the citizens and communities of the United States; and

WHEREAS, It is in the interest of the public for citizens and civic leaders to understand the role that public infrastructure plays in -

- (1) protecting the environment;
- (2) improving public health and safety;
- (3) contributing to economic vitality; and
- (4) enhancing the quality of life of every community of the United States.

WHEREAS, Every day public works professionals make important contributions to improve -

- (1) The public infrastructure of the United States; and
- (2) the communities that those professionals serve.

NOW, THEREFORE, I, James C. Hornaday, Mayor of the City of Homer, do hereby proclaim the week of May 20 - 26, 2012 as:

NATIONAL PUBLIC WORKS WEEK


and encourage all residents to recognize the substantial contributions that public works professionals make to the City and the Nation.

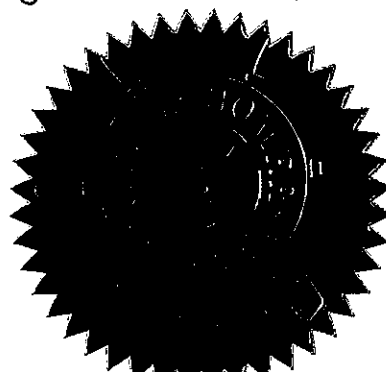
IN WITNESS WHEREOF, I have hereunto set my hand and caused the Seal of the City of Homer, Alaska to be affixed this 14th day of May, 2012.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

ATTEST:


JO JOHNSON, CMC, CITY CLERK



CITY OF HOMER
HOMER, ALASKA

MAYOR'S PROCLAMATION

National Women's Health Week
May 13 – 19, 2012

WHEREAS, National Women's Health Week was initiated in the dawn of the new millennium by a group of public and private organizations dedicated to raising awareness of women's health issues; and

WHEREAS, From that day forward National Women's Health Week has occurred in the spring with an annual celebration held the week following Mother's Day; and

WHEREAS, National Women's Health Week is a celebration of women taking responsibility for their own health through greater knowledge and understanding; and

WHEREAS, National Women's Health Week celebrates the efforts of national and community organizations working with partners and volunteers to improve awareness of key women's health issues; and

WHEREAS, Kachemak Bay Family Planning Clinic has created events in Homer, Alaska, to officially launch our local efforts.

NOW, THEREFORE, I, James C. Hornaday, Mayor of the City of Homer, do hereby proclaim the week of May 13 - 19, 2012 as:

NATIONAL WOMEN'S HEALTH WEEK

and encourage all citizens to participate in the activities planned hereforth.

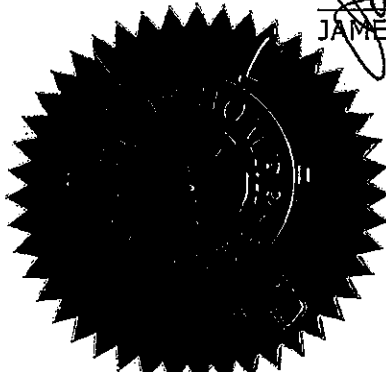
IN WITNESS WHEREOF, I have hereunto set my hand and caused the Seal of the City of Homer, Alaska to be affixed this 14th day of May, 2012.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

ATTEST:


JO JOHNSON, CMC, CITY CLERK



**CITY OF HOMER
HOMER, ALASKA**

MAYOR'S PROCLAMATION

**BIKE TO WORK WEEK
May 14 - 18, 2012**

WHEREAS, For more than a century the bicycle has been an utilitarian, economical, environmentally sound and effective means of personal transportation, recreation and fitness; and

WHEREAS, The United States Census Bureau's American Community Survey shows that Alaska ranks sixth in the nation in the proportion of adults who bike to work; and

WHEREAS, Many Homer residents enjoy riding bicycles, especially during the spring and summer months, and Homer is blessed with unique riding trails and landscapes suited for bicyclists of all levels; and

WHEREAS, The City of Homer, Alaska recognizes the bicycle as a legitimate roadway vehicle and therefore is entitled to legal and responsible use of all public roadway facilities in Alaska except highways constructed to interstate standards; and

WHEREAS, The bicycle is an affordable and viable form of transportation as well as an excellent form of health and fitness; and

WHEREAS, Both bicyclists and motorists should always be careful and conscientious of their surroundings. Bicycling does not require additional infrastructure or maintenance if we "share the road;" and

WHEREAS, The City of Homer, Alaska encourages the increased use of the bicycle, benefiting all citizens of Homer by improving air quality, reducing traffic congestion and noise, decreasing the use of and dependence upon finite energy sources, and fostering exercise; and

WHEREAS, During the month of May, Homer Cycling Club along with, schools, parks and recreation departments, police departments, hospitals, businesses, and civic groups throughout Alaska will be promoting bicycling as a wholesome transportation activity, as well as an environmentally friendly supplement to automobile travel; and

WHEREAS, Many Alaskans will experience the joys of bicycling through educational programs, commuting events, helmet promotions, and other bicycling events.

NOW, THEREFORE, I, James C. Hornaday, Mayor of the City of Homer, do hereby proclaim May 14 - 18, 2012 as:

BIKE TO WORK WEEK


and encourage all Alaskans to participate in Bike Month activities, to always be aware of cyclists on our roads, and to recognize and practice bicycle safety throughout the year.

IN WITNESS WHEREOF, I have hereunto set my hand and caused the Seal of the City of Homer, Alaska to be affixed this 14th day of May, 2012.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

ATTEST:


JOHNSON, CMC, CITY CLERK



HOSPICE OF HOMER



**COMPASSION
IN ACTION**

Post Office Box 4174
Homer, Alaska 99603-4174

(907) 235-6899
hospiceofhomer.org

April 17, 2012

Walt Wrede
City Manager
The City of Homer
491 East Pioneer Avenue
Homer, Alaska 99603

Dear Mr. Wrede:

On behalf of Hospice of Homer's board members, staff, volunteers and clients, it is my pleasure to thank the City of Homer and The Homer Foundation for the recent generous City grant Hospice was awarded. In these challenging economic times the support of the City by way of an operating expense grant is especially appreciated and needed. HOH continues to experience an increasing demand for the services we provide the community so monetary assistance is vital.

Hospice of Homer will use the grant monies to offset increasing general operating costs. Operating expense grant monies are difficult to secure and they are basic to Hospice finances as they assist us in keeping our unique and special approach to care available to Homer.

Again, I want to thank the City of Homer and The Homer Foundation. As a small non-profit, Hospice feels very fortunate to have a City that recognizes the value of such organizations and backs up that belief with financial support, and for having a strong local Foundation with the skills and desire to administer the City Grants Program.

Working together we can continue to ensure Homer to be a caring, quality place to live. Working together we can continue to carryout the Hospice of Homer's mission, and core purpose of compassion in action

Warmest thanks,


Darlene Hilderbrand, Executive Director

Cc: The Homer Foundation



Kenai Peninsula
United Way

PUBLIC HEARING(S)

**CITY OF HOMER
PUBLIC HEARING NOTICE
CITY COUNCIL MEETING**

**Port and Harbor Revenue Bond Project
Resolutions 12-037 & 12-038
Ordinances 12-17(S) & 12-18**

A **public hearing** is scheduled for **Monday, May 14, 2012** during a Regular City Council Meeting. The meeting begins at 6:00 p.m. in the Homer City Hall Cowles Council Chambers located at 491 E. Pioneer Avenue, Homer, Alaska.

Port and Harbor Revenue Bond Project

Resolution 12-037 and 12-038 internet address:
<http://www.cityofhomer-ak.gov/resolutions>

Resolution 12-037, A Resolution of the City Council of Homer, Alaska, Amending the Fee Schedule for Portions of the Port and Harbor Fees for the Purpose of Repaying a Revenue Bond and Contributing to the Port and Harbor Enterprise Reserve Fund. City Clerk/Port and Harbor Advisory Commission.

Resolution 12-038, A Resolution of the City Council of Homer, Alaska, Amending Portions of the Port of Homer Terminal Tariff No. 600 for the Purpose of Repaying a Revenue Bond and Contributing to the Port and Harbor Enterprise Reserve Fund. City Clerk/Port and Harbor Advisory Commission.

Ordinance 12-17(S) and 12-18 internet address:
<http://www.cityofhomer-ak.gov/ordinances>

Ordinance 12-17(S), An Ordinance of the City Council of Homer, Alaska, Appropriating \$30,000 from the Port and Harbor Reserves to Make Emergency Repairs and/or Replace Damaged Float Piles in the Homer Harbor and Authorizing the City Manager to Execute a Change Order to the Existing Contract with Pacific Pile and Marine to Conduct the Harbor Float Piling Repair and Replacement Work in an Amount Not to Exceed \$30,000. City Manager.

Ordinance 12-18, An Ordinance of the City Council of Homer, Alaska, Amending the 2012 Operating Budget to Allow for the Completion of the 2012 Streets Repaving Project Utilizing Homer Accelerated Roads and Trails Program (HART) Funds in an Amount Not to Exceed \$900,000. City Manager/Public Works Director.



All interested persons are welcomed to attend and give testimony. Written testimony received by the Clerk's Office prior to the meeting will be provided to Council.

** Copies of proposed Ordinances, in entirety, are available for review at Homer City Clerk's Office. Copies of the proposed Ordinances are available for review at City Hall, the Homer Public Library, the City of Homer Kiosks at City Clerk's Office, Captain's Coffee, Harbormaster's Office, and Redden Marine Supply of Homer and the City's homepage - <http://clerk.ci.homer.ak.us>. Contact the Clerk's Office at City Hall if you have any questions. 235-3130, Email: clerk@ci.homer.ak.us or fax 235-3143.

Jo Johnson, CMC, City Clerk

Publish: Homer Tribune: May 9, 2012

A handwritten signature in black ink, appearing to read "Jo Johnson".

CLERK'S AFFIDAVIT OF POSTING

I, Renee Krause, CMC, Deputy City Clerk I for the City of Homer, Alaska, do hereby certify that a copy of the Public Hearing Notice for Port and Harbor Revenue Bond Project; Ordinance 12-17(S), Appropriating \$30,000 from the Port and Harbor Reserves to Make Emergency Repairs and/or Replace Damaged Float Piles in the Homer Harbor and Authorizing the City Manager to Execute a Change Order to the Existing Contract with Pacific Pile and Marine to Conduct the Harbor Float Piling Repair and Replacement Work in an Amount Not to Exceed \$30,000; and Ordinance 12-18, Amending the 2012 Operating Budget to Allow for the Completion of the 2012 Streets Repaving Project Utilizing Homer Accelerated Roads and trails Program (HART) Funds in an Amount Not to Exceed \$900,000; Resolution 12-037, Amending the Fee Schedule for Portions of the Port and Harbor Fees for the Purpose of Repaying a Revenue Bond and Contributing to the Port and harbor Enterprise Reserve Fund; Resolution 12-038, Amending Portions of the Port of Homer Terminal Tariff No. 600 for the Purpose of Repaying a Revenue Bond and Contributing to the Port and Harbor Enterprise Reserve Fund at the City of Homer kiosks located at City Clerk's Office, Captain's Coffee Roasting Co., Harbormaster's Office, and Redden Marine Supply of Homer, on May 11, 2012 and that the City Clerk posted same on City of Homer Homepage on May 8, 2012.

IN TESTIMONY WHEREOF, I have hereunto set my hand and seal of said City of Homer this 9th day of May, 2012.


Renee Krause, CMC, Deputy City Clerk I



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**CITY OF HOMER
HOMER, ALASKA**

City Clerk/
Port and Harbor Advisory Commission

RESOLUTION 12-037

A RESOLUTION OF THE CITY COUNCIL OF HOMER,
ALASKA, AMENDING THE CITY OF HOMER FEE SCHEDULE
FOR PORT AND HARBOR FEES FOR THE PURPOSE OF
REPAYING A REVENUE BOND AND CONTRIBUTING TO THE
PORT AND HARBOR ENTERPRISE RESERVES.

WHEREAS, The Port and Harbor Improvement Committee was established by Resolution 11-060 to develop a Port and Harbor Improvement Revenue Bonding Plan and provide committee review and oversight throughout the implementation and completion of any approved plan; and

WHEREAS, Revenue Bonds are backed by a specific revenue stream and users pay the cost of the project and financing; and

WHEREAS, Amending the Port of Homer Terminal Tariff No. 600 to increase certain fees to harbor users is necessary for the purpose of repaying the revenue bond; and

WHEREAS, The Port and Harbor Improvement Committee appointed a Tariff Review Subcommittee that met twice and provided recommended changes to the Committee who in turn presented the recommended changes to the Port and Harbor Advisory Commission; and

WHEREAS, The Port and Harbor Advisory Commission held a public hearing regarding the proposed amendments to the Port of Homer Terminal Tariff No. 600, in which 13 people testified and the Commission proposed its amendment to the tariff rates; and

WHEREAS, Recognizing the need for maintenance for the Ice Plant, the Port and Harbor Improvement Committee recommends increasing the ice rate and monies gained to go towards Fish Dock and Ice Plant maintenance; and

WHEREAS, The Port and Harbor Improvement Committee reviewed the Commission's recommendations and recommends the amendments to the City of Homer Fee Schedule as outlined in the attached Exhibit A.

WHEREAS, The estimated proceeds may exceed the estimated repayment amount and any excess funds collected through these increases will be contributions to the Port and Harbor Enterprise Fund Reserve Account.

NOW, THEREFORE, BE IT RESOLVED that the City Council hereby amends the City of Homer Fee Schedule as outlined in Exhibit A for the purpose of repaying a revenue bond and contributing to the Port and Harbor Enterprise Fund Reserve Account.

PASSED AND ADOPTED by the City Council of Homer, Alaska, this ____ day of _____, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

ATTEST:

JO JOHNSON, CMC, CITY CLERK

Fiscal Note: Revenue amounts increase \$252,726 in 2012 and \$496,590 in 2013.

EXHIBIT A
Resolution 12-037

PORT AND HARBOR DEPARTMENT

(The following fees have been set by legislative enactments to HCC 10, Ord. 95-18(A) and Resolutions 12-023, 10-89, 06-52, 06-04, 05-123, 04-96, 03-154(S), 03-104, 03-88, 00-39, 99-118(A), 99-101, 99-78(S), 99-30(A), 95-69 (Port/Harbor Tariff No. 600), Resolution 95-19, Resolution 01-84(S)(A), Resolution 02-81(A), Resolution 07-121, and Resolution 08-123)
Harbor Office - 235-3160
Fish Dock - 235-3162

All rates except load and launch ramp fees and parking fees for Ramps 1 - 4, which are inclusive of sales tax, will have sales tax applied. The resulting figure will be rounded to the nearest half dollar for billing purposes.

Administrative cost for changing boat in slip - \$25.00
A \$30.00 per year charge will be assessed for a listing on a permanent reserved stall assignment.
Large quantity waste oil disposal (with Harbor Master approval) - \$3.25 gallon

PARKING FEES

Parking fees to be collected at Ramp 1, Ramp 2, Ramp 3 and Ramp 4 seasonally (Memorial Day through Labor Day). Parking fee is \$5 per calendar day. Posted parking time limits will be established and enforced as per Homer city code 10.04.100.

Seasonal permits for day use parking (Ramps 1-4): \$250.00.
Long Term parking permits required for Vehicles 20' or less parked in excess of seven (7) consecutive 24-hour days.

Long Term Parking annual permit (January 1st through December 31st): fee \$200.00.

Long Term Parking annual permit fee for vessel owners paying annual moorage in the Homer Harbor: fee \$100.00.

Vehicles over 20' and trailers are not eligible for long term parking permits.

Monthly parking permit for vehicles less than 20': fee \$70.00 for 30 consecutive days.

Monthly parking permit for vehicles over 20': fee \$85.00 for 30 consecutive days in a portion of Lot 9 only.

Long term parking will be enforced year around.

Parking lot restrictions for long term parking, May 1 through October 1, as depicted on attached map (Attachment A).

Existing code definitions for restricted parking, vehicles, junk vehicles, and fines for violations apply.

Fines, \$25.00 per calendar day, limited to \$250.00 fine per calendar year, with \$200.00 of the fine credited towards the long term parking annual permit.

Annual Moorage fee - ~~\$35.22~~ **\$37.86 (2012) \$40.50 (2013)** per lineal foot, plus \$50.00 administrative charge.

Reserved Stall - length of the float stall assigned, or overall length of vessel whichever is greater, plus \$50.00 administrative charge.

Float Plane Fee - daily moorage rate of (2) 24' vessels shall be assessed on a daily basis for float planes or a monthly rate equal to the monthly rate for (2) 24' vessels.

Dockage charges will be assessed **as follows:** ~~per calendar day or portion thereof at the rate of \$2.27 per foot for the length of the vessel.~~

	2013	2013
0' to 100'	\$254.13	\$338.00
101' to 200'	\$423.25	\$506.00
201' to 300'	\$677.75	\$788.00
301' to 350'	\$871.38	\$1,005.00
351' to 375'	\$961.01	\$1,098.00
376' to 400'	\$1,043.38	\$1,206.00
401' to 425'	\$1,137.26	\$1,337.00
426' to 450'	\$1,243.13	\$1,490.00
451' to 475'	\$1,327.51	\$1,604.00
476' to 500'	\$1,434.88	\$1,762.00
501' to 525'	\$1,580.26	\$1,996.00
526' to 550'	\$1,687.63	\$2,154.00
551' to 575'	\$1,806.01	\$2,334.00
576' to 600'	\$1,958.38	\$2,582.00
601' to 625'	\$2,174.26	\$2,957.00
626' to 650'	\$2,445.63	\$3,443.00
651' to 675'	\$2,711.10	\$3,917.00
676' to 700'	\$2,990.88	\$4,420.00
701' to 725'	\$3,368.76	\$5,119.00
726' to 750'	\$3,766.63	\$5,858.00
751' to 775'	\$4,188.01	\$6,644.00
776' to 800'	\$4,668.88	\$7,459.00

A service charge of \$52 will be assessed to each vessel.

These charges are applicable to the "outer face" and "trestle berth" of Deep Water Dock and to all berthing locations on Pioneer Dock. ~~The "inside berth" (berth No. 2) of Deep Water Dock will have a 4 hour minimum dockage charge of 1/6 the daily rate, and a half day (up to 12 hours) docking charge of 1/2 the daily rate, with no service charge applicable.~~

~~Cruise Ship dockage will be assessed per calendar day or portion thereof at the rate of \$2.27 per foot for the length of the vessel. A service charge of \$481.53 will be assessed to each Cruise Ship.~~

The fees for general storage are as follows:

Open Areas, fishing gear	.11 per sq. ft.
Open Areas, non-fishing gear	.15 per sq. ft.
Fenced Storage Yard	.22 per sq. ft.

The fees for trailer parking area/long term storage Oct. 1 to May 1 are as follows:

Up to 30 feet	\$ 75.00 per month
Over 30 feet up to 40 feet	\$100.00 per month

Storage:

Short term storage seven days or less, on a space available basis.

Long term storage eight days or more, not available during May 1 through September 30.

Wharfage:

Minimum wharfage on any shipment will be ten dollars (\$10). Except as otherwise specifically provided, rates are in dollars per short ton of 2,000 lbs. or per 40 cu. ft.

COMMODITY

WHARFAGE RATE

Freight N.O.S.

\$7.96

(Not Otherwise Specified)

Freight at barge ramp

\$5.14

Poles, logs, cants or cut finished lumber per M.M.

\$3.95/thousand board ft.

(Note: Industry standard conversion formulas shall be used in converting pounds to board feet measure.)

Logs that are unloaded at Port of Homer barge beaching site will be charged 50% of the wharfage rate applicable to outbound (export) shipment. However if these cargoes are not exported over Deep Water Dock with full payment of outbound wharfage within 60 days of unloading at the barge beaching site, then the additional 50% of wharfage will be owed and paid for inbound products.

Petroleum products

~~\$.39~~ **\$0.6363 (2012) \$0.84 (2013)** /barrel

(inbound and outbound)

~~\$.0103~~ **\$0.01515 (2012) \$0.02 (2013)**

Wood Chips (all grades)

\$ as per contract

Seafood/fish product

Setting a tariff of \$4.76 per ton of seafood/fish product across the dock, regardless of species.

Livestock: Horses, mules, cattle, hogs, sheep, goats, all other livestock

\$10.12 per head

Fowl: Any kind, crated

\$10.12 per crate

Boats: Up to and including twenty (20) feet LOA

\$15.66 each

Over twenty (20) feet LOA

\$1.60 per lineal ft.

(Fishing boats, pleasure craft, skiffs, dinghies and other boats moved over the docks.)

Demurrage

0.09/sq. ft.

Uplands Storage:

Land for Gear Storage-

First come-first served basis; approved by Harbormaster; primarily for fishing related gear.

Open areas, fishing gear 0.12/ sq. ft.

Open areas, non-fishing gear 0.17/ sq. ft.

Fenced storage yard 0.24/ sq. ft.

Boat Trailers-

Short term storage, up to 7 days - space available basis - no fee.

Long term storage, 8 days or more - not available May 1 to Oct 1
 Up to 30 feet \$ 75.00/month Oct 1 to May 1
 Over 30 feet \$100.00/month Oct 1 to May 1

TIDAL GRIDS:

The City of Homer operates two tidal grids. The wooden grid is for vessels of less than 60 feet in length. The steel grid is only for use by vessels of 60 feet or greater in length. Vessels that remain on either grid after their scheduled tide may be assessed a 50% surcharge for each unscheduled tide. Use of the steel grid shall be charged at the minimum rate applicable for a 60' boat if a boat of less length is allowed to use this grid.

The rate per foot per tide is \$1.05 for vessels 0' - 59'
 The rate per foot per tide is \$2.55 for vessels 60' - 80'
 The rate per foot per tide is \$3.25 for vessels 81' - 100'
 The rate per foot per tide is \$3.82 for vessels 101' - 120'
 The rate per foot per tide is \$4.24 for vessels 121' - 140'

WATER:

Potable water furnished to vessels at the Deep Water Dock and Main Dock:
 Quantity charge - \$38.81 per one thousand gallons (minimum five thousand gallons).
 Scheduled deliveries will have a minimum charge of one hundred and two (\$102.00) dollars for combined connection and disconnection.
 Unscheduled deliveries will have a minimum charge of one hundred thirty nine dollars and thirty two cents (\$139.32) for combined connection and disconnection.

ELECTRICITY (per kilowatt):

Reserved stalls having a meter base at the berth shall be charged a meter availability fee.
 The meter availability fee - \$23.95 per month
 Connect/disconnect fee - \$28.80

Metered transient vessels will be charged a meter availability fee of \$28.80 per month with a one month minimum charge to be applied for shorter connection periods. Connect/Disconnect fee 28.80.

Unless other arrangements have been made in writing with the Harbormaster, transient vessels shall be charged the following rates (where metered power is unavailable).

	<u>110 volt</u>	<u>220 volt</u>	<u>208 volt/3-phase</u>
Daily (or part thereof)	\$ 10.20	\$ 20.12	\$42.50
Monthly	\$152.67	\$341.70	available meter only

* Vessels requiring conversion plugs may purchase them from the Harbormaster's office for a nominal fee.

208 volt/3-phase electrical power is available at System 5 on a first come first served basis, for vessels will be charged the following rates:

1. There will be an electrical usage charge per kilowatt hour as determined by the local public utility;
2. Vessels will be charged a meter availability fee of \$28.80 per month with a one month minimum charge to be applied for shorter connection periods.

3. There will be a \$28.80 connect/disconnect fee.

TOWING: Inside small boat harbor: Skiff with operator – 1/2 hour \$68.00, Skiff with operator - 1 hour \$102.00. Any additional personnel required will be charged at rate of \$99.00 per hour each.

PUMPING VESSEL: \$40.79 per day or portion thereof for electrical pumps.
\$69.97 per hour or portion thereof for gas pumps.

LABOR/PERSONNEL:

All labor provided by City personnel shall be charged at \$102.00 per hour (1/2 hour minimum at \$51.00). Work requiring callouts shall be charged at a minimum of two hours.

SPECIAL SERVICES:

Special services, including waste, bulk oil, or garbage disposal shall be billed at the City's actual cost plus 125% of city costs for services arranged for by the City but provided by others. Waste oil in quantities greater than 5 gallons, shall be charged a \$3.35 per gallon handling and disposal fee.

REGULATED GARBAGE HANDLING FEE:

\$800.00 per round trip for certificated truck pickup plus \$90.00 per one hundred pounds. Regulated garbage, as per the Code of Federal Regulations, is garbage from foreign going vessels that contains, or that is suspected of containing food scraps or food waste.

SEARCH AND RESCUE FEES:

When the City utilizes city equipment and personnel to provide search and rescue assistance to vessels outside of the Homer Port and Harbor, such as towing and rescue, the Harbormaster will charge users of those services \$102.00 per hour for skiff and operator for the first hour and for additional search and rescue assistance beyond one hour. Additional personnel will be charged at the rate of \$99.00 per man per hour.

PUBLIC LAUNCH RAMPS:

Vessels shall be charged \$13.00 per day to launch from the public launch ramps from April 1 through October 15. (Reserved stall lessees exempt for the boat assigned to and registered to the reserved stall only, not for other boats owned by the same individual.)

Vessel owners or operators may obtain a seasonal permit for \$130.00 entitling a specific vessel and owner to launch from April 1 through October 15. (Reserved stall lessees exempt for the boat assigned to and registered to the reserved stall only, not for other boats owned by the same individual.)

BEACHES AND BARGE RAMP:

The use of beaches and barge ramp under the City ownership or control for commercial barge vessel repair, equipment loading or similar purposes, must be approved by the Harbormaster. A beach use agreement will be filled out and signed by the user and Harbormaster prior to use of the beach.

The Harbormaster shall charge a fee of \$1.50 per foot based on the overall length of the vessel, for vessels landing or parking on the beaches under City ownership or control. This same rate shall apply to vessels using the barge ramp.

Charges for extended beach or barge ramp use may be adjusted by the Harbormaster under appropriate circumstances.

The user of any beach area or the barge ramp must repair any damage to the beach or ramp and remove all debris. Failure to make such repairs and removal will result in repairs and cleanup by the harbor staff. The costs incurred by the harbor staff will be fully charged to the beach user. Labor rate for the harbor staff will be ninety dollars (\$99.00) per hour per person, plus appropriate equipment rental and material costs.

Sandblasting of vessel hull is not permitted on City beaches or barge ramp; water blasting using pressures that result in removal of paint is also prohibited. No paint chips or other paint materials are to be put into the water as a result of any maintenance done on the beach or ramp.

FISH DOCK:

The Fish Dock is to be used primarily for the loading and unloading of fish, fish products and fishing gear.

Cranes located onboard the vessel moored at Fish Dock may be utilized for loading/unloading the vessel only with prior approval granted by the Harbor Officer on duty.

Every person using a crane on the Fish Dock shall first obtain a license from the City.

Blocking access to cranes - \$150.00/hr.

Unattended vessels - \$150.00/hr.

Failure to obtain prior approval for a use other than loading and unloading fish, fish products or fishing gear will result in the imposition of a surcharge of thirty (\$30.00) dollars per hour in addition to the regular fee.

ITEM	FEE
Annual access	\$52.00 per year
Card (private license)	(annual renewal fee)
Card replacement fee	\$15.00 per occurrence
Cold Storage	\$334.75/month
(Cold storage rate structure is for storage area of eight (8) feet by ten (10) feet	\$309/per month for two (2) consecutive months
	\$283.25/per month for three (3) consecutive months
	\$275.50/per month for nine (9) month season
	Minimum one month rental
	Inspection \$50/per hour
Bait Storage Fee (4x4x4)	
Per Day	\$5.15
Per Week	\$25.75
Per Month	\$77.25
Ice Plant Bin Storage (Roofed over, open sided storage bins at west end of	\$200/per month, minimum two (2) months

of ice plant building sixteen (16) feet
by twelve (12) feet)

Fish Dock crane
Minimum charge per hour for crane
Ice

\$90.64/per hour
Fifteen minutes
~~\$119.00~~ **\$124.95 (2012) \$130.90**
(2013)/per ton

Seafood Wharfage/Fish product

Setting a tariff of \$4.76 per ton of
seafood/fish product across the docks.
Regardless of species bait in quantities
greater than one ton that is loaded onto a
vessel at Fish Dock, shall be charged
seafood wharfage.

Freight NOS, Nonfish Cargo

\$14.50/per ton for cargo going over the
Fish Dock.

Fish Waste Disposal Fees/Fish Grinder

\$5.00/Tub
\$30.00/Tote

1 CITY OF HOMER
2 HOMER, ALASKA

Wythe

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4 RESOLUTION 11-060

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6 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
7 ALASKA, ESTABLISHING A COMMITTEE TO DEVELOP A
8 PORT AND HARBOR IMPROVEMENT REVENUE BONDING
9 PLAN AND PROVIDE COMMITTEE REVIEW AND
10 OVERSIGHT THROUGHOUT THE IMPLEMENTATION AND
11 COMPLETION OF ANY APPROVED PLAN.
12

13 WHEREAS, The City of Homer Capital Improvement Plan (CIP) contains a variety of
14 repair and improvement items for the Port & Harbor district, many of which have been
15 languishing on the CIP list for a number of years with the continued hope of receiving funding
16 from State and Federal agencies; and
17

18 WHEREAS, The Homer City Council recognizes the Port and Harbor Enterprise as a
19 vital economic contributor to the community and as such desires to maintain and improve the
20 facilities to ensure continued economic vitality and support economic development within the
21 community; and
22

23 WHEREAS, State and Federal funding of these projects becomes increasingly unlikely
24 due to economic shortfalls at all levels of government; and
25

26 WHEREAS, The Port and Harbor is an Enterprise Fund which generates revenues that
27 can be allocated to the repayment of financed projects, which qualifies the Enterprise Fund for
28 Revenue Bonding consideration; and
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30 WHEREAS, The Port and Harbor Enterprise Fund currently has no indebtedness; and
31

32 WHEREAS, The current Port and Harbor Enterprise Fund depreciation reserve account
33 has a balance of \$1,653,816 as of December 31, 2010.
34

35 NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska,
36 supports the establishment of a committee to develop a plan for the implementation of a Port and
37 Harbor Improvement Revenue Bonding Project; and
38

39 BE IT FURTHER RESOLVED that the committee will consist of six members, three
40 representatives from the Homer City Council (to be appointed by the Mayor and approved by the
41 Council), the Harbormaster, and two members from the Port and Harbor Advisory Commission
42 (to be selected by the Port and Harbor Advisory Commission). Other staff members will provide
43 administrative and consultative support as requested by the committee or directed by the City
44 Manager.
45

46 BE IT FURTHER RESOLVED that the committee will make a recommendation to the
47 Council not later than November 1, 2011, including information on the process for revenue bond
48 funding, a proposed timeline for project completion in not longer than three years with a
49 beginning date of no later than June 1, 2012, and including the following items from the Capital
50 Improvement Projects list:

- 51
- | | | | |
|----|----|--|-------------|
| 52 | a. | Harbormaster's Office (to be developed as a model over-slope development project and include public restroom facilities with a separated entrance from the building to allow 24-hour access) | \$2,875,000 |
| 53 | | | |
| 54 | | | |
| 55 | | | |
| 56 | b. | Harbor Entrance Erosion Control | 600,000 |
| 57 | | | |
| 58 | c. | Ramp 3 Gangway replacement (to include disability access) | 1,700,000 |
| 59 | | | |
| 60 | d. | Harbor Float Replacement | 3,500,000 |
| 61 | | | |
| 62 | e. | Upgrade System 5 – Vessel Shore Power & Water | 530,000 |
| 63 | | | |
| 64 | f. | Fishdock Restrooms | 400,000 |
| 65 | | | |

66 PASSED AND ADOPTED by the Homer City Council this 13th day of June, 2011.

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JO JOHNSON, CMC, CITY CLERK

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

79 Fiscal Note: \$6,720, Acct. No. 165-375 (HART Trails Reserve)

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1 CITY OF HOMER
2 HOMER, ALASKA

3 City Clerk/
4 Port and Harbor Advisory Commission

5 RESOLUTION 12-038
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7 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
8 ALASKA, AMENDING THE PORT OF HOMER TERMINAL
9 TARIFF NO. 600 FOR THE PURPOSE OF REPAYING A
10 REVENUE BOND AND CONTRIBUTING TO THE PORT AND
11 HARBOR ENTERPRISE RESERVES.
12

13 WHEREAS, The Port and Harbor Improvement Committee was established by Resolution
14 11-060 to develop a port and harbor improvement revenue bonding plan and provide committee
15 review and oversight throughout the implementation and completion of any approved plan; and
16

17 WHEREAS, Revenue Bonds are backed by a specific revenue stream and users pay the cost
18 of the project and financing; and
19

20 WHEREAS, Amending the Port of Homer Terminal Tariff No. 600 to increase certain fees to
21 harbor users is necessary for the purpose of repaying the revenue bond; and
22

23 WHEREAS, The Port and Harbor Improvement Committee appointed a Tariff Review Sub-
24 Committee that met twice and provided recommended changes to the Committee who in turn
25 presented the recommended changes to the Port and Harbor Advisory Commission; and
26

27 WHEREAS, The Port and Harbor Advisory Commission held a public hearing regarding the
28 proposed amendments to the Port of Homer Terminal Tariff No. 600, in which 13 people testified
29 and the Commission proposed its amendment to the tariff rates; and
30

31 WHEREAS, Recognizing the need for maintenance for the Ice Plant, the Port and Harbor
32 Improvement Committee recommends increasing the ice rate and monies gained to go towards Fish
33 Dock and Ice Plant maintenance; and
34

35 WHEREAS, The Port and Harbor Improvement Committee reviewed the Commission's
36 recommendations and recommends the amendments to the Port of Homer Terminal Tariff No. 600 as
37 outlined in the attached Exhibit A.
38

39 WHEREAS, The estimated proceeds may exceed the estimated repayment amount and any
40 excess funds collected through these increases will be contributions to the Port and Harbor Enterprise
41 Fund Reserve Account.
42

43 NOW, THEREFORE, BE IT RESOLVED that the City Council hereby amends the Port of
44 Homer Terminal Tariff No. 600 as outlined in Exhibit A for the purpose of repaying a revenue bond
45 and contributing to the Port and Harbor Enterprise Fund Reserve Account.
46

47 PASSED AND ADOPTED by the City Council of Homer, Alaska, this ____ day of _____,
48 2012.
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CITY OF HOMER

JAMES C. HORNADAY, MAYOR

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ATTEST:

JO JOHNSON, CMC, CITY CLERK

Fiscal Note: Revenue amounts increase \$252,726 in 2012 and \$496,590 in 2013.

EXHIBIT A
Resolution 12-038

Rules or Category	2010	2011		2012	2013
	RATES	& CURRENT RATES		PROPOSED	PROPOSED
34.13 Wait List Fee	\$ 30.00	\$ 30.00		\$30.00	\$30.00
34.15 Waste Oil over 5 gallon per gallon	\$ 3.25	\$ 3.25		\$3.25	\$3.25
34.18b Annual Moorage Fee	\$ 34.19	\$ 35.22	15% over 2yrs	\$37.86	\$40.50
Administrative Charge	\$ 50.00	\$ 50.00		\$50.00	\$50.00
34.19					
Dockage	\$ 2.20	\$ 2.27	to Anchorage current rates over 2 yrs	using mid- range of LOA	
			0' to 100'	\$254.13	\$338.00
			101' to 200'	\$423.25	\$506.00
			201' to 300'	\$677.75	\$788.00
			301' to 350'	\$871.38	\$1,005.00
			351' to 375'	\$961.01	\$1,098.00
			376' to 400'	\$1,043.38	\$1,206.00
			401' to 425'	\$1,137.26	\$1,337.00
			426' to 450'	\$1,243.13	\$1,490.00
			451' to 475'	\$1,327.51	\$1,604.00
			476' to 500'	\$1,434.88	\$1,762.00
			501' to 525'	\$1,580.26	\$1,996.00
			526' to 550'	\$1,687.63	\$2,154.00
			551' to 575'	\$1,806.01	\$2,334.00
			576' to 600'	\$1,958.38	\$2,582.00
			601' to 625'	\$2,174.26	\$2,957.00
			626' to 650'	\$2,445.63	\$3,443.00
			651' to 675'	\$2,711.10	\$3,917.00
			676' to 700'	\$2,990.88	\$4,420.00
			701' to 725'	\$3,368.76	\$5,119.00
			726' to 750'	\$3,766.63	\$5,858.00
			751' to 775'	\$4,188.01	\$6,644.00
			776' to 800'	\$4,668.88	\$7,459.00
Cruise Ship Dockage	\$ 2.20	\$ 2.27		remove	remove
Cruise Ship Service Charge	\$ 467.50	\$ 481.53		\$481.53	\$481.53
Dockage Service Charge	\$ 50.00	\$ 52.00		\$52.00	\$52.00
34.2					
Freight N.O.S.	\$ 7.96	\$ 7.96		\$7.96	\$7.96
Freight at Barge Ramp	\$ 5.14	\$ 5.14		\$5.14	\$5.14
Poles, Logs, cants or cut	\$ 3.95	\$ 3.95		\$3.95	\$3.95
Petroleum Products					
Barrel	\$ 0.37	\$ 0.39	42gal= 1 barrel	\$0.6363	\$0.84

Gallon	\$ 0.01	\$ 0.0103	to \$.02 over 2yrs	\$0.01515	\$0.02
Seafood/Fish Products	\$ 4.62	\$ 4.76		\$4.76	\$4.76
Livestock	\$ 10.12	\$ 10.12		\$10.12	\$10.12
Fowl	\$ 10.12	\$ 10.12		\$10.12	\$10.12
Boats					
Up to 20 ft. Each	\$ 15.66	\$ 15.66		\$15.66	\$15.66
Over 20 ft. per lineal ft.	\$ 1.60	\$ 1.60		\$1.60	\$1.60
34.21					
Demurrage per sq. ft.	\$ 0.08	\$ 0.09		\$0.09	\$0.09
34.22 Uplands Storage					
Open area fishing gear	\$ 0.12	\$ 0.12		\$0.12	\$0.12
Open area non-fishing gear	\$ 0.17	\$ 0.17		\$0.17	\$0.17
Fenced storage yard	\$ 0.24	\$ 0.24		\$0.24	\$0.24
Shelter Deck Storage/per month	\$ 35.00	\$ 35.00		\$35.00	\$35.00
34.23 Tidal Grids					
0 - 59'	\$ 1.02	\$ 1.05		\$1.05	\$1.05
60 - 80'	\$ 2.48	\$ 2.55		\$2.55	\$2.55
81 - 100'	\$ 3.16	\$ 3.25		\$3.25	\$3.25
101 - 120'	\$ 3.70	\$ 3.82		\$3.82	\$3.82
121 - 140'	\$ 4.11	\$ 4.24		\$4.24	\$4.24
34.25					
DWD Potable Water					
Per Thousand Gallon	\$ 37.68	\$ 38.81		\$38.81	\$38.81
Connect/Disconnect Scheduled	\$ 99.00	\$ 102.00		\$102.00	\$102.00
Connect/Disconnect Not Scheduled	\$ 135.26	\$ 139.32		\$139.32	\$139.32
Electricity					
Reserved Stall					
Meter Availability Fee	\$ 23.25	\$ 23.95		\$23.95	\$23.95
Connect/Disconnect Fee	\$ 27.96	\$ 28.80		\$28.80	\$28.80
Transient Vessel					
Meter Availability Fee	\$ 27.96	\$ 28.80		\$28.80	\$28.80
Connect/Disconnect Fee	\$ 27.96	\$ 28.80		\$28.80	\$28.80
Non-Metered Power					
Daily					
110 volt	\$ 9.90	\$ 10.20		\$10.20	\$10.20
220 volt	\$ 19.53	\$ 20.12		\$20.12	\$20.12
208 volt three phase	\$ 43.88	\$ 45.20		\$45.20	\$45.20
Monthly					
110 volt	\$ 148.22	\$ 152.67		\$152.67	\$152.67

220 volt	\$ 331.74	\$ 341.70		\$341.70	\$341.70
Towing inside Harbor					
1/2 hour	\$ 66.00	\$ 68.00		\$68.00	\$68.00
1 hour	\$ 99.00	\$ 102.00		\$102.00	\$102.00
Pumping Vessel					
Elect Pump per day	\$ 39.60	\$ 40.79		\$40.79	\$40.79
Gas Pump per hour (includes attendant)	\$ 67.93	\$ 69.97		\$69.97	\$69.97
City Personnel Labor					
per hour	\$ 99.00	\$ 102.00		\$102.00	\$102.00
per 1/2 hour	\$ 49.50	\$ 51.00		\$51.00	\$51.00
Waste oil over 5 gal - per gal					
Regulated Garbage Handling	\$ 800.00	\$ 800.00		\$800.00	\$800.00
Regulated Garbage per 100 lbs.	\$ 90.00	\$ 90.00		\$90.00	\$90.00
34.26					
Passenger Fees			remove	remove	remove
34.27 Search & Rescue per hour					
	\$ 99.00	\$ 102.00		\$102.00	\$102.00
34.28					
Boat Launch Ramp					
Per day	\$ 12.00	\$ 13.00		\$13.00	\$13.00
Season Pass	\$ 125.00	\$ 130.00		\$130.00	\$130.00
34.29					
Beaches/Barge Ramp					
Vessels up to 50 ft. per foot	\$ 1.45	\$ 1.50		\$1.50	\$1.50
Vessels over 50 ft. per foot	\$ 1.45	\$ 1.50		\$1.50	\$1.50
34.3					
Fish Dock					
Vessels blocking cranes per hr	\$ 150.00	\$ 150.00		\$150.00	\$150.00
Unattended vessels	\$ 150.00	\$ 150.00		\$150.00	\$150.00
Annual access	\$ 50.00	\$ 52.00		\$52.00	\$52.00
Card Replacement Fee	\$ 15.00	\$ 15.00		\$15.00	\$15.00
Cold Storage Fee per month	\$ 325.00	\$ 334.75		\$334.75	\$334.75
Two consecutive months	\$ 300.00	\$ 309.00		\$309.00	\$309.00
Three consecutive months	\$ 275.00	\$ 283.25		\$283.25	\$283.25
Nine month season	\$ 250.00	\$ 257.50		\$257.50	\$257.50
Bait Storage Fee (4x4x4)					
Per day	\$ 5.00	\$ 5.15		\$5.15	\$5.15
Per week	\$ 25.00	\$ 25.75		\$25.75	\$25.75

Per month	\$ 75.00	\$ 77.25		\$77.25	\$77.25
Crane usage per hour	\$ 88.00	\$ 90.64		\$90.64	\$90.64
Ice per ton	\$ 115.50	\$ 119.00	10% over 2 yrs	\$124.95	\$130.90
Seafood wharfage	\$ 4.62	\$ 4.76		\$4.76	\$4.76
Freight non seafood per ton	\$ 14.00	\$ 14.50		\$14.50	\$14.50
Parking per calendar day	\$ 5.00	\$ 5.00		\$5.00	\$5.00
Labor Day - Memorial Day					
Fish Grinder					
Tub	\$ 4.00	\$ 5.00		\$5.00	\$5.00
Tote	\$ 20.00	\$ 30.00		\$30.00	\$30.00
Swap Request	\$ 25.00	\$ 25.00		\$25.00	\$25.00

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**CITY OF HOMER
HOMER, ALASKA**

Wythe

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RESOLUTION 11-060

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A RESOLUTION OF THE CITY COUNCIL OF HOMER,
ALASKA, ESTABLISHING A COMMITTEE TO DEVELOP A
PORT AND HARBOR IMPROVEMENT REVENUE BONDING
PLAN AND PROVIDE COMMITTEE REVIEW AND
OVERSIGHT THROUGHOUT THE IMPLEMENTATION AND
COMPLETION OF ANY APPROVED PLAN.

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WHEREAS, The City of Homer Capital Improvement Plan (CIP) contains a variety of repair and improvement items for the Port & Harbor district, many of which have been languishing on the CIP list for a number of years with the continued hope of receiving funding from State and Federal agencies; and

WHEREAS, The Homer City Council recognizes the Port and Harbor Enterprise as a vital economic contributor to the community and as such desires to maintain and improve the facilities to ensure continued economic vitality and support economic development within the community; and

WHEREAS, State and Federal funding of these projects becomes increasingly unlikely due to economic shortfalls at all levels of government; and

WHEREAS, The Port and Harbor is an Enterprise Fund which generates revenues that can be allocated to the repayment of financed projects, which qualifies the Enterprise Fund for Revenue Bonding consideration; and

WHEREAS, The Port and Harbor Enterprise Fund currently has no indebtedness; and

WHEREAS, The current Port and Harbor Enterprise Fund depreciation reserve account has a balance of \$1,653,816 as of December 31, 2010.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, supports the establishment of a committee to develop a plan for the implementation of a Port and Harbor Improvement Revenue Bonding Project; and

BE IT FURTHER RESOLVED that the committee will consist of six members, three representatives from the Homer City Council (to be appointed by the Mayor and approved by the Council), the Harbormaster, and two members from the Port and Harbor Advisory Commission (to be selected by the Port and Harbor Advisory Commission). Other staff members will provide administrative and consultative support as requested by the committee or directed by the City Manager.

46 BE IT FURTHER RESOLVED that the committee will make a recommendation to the
47 Council not later than November 1, 2011, including information on the process for revenue bond
48 funding, a proposed timeline for project completion in not longer than three years with a
49 beginning date of no later than June 1, 2012, and including the following items from the Capital
50 Improvement Projects list:

- 51
- 52 a. Harbormaster's Office (to be developed as a model over-slope development
53 project and include public restroom facilities with a separated entrance from the
54 building to allow 24-hour access) \$2,875,000
 - 55
 - 56 b. Harbor Entrance Erosion Control 600,000
 - 57
 - 58 c. Ramp 3 Gangway replacement (to include disability access) 1,700,000
 - 59
 - 60 d. Harbor Float Replacement 3,500,000
 - 61
 - 62 e. Upgrade System 5 – Vessel Shore Power & Water 530,000
 - 63
 - 64 f. Fishdock Restrooms 400,000
 - 65

66 PASSED AND ADOPTED by the Homer City Council this 13th day of June, 2011.

67

68 CITY OF HOMER

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71
72 JAMES C. HORNADAY, MAYOR

73 ATTEST:

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77 JO JOHNSON, CMC, CITY CLERK

78

79 Fiscal Note: \$6,720, Acct. No. 165-375 (HART Trails Reserve)

80
81

ORDINANCE REFERENCE SHEET
2012 ORDINANCE
ORDINANCE 12-17

Emergency Ordinance 12-17, An Emergency Ordinance of the City Council of Homer, Alaska, Appropriating \$30,000 from the Port and Harbor Reserves to Make Emergency Repairs and/or Replace Damaged Float Piles in the Homer Harbor and Authorizing the City Manager to Execute a Change Order to the Existing Contract with Pacific Pile and Marine to Conduct the Harbor Float Piling Repair and Replacement Work in an Amount Not to Exceed \$30,000.

Sponsor: City Manager

1. City Council Regular Meeting April 23, 2012 Introduction
 - a. Memorandum 12-068 from Port and Harbor Director as backup
2. City Council Regular Meeting May 14, 2012 Public Hearing and Second Reading
 - a. Substitute Ordinance 12-17(S) as substituted by Council April 23
 - b. Memorandum 12-068 from Port and Harbor Director as backup
 - c. Memorandum 12-077 from Port and Harbor Director as backup

1 CITY OF HOMER
2 HOMER, ALASKA

City Manager

3
4 ORDINANCE 12-17(S)
5

6 AN ORDINANCE OF THE CITY COUNCIL OF HOMER,
7 ALASKA, APPROPRIATING \$30,000 FROM THE PORT AND
8 HARBOR RESERVES TO MAKE EMERGENCY REPAIRS
9 AND/OR REPLACE DAMAGED FLOAT PILES IN THE
10 HOMER HARBOR AND AUTHORIZING THE CITY
11 MANAGER TO EXECUTE A CHANGE ORDER TO THE
12 EXISTING CONTRACT WITH PACIFIC PILE AND MARINE
13 TO CONDUCT THE HARBOR FLOAT PILING REPAIR AND
14 REPLACEMENT WORK IN AN AMOUNT NOT TO EXCEED
15 \$30,000.
16

17 WHEREAS, R and S floats in the Homer harbor have pilings that are severely damaged
18 from corrosion to the point that their structural integrity may be in question; and
19

20 WHEREAS, Quick repair and replacement actions are necessary in order to avoid
21 potential liability, loss of revenue, loss of service to customers, and a potential closure of these
22 floats; and
23

24 WHEREAS, The damage to these piles is known to the Council, the Port and Harbor
25 Commission, and the Harbor Improvement Committee and the work is part of the proposed
26 improvements to be financed by grants and the sale of revenue bonds; and
27

28 WHEREAS, The piles are in such bad condition that it is not advisable to wait until the
29 Fall of 2013 to replace them; the likely scenario if Council relies on grants and a bond sale; and
30

31 WHEREAS, The Council recently adopted Resolution 12-028 which awarded a contract
32 to repair damaged fenders on the Deep Water Dock to Pacific Pile and Marine; and
33

34 WHEREAS, Pacific Pile and Marine will be bringing a barge with a mounted crane to
35 Homer to do the fender work and this equipment is uniquely suited to do the work in the harbor
36 on the damaged piles; and
37

38 WHEREAS, A change order to the existing contract for fender repair to include repair of
39 the damaged piles would save money by eliminating mobilization costs and address a potential
40 public safety issue in an expedited manner; and
41

42 WHEREAS, An emergency appropriation allows the company to plan for and if
43 necessary, acquire materials prior to its arrival in Homer next week; and
44

45 WHEREAS, The City Attorney has indicated that if the City pays for this work from its
46 own depreciation funds, it may be possible to reimburse the fund with bond sale proceeds later.

NOW, THEREFORE, BE IT ORDAINED BY THE CITY OF HOMER:

Section 1. The Homer City Council hereby appropriates \$30,000 from the Port and Harbor depreciation reserves for emergency repairs to float piles in the Homer harbor as follows:

<u>Expenditure</u>	<u>Account No.</u>	<u>Description</u>	<u>Amount</u>
	456-380-5261	Float Repair and Replacement	\$30,000

Section 2: This ordinance is a budget amendment only, is not of a permanent nature, and shall not be codified.

Section 3: The Council finds that an emergency exists for the reasons stated above and to protect public health and safety.

Section 3. Upon enactment, the City Manager is authorized to execute a change order to the existing contract with Pacific Pile and Marine to conduct the harbor float piling repair and replacement work in an amount not to exceed \$30,000.

ENACTED BY THE CITY COUNCIL OF HOMER, ALASKA, this ____ day of _____, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

ATTEST:

JO JOHNSON, CMC, CITY CLERK

YES:

NO:

ABSTAIN:

ABSENT:

First Reading:

Public Hearing:

Second Reading:

Effective Date:

93 Reviewed and approved as to form:
94
95
96
97 Walt Wrede, City Manager
98
99 Date: _____
100
101
102

Thomas F. Klinkner, City Attorney
Date: _____




City of Homer

Port / Harbor

4350 Homer Spit Road
Homer, Alaska 99603-8005

Telephone (907) 235-3160
Fax (907) 235-3152
E-mail port@ci.homer.ak.us
Web Site <http://port.ci.homer.ak.us>

MEMORANDUM 12-068

TO: HOMER CITY COUNCIL & CITY MANAGER WALT WREDE
FROM: BRYAN HAWKINS, PORT DIRECTOR/HARBORMASTER 
DATE: APRIL 17, 2012
SUBJECT: EMERGENCY REPAIRS TO R & S FLOAT PILINGS

Background

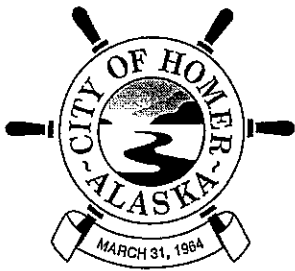
In my last presentation regarding harbor improvements to City Council there was a slide showing a piling on R Float that had a large hole in it. Staff has been working on the problem this spring in an effort to find a workable solution to this serious safety hazard. We have identified three pilings on R and S floats that must be dealt with as soon as possible.

Port Maintenance confirmed that we already have replacement piling in our stockpile suitable for this task. Pacific Pile and Marine, the company who won the bid for making repairs to the Deep Water Dock Fender Repair, has given us a bid to replace the effected piles using their crane and barge while it is here on contract. The only way to perform a repair to pilings in the harbor is with a barge-mounted crane. It is to our advantage to get this work done while they are on the job as it will save the Enterprise mobilization costs under a new contract.

Recommendations

Authorize staff to issue a change order to the Deep Water Dock Fender Repair project, changing the scope of work to include replacing up to three but no more than four pilings identified by the Public Works Director and Port Maintenance staff.

Fiscal Note: GL 456-380-5261



City of Homer

Port / Harbor

4350 Homer Spit Road
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Web Site <http://port.ci.homer.ak.us>

MEMORANDUM 12-077

TO: HOMER CITY COUNCIL
CC: CITY MANAGER WALT WREDE
FROM: PORT DIRECTOR/HARBORMASTER BRYAN HAWKINS
DATE: MAY 8, 2012
RE: CHANGE ORDER TO ORDINANCE 12-17(S), DEEP WATER DOCK REPAIR, TO INCLUDE EMERGENCY REPAIRS TO R & S FLOAT PILINGS

Staff discovered after the company Pacific Pile and Marine's equipment arrived on scene to perform the Deep Water Dock repair that their barge had a wider beam than we had originally believed. Staff believes that the risks versus benefits involved with moving the barge and crane into place between R and S floats were too high for us to guarantee a successful conclusion for the piling replacement project. Staff is working on other solutions to make the three compromised pile in that location safe. R and S floats and the supporting piling are on the list for replacement in our Harbor Improvement Plan.

Recommendation

Vote no on approving the change order to Ordinance 12-17(S).

ORDINANCE REFERENCE SHEET
2012 ORDINANCE
ORDINANCE 12-18

An Ordinance of the City Council of Homer, Alaska, Amending the 2012 Operating Budget to Allow for the Completion of the 2012 Streets Repaving Project Utilizing Homer Accelerated Roads and Trails Program (HART) Funds in an Amount Not to Exceed \$900,000.

Sponsor: City Manager/Public Works Director

1. City Council Regular Meeting April 23, 2012 Introduction
 - a. Memorandum 12-065 from Public Works Director as backup
 - b. Draft Costs Proposal

2. City Council Regular Meeting May 14, 2012 Public Hearing and Second Reading
 - a. Memorandum 12-065 from Public Works Director as backup
 - b. Draft Costs Proposal

1 CITY OF HOMER
2 HOMER, ALASKA

3 City Manager/
4 Public Works Director

5 ORDINANCE 12-18

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7 AN ORDINANCE OF THE CITY COUNCIL OF HOMER,
8 ALASKA, AMENDING THE 2012 OPERATING BUDGET TO
9 ALLOW FOR THE COMPLETION OF THE 2012 STREETS
10 REPAVING PROJECT UTILIZING HOMER ACCELERATED
11 ROADS AND TRAILS PROGRAM (HART) FUNDS IN AN
12 AMOUNT NOT TO EXCEED \$900,000.

13
14 WHEREAS, Periodically, Public Works coordinates the repaving of roads where the
15 pavement has reached its useful life (20-30 years old), including grinding of existing pavement
16 and resurfacing; and

17
18 WHEREAS, Public Works has successfully completed similar projects in 2006 and 2009
19 utilizing HART funds; and

20
21 WHEREAS, The Port and Harbor Department has requested that Ice Dock Road be
22 paved and pavement be extended on Fish Dock Road; and

23
24 WHEREAS, Public Works has inspected pavement conditions on all City maintained
25 roads and recommends that seven road sections be resurfaced (see Memorandum 12-065) and
26 estimated the cost of each.

27
28 NOW, THEREFORE, THE CITY OF HOMER ORDAINS:

29
30 Section 1. The FY 2012 Operating Budget is hereby amended by appropriating not more
31 than \$900,000 from the Homer Accelerated Roads and Trails Program funds for completing the
32 2012 Street Repaving Project.

33
34 Expenditure:

<u>Account No.</u>	<u>Description</u>	<u>Amount</u>
160-766	Homer Accelerated Roads & Trails Program (HART)	\$900,000

35
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38
39 Section 2. This is a budget amendment ordinance, is not permanent in nature, and shall
40 not be codified.

41
42 ENACTED BY THE CITY COUNCIL OF HOMER, ALASKA, this _____ day of
43 _____, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

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54 ATTEST:

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59 JO JOHNSON, CMC, CITY CLERK

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62 YES:
63 NO:
64 ABSTAIN:
65 ABSENT:

66
67 First Reading:
68 Public Hearing:
69 Second Reading:
70 Effective Date:

71
72 Reviewed and approved as to form:

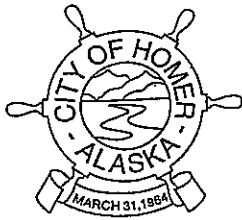
73
74
75 Walt E. Wrede, City Manager

Thomas F. Klinkner, City Attorney

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77
78 Date: _____

Date: _____

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80



CITY OF HOMER

PUBLIC WORKS

3575 HEATH STREET HOMER, AK 99603

TELEPHONE (907)235-3170

FAXSIMILE (907)235-3145

EMAIL: jjonker@ci.homer.ak.us

MEMORANDUM 12-065

To: Walt Wrede, City Manager
From: Jan C. Jonker, Superintendent
Thru: Carey Meyer, PW Director
Date: 16 April 2012
Subject: PROPOSED STREETS REPAVING PROGRAM - 2012

Discussion

In 2006 & 2009 the Public Works Department successfully completed two much needed repaving projects which encompassed (9) streets totaling 3.45 miles. The majority of these streets were originally paved in 1983/84 era.

We are again proposing a 2012 Repaving Project which will include another (4) streets which also were originally paved in 1983/84 and (2) streets that were originally paved in 1991/93. We will also, as requested by Port & Harbor, include the paving of Ice Dock Road and the pavement extension of Fish Dock Road.

Project Costs Estimates

A. Streets Repaving:	\$542,225	(includes design, admin and construction costs)
B. Port/Harbor Paving:	<u>\$339,725</u>	(includes design, admin and construction costs)
Total Combined Project:	\$881,950	

Attached please find two spreadsheets with detailed project costs estimates.

Recommendation

1. Expend an amount "not to exceed" \$900,000.00 this fiscal year from the H.A.R.P. fund for the design & construction to include grinding of existing asphalt and repave the streets and Port/Harbor work as identified on the attached spreadsheets.

Fiscal Note

Currently the HARP Fund contains approximately \$5.19 million dollars as of end of year 2011.

**City of Homer - Public Works Department
2012 Roads Repaving Program**

Road Name	Year Paved	From	To	Asphalt Condition	Roadway Length Linear Feet	Pavement Width Feet	Total Sq Yds	
Klondike Ave.	1983/84	Heath Street	Lucky Shot	poor	680	24	1,813	
Smokey Bay Way	1983/84	Lake St	Ben Walters Lne	poor	570	24	1,520	
Fairview Ave (1)	1991	Bartlett St	Mulliken St	poor	1,600	26	4,622	
Fairview Ave (2)	1983/84	Main St	Hohe St	poor	650	24	1,733	
Towne Hgts Lne	1991	Mountain View	Islandview Ave	poor	645	24	1,720	Priority 1
Greatland Street	1983/84	Pioneer Ave.	150' South	poor	150	24	400	Group "A"
Airport Rd.	1993	Intersection w/ Spit Rd		poor	100	32	356	
					4,395	Total	12,164	2012
Early Spring Street	1983/84	Mark White Ave.	Aspen Lane	good	650	24	1,733	
Main Street	1983/84	Bunnell Ave.	Charles Way	good	300	24	800	
Mark White Ave.	1983/84	Rochelle Road	Early Spring St.	good	310	24	827	Priority 2
Smokey Bay Way	1983/84	Lee Street	Ben Walters Lne.	good	650	24	1,733	Group "B"
					1,260	Total	3,360	2014

City of Homer - Public Works Department
2012 Roads Repaving Program

Estimated Construction Cost

Estimated Construction Costs (grind 4" depth, regrade, adjust utilities & repave @ 2" new asphalt)

Street Name	Total Sq Yds	Grind @ \$5.25/sy	Regrade @ \$0.50 / sy	(300 ton) Level Course @\$50 / ton	2" AC Repave @ \$17.50/sy	(14 SSMH"s) Adjust SSMH \$500 ea	(12 GV's) Adjust Valve Box \$500 ea	Striping @1.25/lf	Mobe @ 20% Const.	Total Estimated
Klondike Ave.	1,813	\$ 9,520.00	\$ 906.67	\$ 2,000.00	\$ 31,733.33	\$ 2,500.00	\$ -	\$ -	\$ 9,332.00	\$ 55,992.00
Smokey Bay Way	1,520	\$ 7,980.00	\$ 760.00	\$ 2,000.00	\$ 26,600.00	\$ 1,000.00	\$ 1,500.00	\$ 3,781.25	\$ 7,968.00	\$ 51,589.25
Fairview Ave (1)	4,622	\$ 24,266.67	\$ 2,311.11	\$ 2,500.00	\$ 80,888.89	\$ 1,000.00	\$ 3,500.00	\$ 3,375.00	\$ 22,893.33	\$ 140,735.00
Fairview Ave (2)	1,733	\$ 9,100.00	\$ 866.67	\$ 2,500.00	\$ 30,333.33	\$ 500.00	\$ -	\$ 3,312.50	\$ 8,660.00	\$ 55,272.50
Towne Hgts Lne	1,720	\$ 9,030.00	\$ 860.00	\$ 1,000.00	\$ 30,100.00	\$ 1,500.00	\$ 500.00	\$ -	\$ 8,598.00	\$ 51,588.00
Greatland St.	400	\$ 2,100.00	\$ 200.00	\$ 2,500.00	\$ 7,000.00	\$ 500.00	\$ 500.00	\$ -	\$ 2,560.00	\$ 15,360.00
Airport Rd. Int.	356	\$ 1,869.00	\$ 178.00	\$ 2,500.00	\$ 6,230.00	\$ 500.00	\$ 500.00	\$ -	\$ 2,355.40	\$ 14,132.40
Totals	12,165	\$ 63,865.67	\$ 6,082.44	\$ 15,000.00	\$ 212,885.56	\$ 7,500.00	\$ 6,500.00	\$ 10,468.75	\$ 62,366.73	\$ 384,669.15

Traffic Control LS

Total Construction \$ 389,669.15
10% Contingency \$ 38,966.92
Grand Total Construction \$ 428,636.07

Estimated Total Project Costs (construction, design & admin overhead)

Construction Costs	\$ 428,636.07
Design/Inspect @ 10%	\$ 42,863.61
Admin @ 15%	\$ 70,724.95
Total Pave/Repave Project	\$ 542,224.62

**City of Homer - Public Works Department
2012 Roads Repaving Program**

Port/Harbor Paving

	Total Sq Yds	2" Level Course Tons	Level Course @ \$50 / ton	2" AC @\$17.50/sy	Storm Drain Lump Sum	SD Detention Lump Sum	Adjust SSMH \$500 ea	Adjust Valve Box \$500 ea	Mobe @ 20% Const.	Total Estimated
Ice Dock Road	1,120	250	\$ 12,500.00	\$ 19,600.00	\$ 30,000.00	\$	\$ 500.00	\$ 2,000.00	\$ 12,920.00	\$ 77,520.00
Fish Dock Road	1,250	400	\$ 20,000.00	\$ 21,875.00	\$ 65,000.00	\$ 25,000.00	\$ 1,500.00	\$	\$ 26,675.00	\$ 160,050.00
Totals	2,370	650	\$ 32,500.00	\$ 41,475.00	\$ 95,000.00	\$ 25,000.00	\$ 2,000.00	\$ 2,000.00	\$ 39,595.00	\$ 237,570.00

Total Construction \$ 237,570.00
10% Contingency \$ 23,757.00
Grand Total Construction \$ 261,327.00

Construction Costs \$ 261,327.00
Design/Inspect @ 15% \$ 39,199.05
Admin @ 15% \$ 39,199.05
Total Pave Project \$ 339,725.10

Grand Total Pave / Repave Projects

PW Streets Repave \$ 542,224.62
Port/Harbor New Pave \$ 339,725.10
Grand Total Project \$ 881,949.72

ORDINANCE(S)

ORDINANCE REFERENCE SHEET
2012 ORDINANCE
ORDINANCE 12-20

An Ordinance of the City Council of Homer, Alaska, Amending the FY 2012 Operating Budget by Appropriating and Transferring \$3,000 from the General Fund Reserve (Fund Balance) to the Public Arts Committee for the Baycrest Overlook Improvement Gateways Project.

Sponsor: City Manager

1. City Council Regular Meeting May 14, 2012 Introduction
 - a. Memorandum 12-070 from PAC as backup
 - b. Memorandum 12-071 from PAC as backup
 - c. Minutes of April 2, 2012 PAC meeting as backup

CITY OF HOMER
HOMER, ALASKA

City Manager

ORDINANCE 12-20

AN ORDINANCE OF THE CITY COUNCIL OF HOMER, ALASKA, AMENDING THE FY 2012 OPERATING BUDGET BY APPROPRIATING AND TRANSFERRING \$3,000 FROM THE GENERAL FUND RESERVE (FUND BALANCE) TO THE PUBLIC ARTS COMMITTEE FOR THE BAYCREST OVERLOOK IMPROVEMENT GATEWAYS PROJECT.

WHEREAS, The Public Arts Committee has an opportunity to move the new Baycrest Hill Gateway improvement project forward with minimal funds; and

WHEREAS, Through the collaboration of the National Parks, State Parks, Kachemak Bay Research Reserve, U.S. Fish and Wildlife, Homer Chamber of Commerce, Pratt Museum, and Alaska State Department of Transportation, it is possible to make the Baycrest Hill an outstanding gateway to the community; and

WHEREAS, The Public Arts Committee requests \$3,000 to contribute to the partners who will work together to make the Baycrest Hill an outstanding gateway to the community.

NOW, THEREFORE, THE CITY OF HOMER ORDAINS:

Section 1. The Homer City Council hereby amends the FY 2012 Operating Budget by appropriating and transferring \$3,000 from the General Fund Reserve (Fund Balance) to the Public Arts Committee for the Baycrest Overlook Improvement Gateways Project as follows:

Appropriation / Transfer From:

<u>Account</u>	<u>Description</u>	<u>Amount</u>
100-100	General Fund Reserves	\$3,000

Transfer to:

<u>Account</u>	<u>Description</u>	<u>Amount</u>
156-367	Public Arts Committee Baycrest Overlook Improvement Gateways Project	\$3,000

Office of the City Clerk

Jo Johnson, CMC, City Clerk

Melissa Jacobsen, CMC, Deputy City Clerk II
Renee Krause, CMC, Deputy City Clerk I



491 E. Pioneer Avenue
Homer, Alaska 99603-7624
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Extension: 2227
Extension: 2224

Fax: (907) 235-3143
Email: clerk@ci.homer.ak.us

MEMORANDUM 12-070

TO: MAYOR HORNADAY AND CITY COUNCIL
FROM: ANGIE NEWBY, CHAIR, PUBLIC ARTS COMMITTEE
THROUGH: WALT WREDE, CITY MANAGER
DATE: APRIL 10, 2012
RE: BAYCREST OVERLOOK IMPROVEMENT GATEWAYS PROJECT

The Public Arts Committee has an extraordinary opportunity to move our new Baycrest Hill project forward. Baycrest Hill is another part of our overall gateway project. You all remember that the beautiful airport diorama photo display which was an outgrowth of the original Gateway concept. Through the efforts of PAC member Marianne Aplin, we received an incredible display with an estimated value of \$35000, for zero City dollars. This was collaboration between partners at the National Parks, State Parks, the Kachemak Bay Research Reserve and USF&W.

These same folks plus the Homer Chamber of Commerce, Pratt Museum and Alaska State DOT have agreed to work together again on making the Baycrest Hill an outstanding gateway to our community. We have met to review how we start the process of visioning, planning and design of interpretative material that will eloquently be the voice of our Homer story. State Parks have offered the services of their planning staff as project facilitators and designers for a bare bones fee of \$6000. Joe Meehan, Lands and Refuge Manager for ADF&G has stepped forward with \$3000 towards that effort. The public Arts Committee has voted unanimously to request a match for the remainder \$3000 from the City of Homer. This project will have a major economic development impact since the Baycrest Hill access to Homer is the primary way we draw people into our magical Cosmic Hamlet by the Sea. In addition, it will provide a signage format for other locations including the Port of Homer. The members on this Baycrest subcommittee are all decision makers in their agencies and include Joe Meehan, Lands and Refuge Manager for ADF&G, Joel Hard, Lake Clark National Park Superintendent, Terry Thompson, Reserve Manger of KBRR, Roger MacCampbell, Kachemak Bay State Park Ranger, Monte Davis, HCOC Executive Director, Chris Post, Alaska DOT Project Manager, Rygil Christianson, Director of Education at the Pratt Museum, Marianne Aplin, Visitor Center Manager at Alaska Islands and Oceans, Louie Flora, from Rep. Paul Seaton's office, and Angie Newby, current PAC Chair.

"WHERE THE LAND ENDS AND THE SEA BEGINS"

To access City Clerk's Home Page on the Internet: <http://clerk.ci.homer.ak.us>

Office of the City Clerk

Jo Johnson, CMC, City Clerk

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MEMORANDUM 12-071

TO: MAYOR HORNADAY AND CITY COUNCIL
FROM: PUBLIC ARTS COMMITTEE
THROUGH: WALT WREDE, CITY MANAGER
DATE: APRIL 10, 2012
RE: REQUEST FOR SUPPORT AND FUNDING CONTRIBUTION FOR THE
BAYCREST OVERLOOK IMPROVEMENT GATEWAYS PROJECT

On February 24, 2012 the committee formed to work on the Baycrest Hill Overlook Gateways Project met to discuss the initial steps to get this project off the ground. Participating in the meeting were representatives of various state and federal agencies plus the Homer Chamber of Commerce and Public Arts Committee. Joe Meehan with the Alaska Department of Fish and Game Division of Wildlife Conservation was able to find in his budget half of the funding for the required Interpretive Site Development Plan. At that meeting the other members stated that upon careful review of their respective budgets they were unable to find the funds necessary to provide the remaining \$3000. A motion was made and seconded to submit a request to the Public Arts Committee and the City of Homer to see if they can find in their budget the funding necessary to get this project started.

During the April 2, 2012 Special Meeting the committee moved and seconded a motion to request City Council approve a request to fund the remaining amount needed to complete the interpretive site plan required for the project. Following is the excerpt from the minutes of that meeting:

PENDING BUSINESS

- A. *Gateways Project – Baycrest Hill Status Update*
1. *Emails Regarding the project*

Chair Newby introduced the item for discussion. She noted the series of emails included in the packet. She commented on the interest from Representative Seaton's office and that is very promising to have his support of this project.

Ms. Aplin provided a brief report on the progress of the project and explained the current roadblock that they are encountering with funding. She provided the members of the sub-committee were in attendance at the last meeting on February 24, 2012. These are Monte Davis, Chamber of Commerce; Joe Meehan, Alaska Department of Fish & Game Division of Wildlife Conservation; Chris Post, Department of Transportation; Terry Thompson, Kachemak Bay Research Reserve; a ranger

"WHERE THE LAND ENDS AND THE SEA BEGINS"

To access City Clerk's Home Page on the Internet: <http://clerk.ci.homer.ak.us>

(name not provided) representing Joel Hard, Lake Clark National Park and Preserve and Rijil Christianson, The Pratt Museum. The committee requires \$3000 more to complete the interpretive site plan before they can proceed to the next phase. Mr. Meehan has found \$3000 to contribute. No other partners are able to come up with the remaining funds needed to get this project started at this time. The sub-committee approved a motion to submit a request for funding from the City of Homer.

APLIN/MILLER – MOVED TO SUBMIT A REQUEST TO CITY COUNCIL TO APPROVE THE REQUEST FOR \$3000 TO COMPLETE THE INTERPRETIVE SITE PLAN FOR THE BAYCREST OVERLOOK GATEWAYS PROJECT.

Ms. Aplin explained that this site plan is required for the project and none of the other partners can fund that amount this year. She also noted that they recently have new members the Kachemak Land Trust and the Kachemak Bay Conservation Society.

VOTE. YES. NON-OBJECTIONS. UNANIMOUS CONSENT.

Motion carried.

Chair Newby commented that the subcommittee members have exhausted all of the possibilities for funding and maybe later on in the project there will be additional opportunities to find funding from outside sources.

There was no further discussion.

RECOMMENDATION

Approve the request of \$3000 to provide the funding necessary to start the improvements on the Baycrest Hill Overlook Gateways Project.

Session 12-02 A Special Meeting of the Public Arts Committee was called to order at 5:15 P.M. on April 2, 2012 by Chair Angie Newby in Cowles Council Chambers located at City Hall, 491 E. Pioneer Avenue, Homer, Alaska.

PRESENT: COMMITTEEMEMBERS FELLOWS, APLIN, MILLER, NEWBY AND WOLFE

STAFF: DEPUTY CITY CLERK KRAUSE

The Public Arts Committee met in a Worksession prior to the Special meeting. They discussed the regular agenda items. Committee member Miller was not present for the Worksession but did attend the Special Meeting.

The members of the committee took a brief tour of City Hall to view the artwork that Committee member Wolfe hung with assistance from Staff member Renee Krause.

AGENDA APPROVAL

The agenda was approved by consensus of the committee.

PUBLIC COMMENTS REGARDING ITEMS ON THE AGENDA

There were no comments.

RECONSIDERATION

There were no items for reconsideration.

APPROVAL OF MINUTES (*Minutes are only approved during regular meeting.*)

There were no minutes for approval.

VISITORS

There were no visitors scheduled.

STAFF & COUNCIL REPORT/COMMITTEE REPORTS/BOROUGH REPORTS

A. Staff Report 12-02 dated March 29, 2012

Chair Newby opened discussion on the staff report and inquired about further information on the Teleconference with NEA.

Ms. Krause noted that the Baycrest project may qualify for the \$10,000.00 one to one match grant. Ms. Miller and Ms. Newby will try to attend. Ms. Miller will RSVP in the morning.

There was a brief discussion on holding an Open House for the City Hall. Ms. Krause will check with City Staff to see when would be the best time. It may be as late as June for this event. There was a brief discussion that the renovated city hall is more for residents than visitors this time. Residents are the prime audience. Ms. Aplin and Ms. Wolfe stated that this was important enough not to piggy back on another event. It was acknowledged that this was to be coordinated with all departments, advertised and marketed in advance. It was recommended that this be held on May 18, 2012, Friday, 4:00 p.m. to 6:00 p.m. Staff will check with Administration to make sure this would be okay.

Chair Newby noted that her report to City Council was included in the packet. She summarized the report for the committee members.

There was no further discussion.

PUBLIC HEARING

There were no items for public hearing.

PENDING BUSINESS

- A. Gateways Project – Baycrest Hill Status Update
 - 1. Emails Regarding the project

Chair Newby introduced the item for discussion. She noted the series of emails included in the packet. She commented on the interest from Representative Seaton's office and that is very promising to have his support of this project.

Ms. Aplin provided a brief report on the progress of the project and explained the current roadblock that they are encountering with funding. She provided the members of the sub-committee were in attendance at the last meeting on February 24, 2012. These are Monte Davis, Chamber of Commerce; Joe Meehan, Alaska Department of Fish & Game Division of Wildlife Conservation; Chris Post, Department of Transportation; Terry Thompson, Kachemak Bay Research Reserve; a ranger (name not provided) representing Joel Hard, Lake Clark National Park and Preserve and Rijil Christianson, The Pratt Museum. The committee requires \$3000 more to complete the interpretive site plan before they can proceed to the next phase. Mr. Meehan has found \$3000 to contribute. No other partners are able to come up with the remaining funds needed to get this project started at this time. The sub-committee approved a motion to submit a request for funding from the City of Homer.

APLIN/MILLER – MOVED TO SUBMIT A REQUEST TO CITY COUNCIL TO APPROVE THE REQUEST FOR \$3000 TO COMPLETE THE INTERPRETIVE SITE PLAN FOR THE BAYCREST OVERLOOK GATEWAYS PROJECT.

Ms. Aplin explained that this site plan is required for the project and none of the other partners can fund that amount this year. She also noted that they recently have new members the Kachemak Land Trust and the Kachemak Bay Conservation Society.

VOTE. YES. NON-OBJECTIONS. UNANIMOUS CONSENT.

Motion carried.

Chair Newby commented that the subcommittee members have exhausted all of the possibilities for funding and maybe later on in the project there will be additional opportunities to find funding from outside sources.

There was no further discussion.

- B. Status Update on the Artwork Display

Chair Newby introduced the item for discussion.

Ms. Wolfe commented on the process and that the results have been very satisfying. She noted for the record that 95% of the artwork owned by the city is sister city related and there is a lack

of Homer or Alaskan artwork. She further commented on research into the Photographic Diorama by Alan Parks was not given to the City of Homer it has been "on loan". The photos are only proofs and she will be working with Mr. Parks on this project. He will be submitting a proposal on what is required, timeline and cost. Mr. Parks approved of the location along the downstairs hallway which is a high traffic area. There is also an acetate overlay with the signatures of many that were in the photos.

Chair Newby noted that they will then have to submit another request to City Council for funding.

Alan Parks noted that this was a 100 year picture which has been in the Council Chambers for 15 years. So this is historical also.

Committee members thanked Ms. Wolfe for her time and discretion in handling this matter.

Next the committee addressed the status update on the Airport Welcome to Homer letters. Ms. Fellows reported that Sherman who did the letters for Islands & Ocean and he wanted \$150.00 - \$175 per letter depending on the metal chosen. Bay Welding does not do this type of work; the business recommended in Sterling is not open yet; Ms. Fellows will look into the costs for painting letters. Painting the letters in place should be a viable option. Ms. Krause noted that she has sent requests for price quotes to three companies listed online and has not received responses yet. She should have an update at the next meeting. Ms. Krause stated that the Procurement Policy must be followed and there were questions regarding security issues. City Manager Wrede is the acting Airport Manager. Ms. Krause noted that Mike Riley could be contacted for information on access. Chair Newby added that all quotes should be in writing and submitted to Ms. Krause at the Clerk's Office.

Chair Newby commented that it would be nice to have this completed before the summer season ends this year.

There was a brief discussion on color selection of the letters and it was recommended that a bright color and anything but brown would be good.

There was no further discussion.

NEW BUSINESS

A. Budget 2012 – Amendments

Chair Newby introduced the item for discussion.

Ms. Krause added this item to the agenda since discovered that in reviewing the budget for Public Arts Committee Council approved \$5000 instead of \$4000. She wanted to give the Committee the opportunity to amend the budget line items.

The committee agreed by consensus to leave the money for now as a surplus and once they have some costs back for the letters and the photos they can make adjustments then.

B. Review and Approve Recommendations for Rack Card Design and Production

Chair Newby introduced the item for discussion and acknowledged that hard efforts of the staff.

A discussion ensued on the benefits to online versus local, ready artwork of adequate quality, free services for first time orders, a la carte for maps, etc. Prices do not seem out of range from Printworks and the question is that there is an additional cost for non-camera ready work, map also. Staff stated that she

could have mock ups ready for view at the next meeting for approval and will contact local services.

Additional comments were noted that inclusion of artwork outside of City Hall and quantity decided on for first printing was 1500.

C. Letter Received Re: Placement of City Owned Artwork in City Hall

Chair Newby introduced the item and commented that they discussed this letter during the artwork placement. Ms. Krause explained that this letter went directly to council and the City Manager stated that he had forwarded the letter to the committee to be addressed.

Chair Newby offered to respond to the letter and will address her response to council acknowledging the personal/public work space, the fact that 95% of the City's collection is Sister City related and the difficulties surrounding the emotional issues involved in artwork.

The committee discussed the possibility of negative comments by the public on the sister city artwork and felt that likely there were questions regarding the choice of Japanese art instead of Homer/Alaskan influenced art. The committee opined that this would have been an opening and used as an information or teaching opportunity on the Sister City program that the City is involved in and that during the hanging process department personnel were assured that this was a temporary placement until something more representative of Homer could be found.

Ms. Wolfe did note that she has shown the two pieces that were going to be donated and recommended as possible replacements for the main upstairs reception area and Ms. Swisher approved of either piece as appropriate.

There was no further discussion.

INFORMATIONAL ITEMS

- A. Alaska Council on the Arts NES Challenge America Fat Track Grant Opportunity – An Opportunity for \$10,000 One to One Match
- B. February 2012 ASCA Communique

There was no discussion on the informational materials.

COMMENTS OF THE AUDIENCE

There was no audience present.

COMMENTS OF THE COUNCILMEMBER *(If one is assigned)*

There was no council member present.

COMMENTS OF THE CITY STAFF

Ms. Krause stated that it was a good meeting and the committee has done a great job.

COMMENTS OF THE CHAIR

Chair Newby really appreciates all the efforts on all these projects and thanked Ms. Wolfe for the time and efforts in the hanging the artwork. They did not realize all that would be involved.

COMMENTS OF THE COMMITTEE

Ms. Wolfe responded to Ms. Fellows question regarding the Mural on Heath Street that she will be contacting Jean since she was the original coordinator and has been involved in previous re-touchups. She has the original children who now have children of their own and it would be a great project to have those same participants. Ms. Fellows volunteered to assist Ms. Wolfe in the project.

Ms. Wolfe informed the committee about the new mural at Paul Banks Elementary and it will be unveiled on Friday April 6th. This project had full support of the faculty and parents.

Ms. Aplin thanked Ms. Wolfe and Ms. Krause for all their work.

Ms. Miller and Ms. Fellows had no additional comments.

ADJOURN

There being no further business to come before the Committee, Chair Newby adjourned the meeting at 6:15 p.m. A worksession is scheduled for 4:00 p.m. prior to the meeting. The next Regular Meeting is scheduled for Wednesday, May 16, 2012 at 5:00 p.m., City Hall, Cowles Council Chambers 491 E. Pioneer Avenue, Homer, Alaska.

RENEE KRAUSE, CMC, DEPUTY CITY CLERK I

Approved: _____

CITY MANAGER'S REPORT

Acting City Manager's Report May 14, 2012

As the City Manager is utilizing some much deserved leave time he suggested that I take this opportunity to provide a short report on the status of the Homer Volunteer Fire Department. This year marks the 60th Anniversary of the Department which was founded in 1952 (incorporated at a non-profit in 1954), predating the establishment of the City of Homer by some 12 years. The Department has a long and proud history of providing fire protection and ambulance services to the citizens of Homer and the surrounding areas. Our members, including the 5 paid staff and some 46 volunteers provide a wide range of services to the community beyond simply extinguishing fires and taking those in need to the hospital for treatment. We provide hundreds of hours each year presenting fire and injury prevention programs to various groups; we oversee the Open Burn Permitting program within the City of Homer; we take blood pressures for folks that drop by the station when requested by their care providers; we function as the City's Emergency Management Agency and house the City's Emergency Operations Center and coordinate the development and revisions of the Emergency Operating Plans (EOP) and All Hazard Disaster Mitigation Plans; we provide CPR and other training to city employees so that they can take care of folks until the ambulance arrives (we were the first city in Alaska to have Automated External Defibrillators in police cars); and we act as ambassadors of good will for the folks that drop by the fire station to trade fire department patches or ask questions about almost anything, all on top of training and responding to every manner of emergency.

In 2011 the Homer Volunteer Fire Department responded to 513 requests for assistance including: 438 ambulance calls; 15 structure fires; 11 wildland fires; 8 vehicle fires; 4 rescue calls and 37 "public assist" calls for some accumulated 2,210 hours of actual service by our members. In addition we conducted a myriad number of training classes for our members totaling over 3,439 "man" hours. We don't even attempt to track the countless hours our members are simply available for call both on their assigned week-long shifts or not (all large fire and EMS calls are what we term as "all call" meaning that any and all available members should respond).

Like many departments within the City we are providing the best services we can while staying within budget and strive to make every dollar we get from the taxpayers go as far as it can but our facility and fleet are aging. While our first out fire engine and ambulance are relatively new and top of the line we have two fire apparatus that are well past recommended time to either retire and replace or refurbish (a 1984 Engine/Pumper and a 1989 Tanker/Pumper). We also have a 1990 F-350 that functions as our only wildland/brush truck that is seriously overloaded and needing to be replaced. Our station (constructed by the volunteer corporation in 1980 and upgraded in 1997 by the City) is also in need of expanding or replacing (was on the City CIP list for many, many years until removed due to lack of potential funding). The fire department vehicles are all currently on the CIP list for funding and we are seeking state grant funding to possibly replace our oldest ambulance next year. The Department has been very fortunate over the last ten years or so in securing federal and state legislative funding. We replaced one ambulance and our 35 Self Contained Breathing Apparatus

(SCBA) through legislative funding and our large breathing air compressor and live-burn training systems through the Assistance to Firefighters Grants.

As busy as we are for a small, mostly volunteer Fire Department we would like to be able to expand our services to include a Fire Marshal's Office/Building Official to be able to conduct local commercial building plan approval and routine fire inspections of commercial occupancies. Currently, as a non-deferred jurisdiction, those responsibilities fall to the State Fire Marshal's Office in Anchorage. With only a handful of plan reviewers and inspectors for all of south central Alaska they don't have the time or ability to inspect every commercial building within this large area. We are the only municipality on the Kenai Peninsula that is not deferred (meaning we cannot conduct our own plan reviews and inspections). While this would require an additional employee trained to conduct plan reviews and inspections we feel that much of that cost could be re-captured in the collection of plan review fees currently being charged by the State Fire Marshal's Office.

Office of the City Clerk

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MEMORANDUM - REPORT

TO: MAYOR HORNADAY AND HOMER CITY COUNCIL
FROM: MELISSA JACOBSEN, CMC, DEPUTY CITY CLERK
DATE: MAY 9, 2012
SUBJECT: BID REPORT

Homer Airport General Concession Lease and Car Rental Concession Lease - Sealed proposals for the leasing of two (2) general concession spaces at the Homer Airport Terminal and Sealed proposals for the leasing of two (2) car rental concession spaces at the Homer Airport Terminal will be received at the office of the City Clerk, 491 E. Pioneer Avenue, Homer, AK 99603 prior to **2:00 p.m. on Friday, May 18, 2012**. General concession leases could include, but are not limited to businesses such as: food vendors, coffee stands, and other airport related services. The time of receipt will be determined by the City Clerk's time stamp. Proposals received after the time fixed for the receipt of the proposals shall not be considered. Proposals are required to be on the plan holder's list to be considered responsive. The City reserves the right to accept or reject any and all proposals and to waive irregularities or informalities in the proposals.

NOTICE OF SALE

Notice is hereby given that a **110-foot steel-hull dredge barge** located at Homer, Alaska, will be sold by the City of Homer to the highest bidder, as is, together with any equipment, gear, furniture, apparel, fixtures, tackle, boats, machinery, anchors, or other appurtenances. The Barge is located on the City of Homer title owned tidelands on the beach adjacent to the Northstar Terminal on the Homer Spit. There is no known license or registry number. The City Harbor impounded the Barge on November 4, 2011. The last known owner is Merrill McGahan, whose last known address is: P.O. Box 7005; Nikiski, Alaska 99635. A bidder's packet and bidding instructions are available at Homer City Hall, Office of the City Clerk, 491 E. Pioneer Avenue, Homer, AK 99603, phone (907) 235-3130. Sealed bids will be received at the office of the City Clerk until **3:00 p.m., Monday, June 11, 2012**, at which time they will be publicly opened and read.

Notice is hereby given that the **22' sailing vessel Water Rat**, AK 8194H, located on the uplands at the Homer Small Boat Harbor in Homer, Alaska, will be sold by the City of Homer to the highest bidder as is, together with its equipment, gear, furniture, apparel, fixtures, tackle, boats, machinery, anchors and all appurtenances. The Water Rat is a 22' fiberglass recreational sailboat with a 7 1/2' beam and a 4' draft. A bidder's packet and bidding instructions are available at Homer City Hall, Office of the City Clerk, 491 E. Pioneer Avenue, Homer, AK 99603, phone (907) 235-3130. Sealed bids will be received at the office of the City Clerk until **3:00 p.m., Monday, June 11, 2012**, at which time they will be publicly opened and read.

CITY ATTORNEY REPORT

COMMITTEE REPORT(S)

PENDING BUSINESS

**CITY OF HOMER
HOMER, ALASKA**

Lewis/Zak/Parks and Recreation
Advisory Commission

RESOLUTION 11-090

A RESOLUTION OF THE CITY COUNCIL OF HOMER,
ALASKA, SUPPORTING THE CONCEPT AND
CONSTRUCTION OF NON-MOTORIZED PATHWAYS TO
INCREASE THE SAFETY FOR MOTORIZED AND NON-
MOTORIZED USERS ALONG KACHEMAK DRIVE
LOCATED WITHIN THE CITY LIMITS, FROM THE BASE OF
THE HOMER SPIT TO EAST END ROAD.

1
2 WHEREAS, The Parks and Recreation Advisory Commission established a committee to
3 specifically address possible solutions to the hazards presented to non-motorized and motorized users
4 of Kachemak Drive; and
5

6 WHEREAS, Public input was sought through a variety of channels for solutions to address
7 these safety concerns; and recommendations to Lower the Speed Limit, Alter the Travel Lane
8 Width and Shoulder, Increase the Use of Signage, Construct Separated, Non-motorized Paths
9 paralleling Kachemak Drive using the existing Utility Easements will be contingent on available
10 funding in the future; and
11

12 WHEREAS, The Homer City Council has shown support in approval of the Homer Non-
13 Motorized Transportation and Trail Plan, Homer Area Transportation Plan, Climate Action Plan,
14 HART Policy Manual and inclusion of the Kachemak Drive Rehabilitation/Pathway on the Capital
15 Improvement Plan; and
16

17 WHEREAS, Increasing active transportation, motorized and non-motorized, offers the
18 potential for improved public health, economic development, a cleaner environment, reduced
19 transportation costs, enhanced community connections, social equity, and more livable
20 communities.
21

22 NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska,
23 hereby supports the concept and construction of non-motorized pathways along Kachemak Drive in,
24 over, and upon property within the City of Homer, and that said improvements are necessary for the
25 use and benefit of the public; and
26

27 BE IT FURTHER RESOLVED that the City Council of Homer, Alaska, further supports
28 the actions increasing the safety for motorized and non-motorized users along Kachemak Drive
29 in any or all of the following ways:

- 30 - Alteration of the existing Kachemak Drive and Shoulder
31 - Separated Paths paralleling Kachemak Drive using the Utility Easements
32 - Lowering the Speed Limit
33 - Increasing the Use of Signage
34

35 PASSED AND ADOPTED by the Homer City Council this 12th day of September, 2011.

36

37

CITY OF HOMER

38

39

40

41

JAMES C. HORNADAY, MAYOR

42 ATTEST:

43

44

45

JO JOHNSON, CMC, CITY CLERK

47

48 Fiscal information: Funding not defined.

49

1 CITY OF HOMER
2 HOMER, ALASKA

3 Zak/Lewis/Parks and
4 Recreation Advisory Commission

5 RESOLUTION 11-090(S)
6

7 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
8 ALASKA, SUPPORTING ~~THE CONCEPT AND~~
9 CONSTRUCTION OF A NON-MOTORIZED PATHWAYS TO
10 INCREASE ~~THE~~ SAFETY FOR MOTORIZED AND NON-
11 MOTORIZED USERS ALONG KACHEMAK DRIVE
12 LOCATED WITHIN THE HOMER CITY LIMITS, FROM THE
13 BASE OF THE HOMER SPIT TO EAST END ROAD.
14

15 WHEREAS, The Parks and Recreation Advisory Commission established a the
16 Kachemak Drive Path ~~Committee~~ to specifically address possible solutions to the hazards
17 presented to non-motorized and motorized users of Kachemak Drive; and
18

19 WHEREAS, The Kachemak Drive Path Committee received substantial public
20 input on safety concerns; and
21

22 WHEREAS, The Homer City Council has shown support for this non-motorized
23 pathway by in approval of the ~~Homer Non-Motorized Transportation and Trail Plan, Homer~~
24 ~~Area Transportation Plan, Climate Action Plan, HART Policy Manual~~ and inclusion of the
25 Kachemak Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the
26 Homer Non-Motorized Transportation and Trail Plan; the Homer Area Transportation
27 Plan; the Climate Action Plan; and the Homer Accelerated Roads and Trails (HART)
28 Policy Manual; and
29

30 ~~WHEREAS, Increasing active transportation, motorized and non-motorized~~
31 transportation, ~~offers the potential for improved~~ improves public health and safety,
32 encourages tourism, ~~economic development, a cleaner~~ cleans the environment, ~~reduced~~
33 transportation costs, and ~~enhanced~~ community connections, social equity, and ~~more livable~~
34 communities; and
35

36 WHEREAS, The City of Homer has available HART trail funds that can, including
37 other sources, form a basis for funding this project; and
38

39 WHEREAS, Utility easements can be employed with the permission of property
40 owners.
41

42 NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska,
43 hereby supports the ~~concept and~~ construction of the non-motorized pathways along Kachemak
44 Drive ~~in, over, and upon~~ property within the City of Homer, and that said ~~improvements are~~
45 necessary for the use will enhance safety and benefit of the public; and
46

47 BE IT FURTHER RESOLVED that the City Council of Homer, Alaska, further supports
48 the actions increasing the safety for motorized and non-motorized users along Kachemak Drive
49 in any or all of the following ways:

- 50 - ~~Alteration of the existing Kachemak Drive and Shoulder~~
- 51 - ~~Separated Paths paralleling Kachemak Drive using the Utility Easements~~
- 52 - ~~Lowering the Speed Limit~~
- 53 - ~~Increasing the Use of Signage~~

- 54
- 55 1. Increasing the usage of signage warning drivers of bicycles and pedestrians on
56 roadway.
 - 57 2. Building a separated path paralleling Kachemak Drive using utility easements and
58 public property.

59
60 PASSED AND ADOPTED by the Homer City Council this 23rd day of April, 2012.

61 CITY OF HOMER

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JAMES C. HORNADAY, MAYOR

ATTEST:

JO JOHNSON, CMC, CITY CLERK

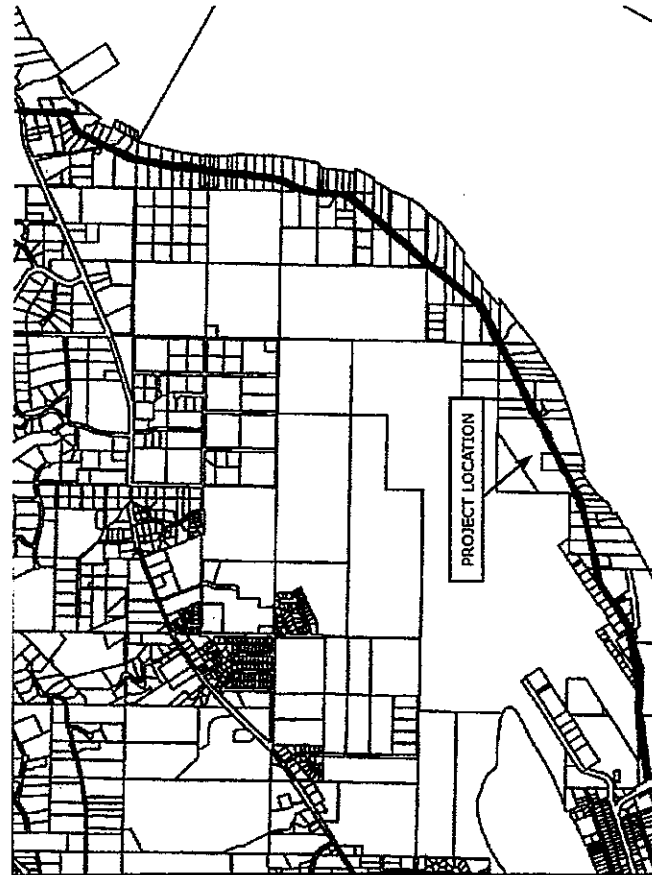
Fiscal Information: Funding not defined.

Kachemak Drive Path

Preliminary Engineering Packet

March 8, 2012

Mission Statement: To build a safe, separate non motorized trail along Kachemak Drive connecting East End Road to the Spit Road via the airport.



Contents / Index

Pg	Description
1	Cover Sheet, Vicinity Map, Index
2	General Notes
3	Typical Path Sections
4-7	Aerial Views of Path and Road

References:

- Homer Non Motorized Transportation & Trail Plan
 - City of Homer Capital Improvement Plan (CIP) 2012-2017
 - City of Homer Trail Manual Design Criteria
- References are available from the City Clerk, as hard copy or online at:
<http://www.cityofhomer-ak.gov/documentsandforms>

Kachemak Drive Path - Preliminary Engineering Packet

March 8, 2012

General Notes

PROJECT DESCRIPTION & BENEFIT: Kachemak Drive provides an alternate route for east-of-Homer traffic to the airport, Spit and harbor, and Ocean Drive commercial district (approximate daily traffic 1,500 vehicles). The road accesses the largest industrial marine storage repair and boat launch complex on the southern peninsula, passes residences, light commercial/industrial businesses, and moose wetlands. Rehabilitation needs have been identified for raising the embankment, surfacing, widening, and drainage improvements as a State project for the road.

Automobile and large truck traffic on Kachemak Drive has increased in recent years, with drivers showing a greater tendency to speed. These conditions make the road treacherous, at best, for bicycle and pedestrian traffic. Construction of a separated pathway along East End Road, as proposed, will increase recreational and commuter bicycle and pedestrian traffic on Kachemak Drive and will improve driver, bicycle, and pedestrian safety. Because of the significant right-of-way acquisition involved, the project to build a separated pathway along Kachemak Drive will take several years to complete. (from CIP, Pg 43)

The purpose of this document is to propose a general route and guidelines for construction details of the path so easements and detailed data can be collected.

All aerial photos in this package are from Google Earth and the Kenai Borough websites. They are not to scale and not current. The ongoing sewer and water improvements are not shown.

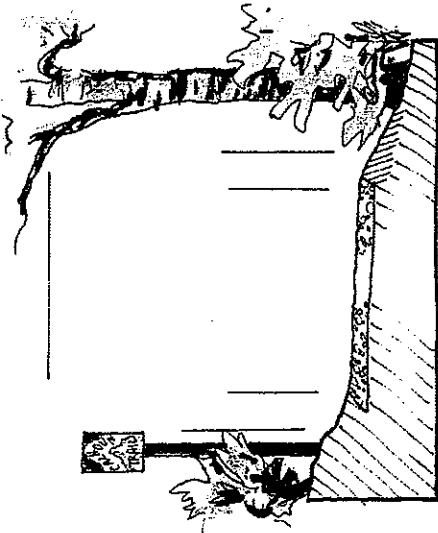
No detailed survey data has been taken for this project. Very rough stationing estimates have been created from available public information, Google Earth and Kenai Peninsula. Detailed survey information needs to be acquired.

The Kachemak Drive Path will attempt to follow existing water, sewer, and electric easements. The public access easements have not been acquired along these utility easements. A letter requesting public access easements from the City to landowners is needed.

Recommendation that the construction of the proposed path be done in a manner that can be upgraded to a higher level in the future.

TYPICAL SECTIONS

LVL3-Dry



LVL5-Ultimate

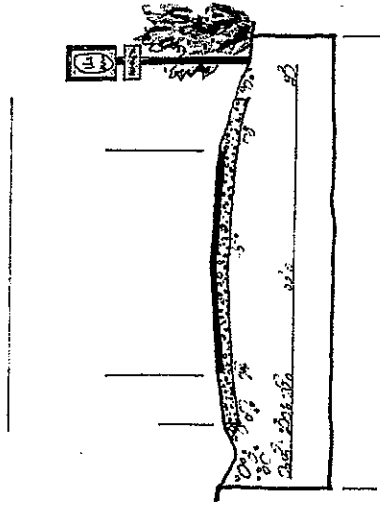


Figure D-15 Puncheon
over Wetland

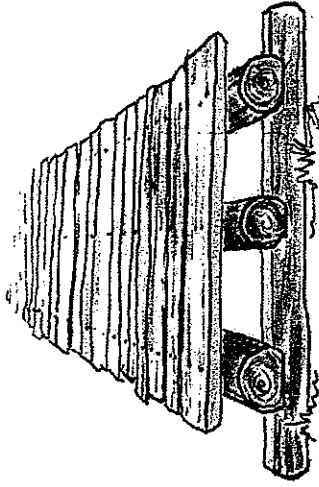
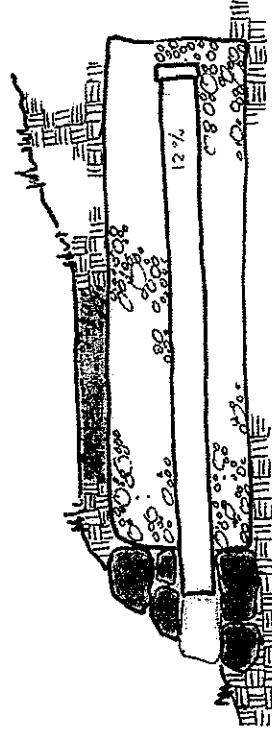


FIGURE D-10 STONE DIP WITH TURNPIKE LOGS

FIGURE D-11 UNDERDRAIN, OR FRENCH DRAIN



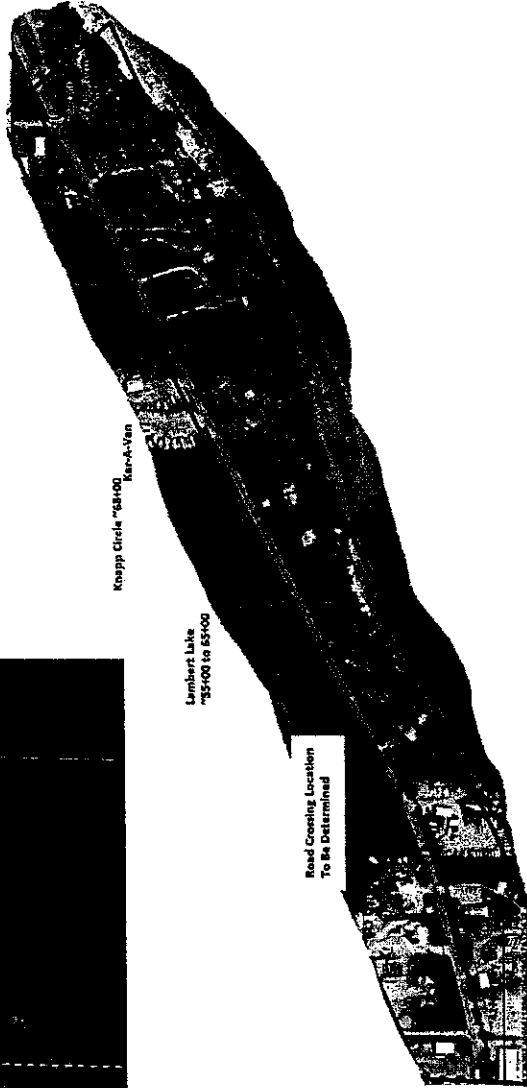
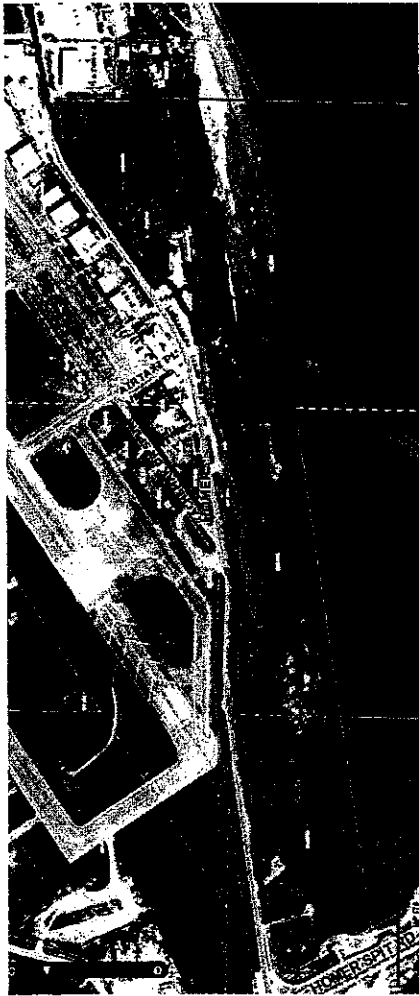
SOURCE OF (SOME) DRAWINGS: Wetland Trail Design and Construction, USDA Forest Service, 2007.
and CITY OF HOMER PUBLIC ACCESS EASEMENTS; AND TRAILS DESIGN CRITERIA MANUAL

There are several other suggested path types, including bridge sections over gullies, recycled City owned plastic sections near Islands and Oceans, other sections shown in the City of Homer Non-Motorized Transportation & Trail Plan (pgs 3, 17, 32, 33); the Trail Manual (pgs 27, 29, 31, 38, 39, 40, 44, 45, 47, and 49); and other public sources.

Stations Description Sheet 1

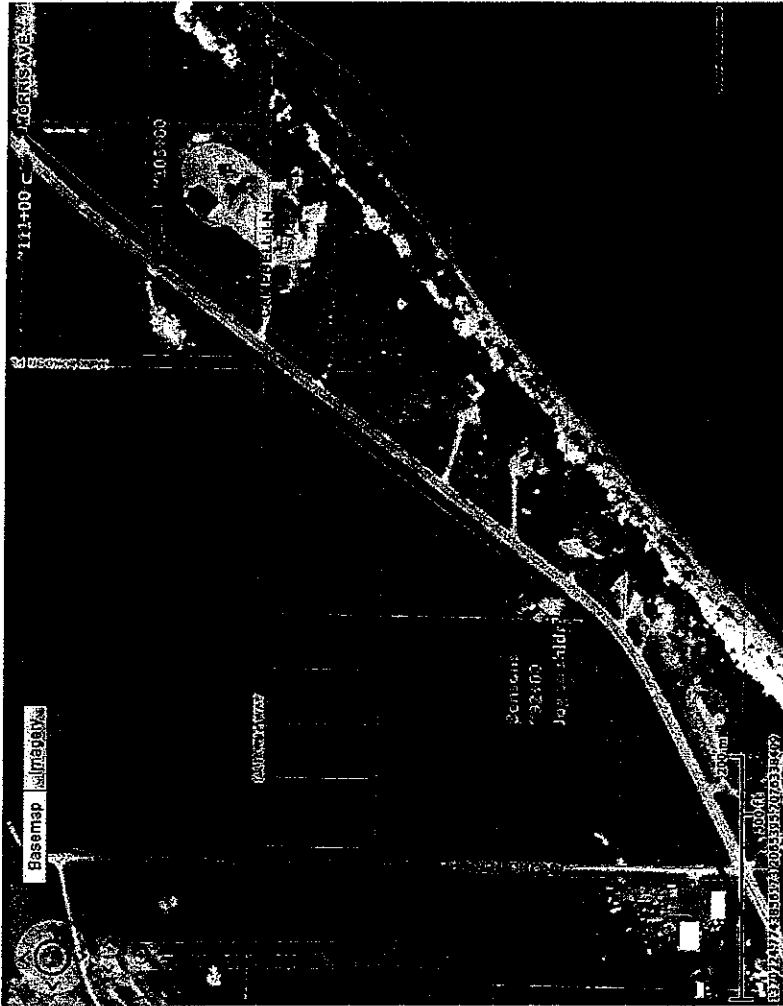
- 0+00 to ~5+00 Flat, continue existing Spit Path with separate trail from road
 - ~5+00 to ~11+00 Path is on or near toe of embankment, in and out of trees
 - ~11+00 to ~28+00 Trail follows beach to existing road that climbs to top of hill, on existing one lane road to beach from parking.
- Alternate trail would be a new route up the slope (Yellow line)
- ~17+00 to ~28+00 Path is in back of airport long term parking. Remove junk cars, need airport leasing approval.
 - ~28+00 to 41+00 Adjacent to, but separate from road to Bay Club

Sheet 2



- ~41+00 to ~49+00 Bay Club to AP Mgr or boatyard road crossing. Exact crossing location to be determined, check sight distances on road, utility obstacles on north side, driveways, and topography. Follow electric or sewer / water easement.
- ~49+00 to ~85+00 Road crossing to Arctic Tern. Follow electric easement. Damp ground by Lambert Lake.

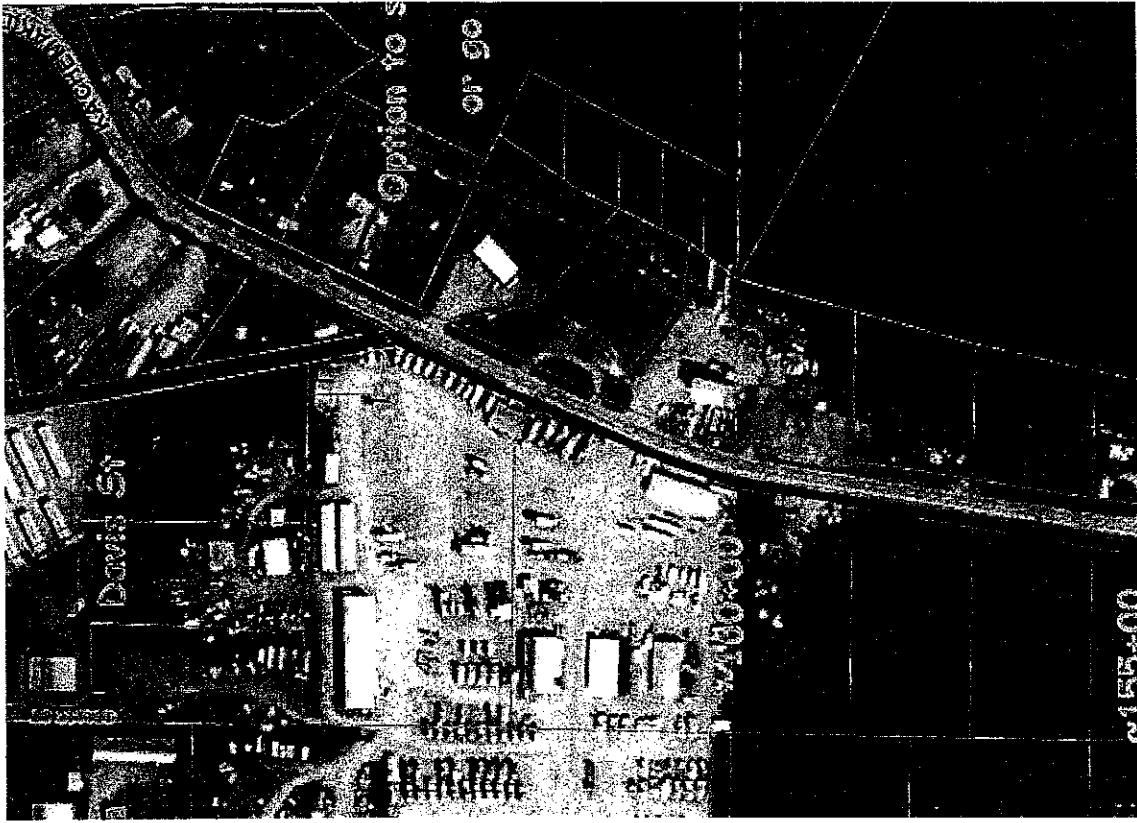
Sheet 3
~85+00 to ~111+00 Arctic Tern to Morris Ave (platted road only). Follow power line? May have to jog to road shoulder to get around private property at ~92+00.



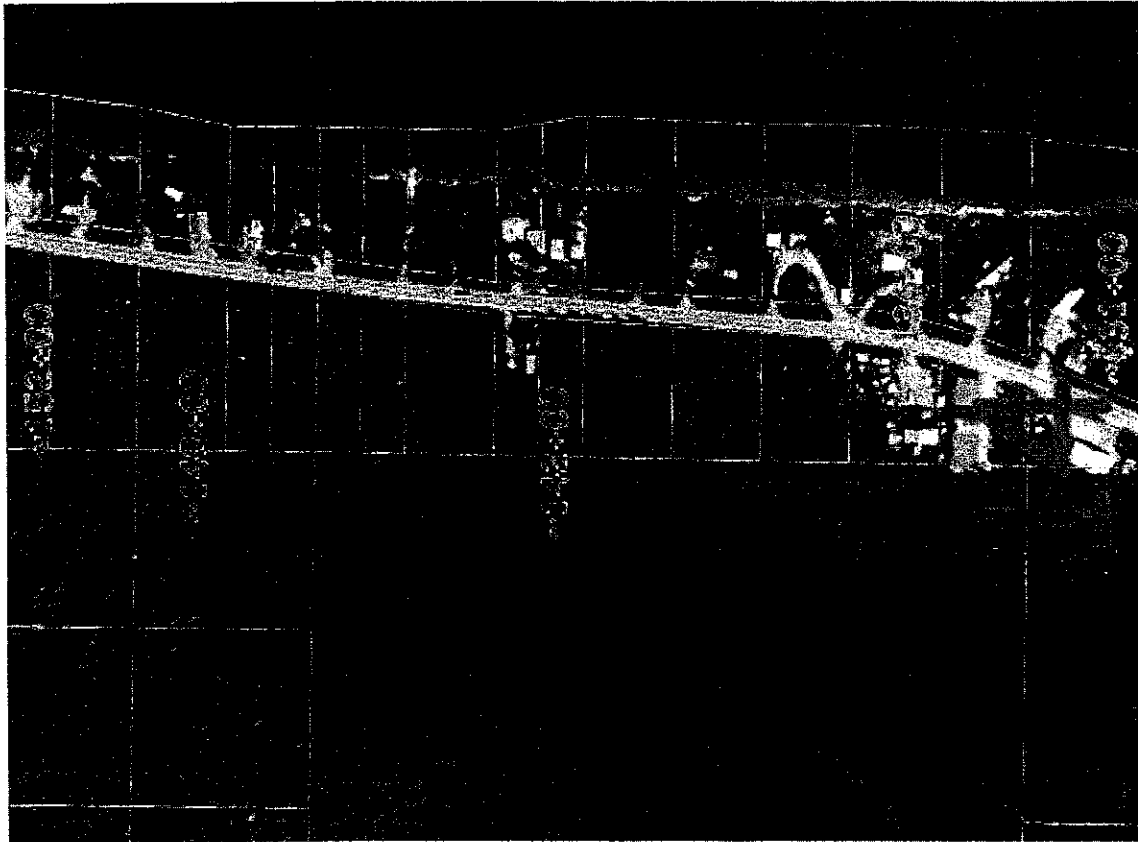
Sheet 4 ~111+00 to ~131+00 Morris Ave thru curves, ----->
follow new sewer line easement?

Sheet 6 ~152+00 to ~172+00

Follow new sewer line past the Northern Enterprises boatyard



Sheet 5 ~135+00 to ~152+00 Follow new sewer line easement?



Sheet 7 ~172+00 to E-End Rd The Davis St option would require about 900' of clearing and possible large culvert installation, then another ~300' to get to East End Road through a congested area between the Gear Shed, a coffee shop, and a bike shop, and a bike shop, all good terminations for the trail. The Kachemak Drive option would involve building the trail across several driveways, without substantial drainage issues.



Proposed Funding Plan for Kachemak Drive Path

Utilizing Section VII Trail Prioritizing Criteria and Planning Guidelines of the H.A.R.T Policy manual, the trail would be identified as a high priority trail during the annual review by TAC, (Transportation Advisory Committee) and Parks and Recreation Advisory Commission.

As of 2012 there is an ending balance of \$304,554 in the HART Trail Reserves. Approximately \$100,000 added annually plus investment interest of \$500 to \$1000 annually.

These funds could be utilized in amounts to be determined for initial surveying, engineering and design work, consultation fees, and basic materials to get started, i.e. trucking, geoblock trail hardening material, culverts, typar road fabric, treated wood 4x4's and decking rental of equipment.

By utilizing volunteers for basic construction and labor, donated materials, and rented equipment, costs would be much less than if done by a contractor.

Possible sources of funding:

Sale of City owned Lot on Kachemak Dr. , Parcel #17910001, assessed at \$38,100

Homer Foundation

Rasmussen Foundation

ATI, Alaska Trails Initiative Grant

Private donations

Fundraisers

STIP

Corporate Grants - such as R.E.I.

Other future sources of funding as discovered or available.

Maintenance Plan for the Proposed Kachemak Drive Path

Proposed trail-

An 8 foot wide, compacted gravel path from the end of the existing spit trail along Kachemak Drive to East End Road.

1. Annual spring workday by volunteers, litter pick up, minor tread repairs, drainage
2. Encourage individual / groups to utilize Adopt-a-Trail program for a portion or all of the trail.
3. Bi-Annual inspection by City Parks and Recreation Maintenance staff.
4. Use of city ATV and ATV grader to grade the trail twice a year or as needed
- city staff or designated volunteer.

Winter maintenance:

Plow for pedestrians and bikes using city atv with plow, city staff or volunteer
OR

Don't plow, pack and drag for multi-use, ski, snowshoe, bike, pedestrian.
Use snowmachine, roller, drag.

Sign maintenance:

Volunteer / user reporting system for maintenance needs

Adopt-a Trail volunteers

Bike Club

Ski Club

Individual volunteers

Funded by private donations

Grants

HART Funds

City Maintenance as per other city trails

References Cited - Proposed Kachemak Drive Pedestrian Bike Pathway

Preliminary Engineering and Path Design

City of Homer Capital Improvement Plan 2011-2016, Pages i, v, vi, 41, 43, A-11	
- Resolution 11-096(A) Page i	Page 10
- Introduction: The Capital Improvement Program, Page v, vi	Page 11
- State Projects, Page 41	Page 13
- Kachemak Drive Rehabilitation/Pathway, Page 43	Page 14
- City of Homer Financing Assumptions Capital Improvement Program, Page A-11	Page 15

City of Homer Non-Motorized Transportation & Trail Plan, Pages 3, 17, 18, 32, 33	
- Introduction, Page 3	Page 16
- Functional Aspects, Sidewalks, Walkways and Safe Crossings, Page 17	Page 17
- Figure A: Sidewalks, Walkways and Safe Crossings, Page 18	Page 18
- Functional Aspects, Connectivity and Safety: Bicycle Transportation, Page 32	Page 19
Figure 12: Bicycle and Pedestrian Infrastructure	
Functional Aspects, Connectivity and Safety: Bicycle Transportation, Page 33	
Bicycle Lane	Page 20

City of Homer Trail Manual Design Criteria - Non-Motorized Trails and Access Easements	
Pages 27, 29, 31, 38, 39, 40, 44, 45, 47, and 49	
- Level Three Semi-Improved Trail, Page 27	Page 21
- Level Four - Fully Improved Trail, Page 29	Page 22
- Level Five - High Use Trail, Page 31	Page 23
- D. Trail Design Criteria, Running Grade Criteria by Trail Level, Grade Reversals, Cross Slope & Cut/Fill, Page 38-39	Page 24
- D. Trail Design Criteria, Widths, Page 40	Page 26
- D. Trail Design Criteria, Structures, Page 44-45	Page 27
- D. Trail Design Criteria, Planks with Piles, Cribbing or Bents; Puncheons, Boardwalks Other Techniques; Materials, Page 47	Page 29
- D. Trail Design Criteria, Trail Heads & Parking; Amenities, Benches, Trash & Recycling Receptacles, Lighting, Information, and Bicycle Racks, Page 49	Page 30

References Cited - Proposed Kachemak Drive Pedestrian Bike Pathway

Proposed Funding for the Kachemak Drive Path

City of Homer Accelerated Roads and Trails Program Policy Manual
- Section VII, Trails Prioritizing Criteria and Planning Guidelines, Page 8 Page 31

Resolution 11-090(S)

Climate Action Plan, Page 33 & 34 Page 32

2005 Homer Area Transportation Plan, Pages I-3, I-16, I-17, I-18,
I-24 through I-33 Page 34

Homer Accelerated Roads and Trails Program and Policy (H.A.R.T.) Pages 2, 4, 5, 8 Page 48

CITY OF HOMER
HOMER, ALASKA

Mayor/City Council

RESOLUTION 11-096(A)

A RESOLUTION OF THE HOMER CITY COUNCIL ADOPTING THE 2012-2017 CAPITAL IMPROVEMENT PLAN AND ESTABLISHING CAPITAL PROJECT LEGISLATIVE PRIORITIES FOR FISCAL YEAR 2013.

WHEREAS, A duly published hearing was held on September 26, 2011 in order to obtain public comments on capital improvement projects and legislative priorities; and

WHEREAS, It is the intent of the City Council to provide the Governor, the State Legislature, State agencies, the Alaska Congressional Delegation, and other potential funding sources with adequate information regarding the City's capital project funding needs.

NOW, THEREFORE BE IT RESOLVED by the City Council of Homer, Alaska, that the "City of Homer Capital Improvement Plan 2012-2017" is hereby adopted as the official 6-year capital improvement plan for the City of Homer.

BE IT FURTHER RESOLVED that the following capital improvement projects are identified as priorities for the FY 2013 State Legislative Request:

1. Sewer Treatment Plant Bio-solids Treatment Improvements
2. Homer Area Natural Gas Pipeline, Phase 2
3. Harbor Improvement Revenue Bond Projects (Bundled Projects)
4. Skyline Fire Station
5. Fishing Lagoon Improvements
6. Karen Hornaday Park Improvements, Phase I
7. Tanker 2 Refurbishment and Fire Engine 4 Refurbishment HVFD
8. Homer High School Track Renovation
9. Alternative Water Source
10. Deep Water/Cruise Ship Dock Expansion, Phase I
11. Homer Intersection Improvements
12. Ocean Drive Reconstruction with Turn Lane
13. Mariner Park Restroom
14. Kachemak Drive Rehabilitation/Pathway
15. Truck Loading Facility Upgrades at Fish Dock

BE IT FURTHER RESOLVED that projects for the FY 2013 Federal Legislative Request will be selected from this list.

BE IT FINALLY RESOLVED that the City Manager is hereby instructed to advise appropriate State and Federal representatives and personnel of the City's FY 2013 capital project priorities and take appropriate steps to provide necessary background information.

PASSED AND ADOPTED by a duly constituted quorum of the City Council for the City of Homer on this 10th day of October, 2011.



ATTEST

 JO JOHNSON, CMC, CITY CLERK

CITY OF HOMER

 JAMES C. HORNADAY, MAYOR

Introduction: The Capital Improvement Program

A capital improvement plan (CIP) is a long-term guide for capital project expenditures. The CIP includes a list of capital projects a community envisions for the future, and a plan that integrates timing of expenditures with the City's annual budget. The program identifies ways projects will benefit the community. The CIP also indicates the priorities assigned to different projects and presents a target construction schedule.

A carefully prepared capital improvement plan has many uses. It can assist a community to:

- Anticipate community needs in advance, before needs become critical;
- Rank capital improvements needs so the most important projects are given consideration for funding before projects not as urgently needed;
- Plan for maintenance and operations costs so expenses are budgeted in advance and projects communities cannot afford to operate are avoided;
- Provide a written description and justification for projects submitted for state funding so the legislature, governor, and appropriate agencies have the information necessary to make decisions about funding capital projects; and
- Provide the basis for capital projects as part of the annual budget.

A capital improvement project is one that warrants special attention in the municipal budget. Normally, public funds are not expended if the project is not listed in the CIP. A capital expenditure should be a major, nonrecurring budget item that results in a fixed asset with an anticipated life of at least two years. Projects eligible for inclusion in the City of Homer CIP have a lower cost limit of \$50,000 for City projects and \$25,000 for those proposed by non-profit organizations. Projects proposed by non-profit organizations and other non-City groups may be included in the CIP with City Council approval but such inclusion does not indicate that the City intends to provide funding for the project.

The municipality's capital improvement plan is prepared in accordance with a planning schedule, usually adopted by City Council at the onset of the CIP process. A copy of the City of Homer CIP schedule appears in the appendix of this document.

The number of years over which capital projects are scheduled is called the capital programming period. The City of Homer's capital programming period coincides with the State's, which is a six year period. The CIP is updated annually, since only some of the projects are funded and completed each year.

A capital improvement plan is not complete without public input. The public should be involved throughout the CIP process, including nomination and adoption stages of the process. The City of Homer solicits input from City advisory bodies, advertises for public input during the CIP public hearing, and invites the public to participate throughout the entire process.

The City's capital improvement program integrates the City's annual budget with planning for larger projects that meet community goals. The CIP program involves a process where the City Council, with technical support from the administration and ideas and suggestions from the public, compiles a viable way to implement goals for the community.

Determining project priorities. City of Homer CIP projects are assigned a priority level of 1, 2, or 3, with 1 being the highest priority. To determine priority, the Council considers such questions as:

- Will the project correct a problem that poses a clear danger to human health and safety?
- Will the project significantly enhance City revenues or prevent significant financial loss?
- Is the project widely supported within the community?
- Has the project already been partially funded?
- Is it likely that the project will be funded only if it is identified as being of highest priority?
- Has the project been in the CIP for a long time?
- Is the project specifically recommended in other City of Homer long-range plans?
- Is the project strongly supported by one or more City advisory bodies?

Once the overall CIP list is finalized, the City Council names a subset of projects that will be the focus of efforts to obtain state and/or federal funding in the coming year. The overall CIP and the legislative priority list are approved by resolution.

Integration of the CIP with Comprehensive Plan Goals

Each project listed in the CIP document has been evaluated for consistency with the City's goals as outlined in the Comprehensive Plan. The following goals were taken into account in project evaluation:

- Land Use:** Guide the amount and location of Homer's growth to increase the supply and diversity of housing, protect important environmental resources and community character, reduce sprawl by encouraging infill, make efficient use of infrastructure, support a healthy local economy, and help reduce global impacts including limiting greenhouse gas emissions.
- Transportation:** Address future transportation needs while considering land use, economics, and aesthetics, and increasing community connectivity for vehicles, pedestrians, and cyclists.
- Public Service & Facilities:** Provide public services and facilities that meet current needs while planning for the future. Develop strategies to work with community partners that provide beneficial community services outside of the scope of City government.
- Parks, Recreation & Culture:** Encourage a wide range of health-promoting recreation services and facilities, provide ready access to open space, parks, and recreation, and take pride in supporting the arts.
- Economic Vitality:** Promote strength and continued growth of Homer's economic industries including marine trades, commercial fishing, tourism, education, arts, and culture. Preserve quality of life while supporting the creation of more year-round living wage jobs.
- Energy:** Promote energy conservation, wise use of environmental resources, and development of renewable energy through the actions of local government as well as the private sector.
- Homer Spit:** Manage the land and other resources of the Spit to accommodate its natural processes, while allowing fishing, tourism, other marine-related development, and open space/recreational uses.
- Town Center:** Create a community focal point to provide for business development, instill a greater sense of pride in the downtown area, enhance mobility for all forms of transportation, and contribute to a higher quality of life.

State Projects

The City of Homer supports the following state projects which, if completed, will bring significant benefits to Homer residents:

Transportation projects within city limits:

Homer Intersection Improvements
Kachemak Drive Rehabilitation/Pathway
Main Street Reconstruction/Intersection
Ocean Drive Reconstruction with Turn Lane
Pioneer Avenue Upgrade

Transportation projects outside city limits:

Sterling Highway Reconstruction, Anchor Point to Baycrest Hill
Sterling Highway Realignment, MP 150-157

Non-transportation projects:

Alaska Maritime Academy

See following pages for project descriptions.

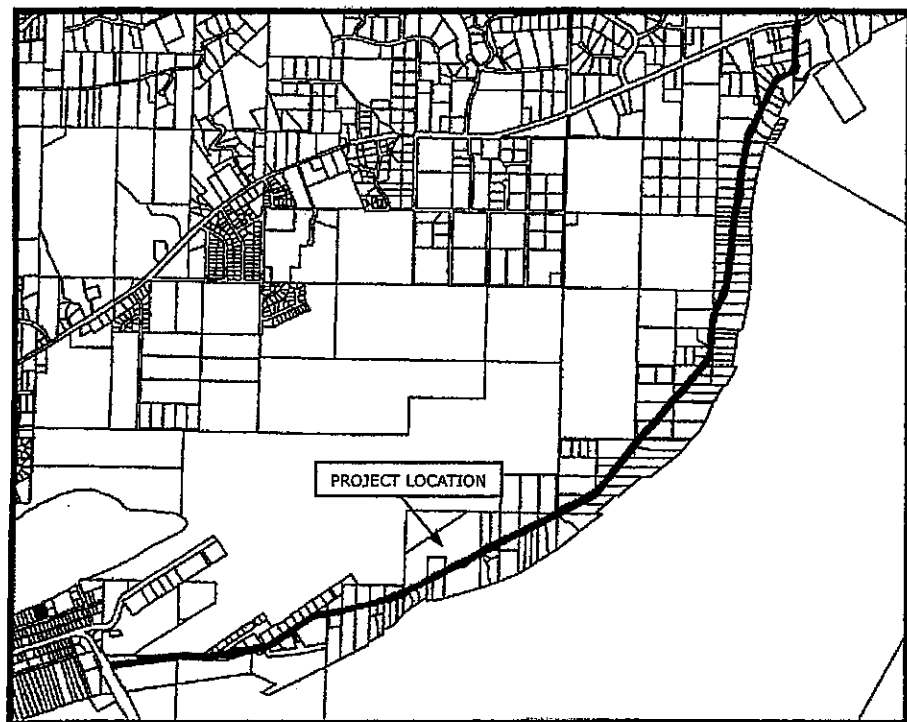


City of Homer Capital Improvement Plan • 2011 - 2016

Kachemak Drive Rehabilitation/Pathway

PROJECT DESCRIPTION & BENEFIT: Kachemak Drive provides an alternate route for east-of-Homer traffic to the airport, Spit and harbor, and Ocean Drive commercial district (approximate daily traffic 1,500 vehicles). The road accesses the largest industrial marine storage repair and boat launch complex on the southern peninsula, passes residences, light commercial/industrial businesses, and moose wetlands. Rehabilitation needs have been identified for raising the embankment, surfacing, widening, and drainage improvements.

Automobile and large truck traffic on Kachemak Drive has increased in recent years, with drivers showing a greater tendency to speed. These conditions make the road treacherous, at best, for bicycle and pedestrian traffic. Construction of a separated pathway along East End Road, as proposed, will increase recreational and commuter bicycle and pedestrian traffic on Kachemak Drive and will improve driver, bicycle, and pedestrian safety. Because of the significant right-of-way acquisition involved, the project to build a separated pathway along Kachemak Drive will take several years to complete.



**CITY OF HOMER FINANCING ASSUMPTIONS
CAPITAL IMPROVEMENT PROGRAM**

Implementation of the City of Homer Capital Improvement Plan requires utilization of various financing mechanisms. Financing mechanisms available to the City of Homer include:

- Federal grants or loans
- State grants or loans
- General obligation bonds
- Limited obligation bonds
- Revenue bonds
- Special assessment bonds
- Bank loans
- Pay-as-you-go
- Private sector development agreements
- Property owner contributions
- Lease or lease-purchase agreements

The use of any of the financing mechanisms listed above must be based upon the financial capability of the City as well as the specific capital improvement project. In this regard, financing the CIP should take into consideration the following assumptions:

1. The six-mill property tax limitation precludes utilizing General Fund operating revenue to fund major capital improvements. Available revenue should be utilized to fund operation and maintenance activities.
2. The operating revenue of enterprise funds (Port & Harbor, Water & Sewer) will be limited and as such, currently only fund operation and maintenance activities.
3. The utilization of Federal and State grants will continue to be significant funding mechanisms. Grants will be pursued whenever possible.
4. The 1½ percent sales tax approved by voters of Homer for debt service and CIP projects is dedicated at ¾ percent to sewer treatment plant debt retirement with the remaining balance to be used in water and sewer system improvement projects, and ¾ percent to the Homer Accelerated Roads and Trails (HART) Program.
5. The HART Program will require property owner contribution of \$30 per front foot for road reconstruction, with an additional \$17 per front foot for paving.
6. The Accelerated Water and Sewer Program will require substantial property owner contributions through improvement districts/assessment funding, set currently at 75 percent.
7. The private sector will be encouraged to finance, construct, and operate certain non-essential capital improvements (e.g., overslope development).
8. The utilization of bonds will be determined on a project-by-project basis.
9. The lease and/or lease-purchase of capital improvements will be determined on a project-by-project basis.

Introduction

Homer is a community whose charm, outdoor culture, and beautiful natural surroundings draw hundreds of thousands of visitors each year. It is known for its world-class halibut fishing, thriving arts community, adventurous outdoor activities, numerous festivals, and the Homer Spit, a 4.5-mile narrow slip of land that curls outward into Kachemak Bay. This small coastal hamlet has captured the hearts of almost everyone who lives or visits here and the imaginations of many who hear of this place "at the end of the road." Despite this success, Homer faces economic development challenges, especially in its central business district. Most of the community's visitors spend their time on the Homer Spit where one can book fishing tours and kayak adventures, shop along the boardwalk, stroll the beaches, visit the harbor, and dine in a number of restaurants, all without needing a car.

In an effort to promote economic development throughout the community, the City of Homer applied for and received a grant from the Alaska Department of Economic and Community Development to create a comprehensive non-motorized transportation and trail plan. Developing and implementing such a plan is essential for enhancing the community's image as a wonderful place to live, visit, and engage in business. By establishing a truly superb trails network that enables visitors and residents alike to travel safely and comfortably through Homer without the need for an automobile, the community will capitalize on its outdoor culture and unmatched natural setting.

...a development manual to be consulted, referred-to, dog-eared, rained-on, coffee-stained, finger-print smeared, and otherwise continuously used...

Beluga Shelgin Boardwalk

3.0

Functional Aspects
Sidewalks, Walkways and Safe Crossings

To complete and connect the community's pedestrian system, walkways (referred to as separated pathways in the bicycle transportation section of this report) should be added to the following locations:

- Beluga Lake Trail System (connecting Ben Walkers Park to Paul Banks Elementary and beyond to East End Road)
- Chamber of Commerce Trail (connecting the Homer Chamber of Commerce with the Islands and Ocean Visitor Center)
- Homer Coastal Trail
- Homer Public Library Trail (between library and Poopdeck Trail)

- Kachemak Drive Pathway (connecting Fairview Avenue to Soundview Avenue Trail)
- W.R. Bell Trail (connecting Fairview Avenue to Reber Road)
- Smoky Bay Way (connecting Wright Street to Waddell Way)

"Walking revitalizes me. After one day on the trail I become different from the way I am at home. I am in touch with the seasons, the weather, the varied hours of each day. I see more keenly. I am aware of the details."
-Marlyn Doan, 'Hiking Light', 1982

A map of these routes appears in Figure A.

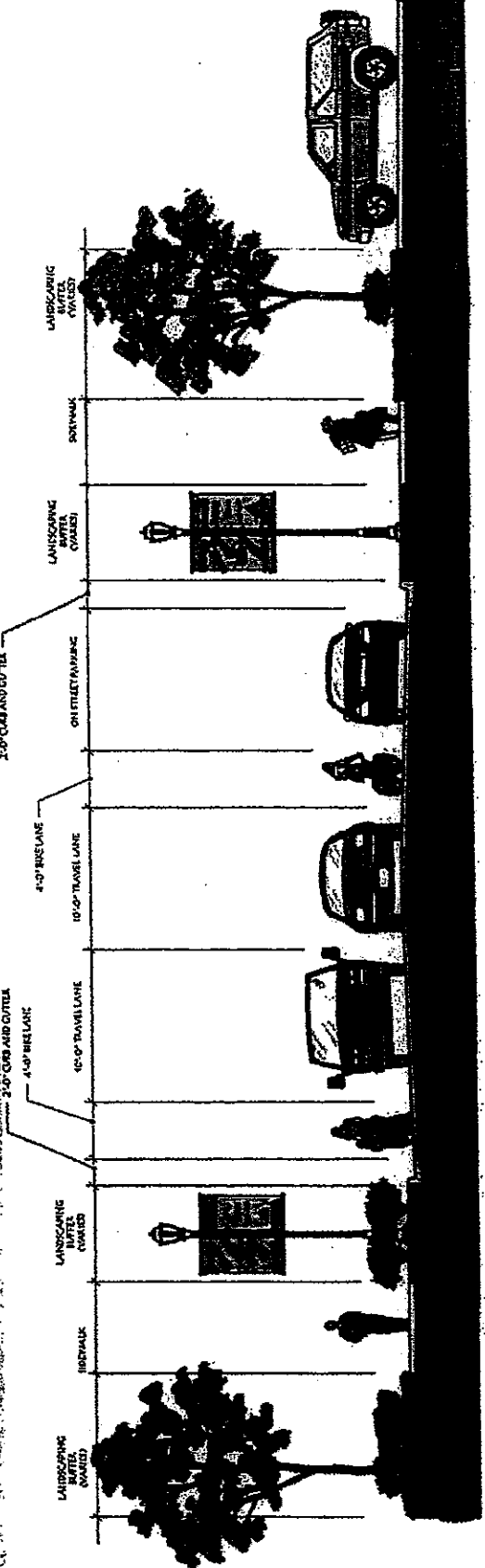


Figure 7: Pioneer Avenue

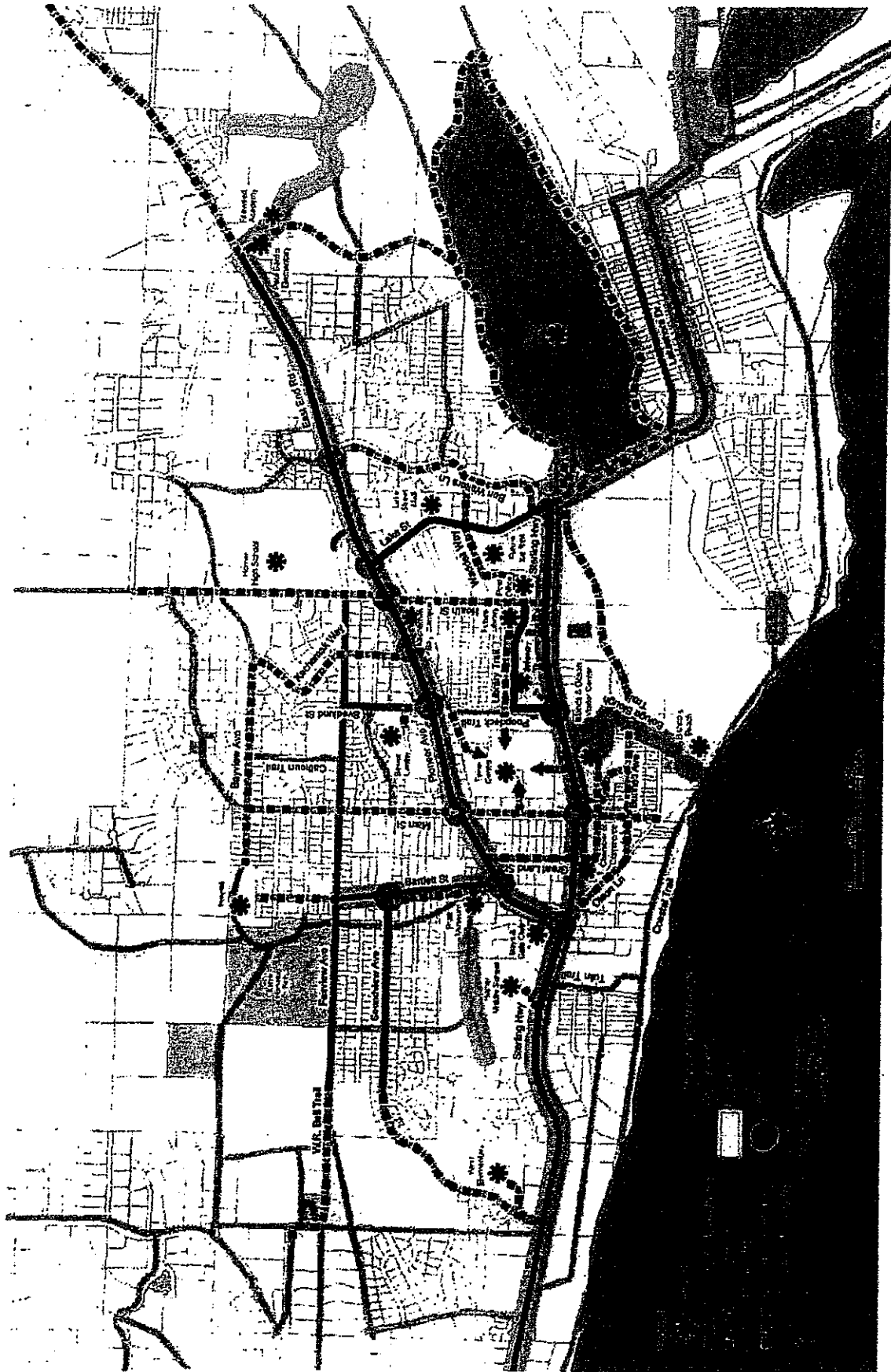


Figure A: Sidewalks, Walkways and Safe Crossings

3.3

Functional Aspects
Connectivity and Safety: Bicycle Transportation

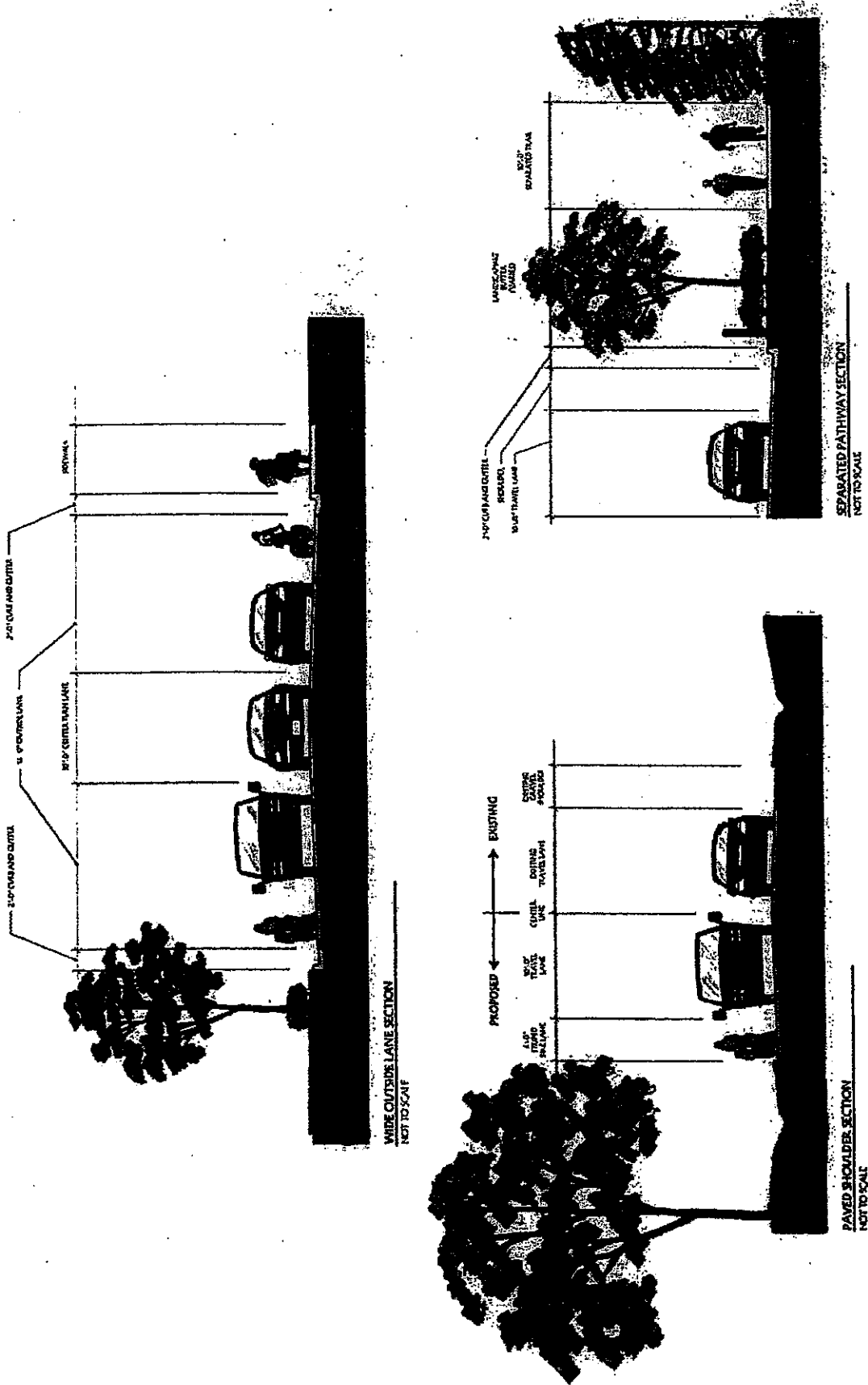


Figure 12: Bicycle and Pedestrian Infrastructure

Bicycle Lane

Bicycle lanes are designated portions of paved roadways intended for bicycle traffic. In Homer this type of facility is appropriate for arterial and collector roads and in locations where bicycle traffic is significant. Bicycle lanes are one-way only and travel with the flow of traffic. Streets that include bicycle lanes should have smooth pavement, a schedule of regular maintenance to repair potholes and remove debris, and drainage inlets that are flush with the pavement surface and have bicycle-safe grates designed to prevent trapped bicycle tires. Bicycle lanes are created by striping and stenciling pavement with lines and symbols. They are inexpensive to install on existing roads. Possible locations for bicycle lanes include the following routes:

- Pioneer Avenue
- Main Street
- Sterling Highway
- (between Pioneer Avenue and Lake Street)
- East End Road
- (between Lake Street and East Hill Road)
- Kaenemak Drive
- (until a separated pathway is constructed)
- Burnell Avenue
- (between Main Street and Beluga Place)
- Beluga Place

A map of Homer's existing and proposed bicycle lanes appears in Figure B.

Wide Outside Lane

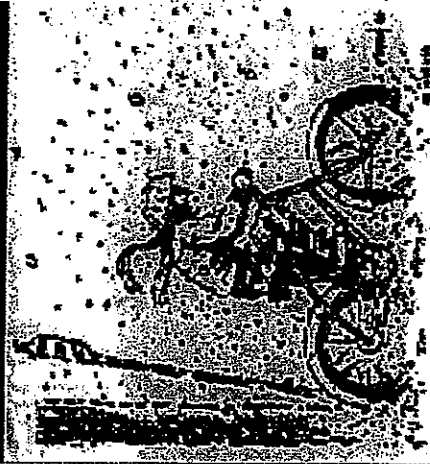
A wide outside lane is not differentiated from the vehicle travel way with striping. It is a paved travel lane that is wide enough to comfortably accommodate both bicycle and vehicular traffic. A wide outside lane measures 14 feet unless there is on-street parking or a steep grade in which case a width of 15 feet is more appropriate. A lane less than 14 feet does not provide comfortable maneuvering room for both motorized and non-motorized vehicles while a wider lane may lead motorists to believe that there is more than one lane available for travel. Wide outside lanes are appropriate on arterials, roads with high traffic volumes and speeds, roads without shoulders, and in restrictive urban environments such as busy commercial districts. In Homer, possible locations for wide outside lanes include the following routes:

- Sterling Highway
- (between Pioneer Avenue and West Hill Road)
- Soundview Avenue
- Fairview Avenue
- Bayview Avenue
- Lake Street
- Heath Street
- Barlett Street

A map of these routes appears in Figure B.

"Toleration is the greatest gift of the mind; it requires the same effort of the brain that it takes to balance oneself on a bicycle."

-Helen Keller, deaf and blind US lecturer, 1880-1968



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City of Homer
Non-Motorized Trail Planning & Design Criteria Summary
Level 3 Semi-Improved Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

TRAIL DESIGN CRITERIA

Trail Width & Shoulders 3 - 5 foot wide improved trail.

- 3 - 4 foot wide trail - for routes with lower volumes of traffic, and one-way or no bicycle use.
- 5 foot wide trail - for routes with moderate to high pedestrian volumes and/or two-way bicycle or equestrian uses.
- Trails should widen in areas of switchbacks, turns, steep side slopes, and as needed near structures or amenities.

Surface 4 inches NFS gravel over geotextile fabric, which may be placed over native vegetation. Alternate surfacing: porous pavement panels filled with native or imported material. Medium duty boardwalk or bridges where needed. Generally clear, with protrusions <4 inches and steps to 10 inches.

Clearance

- Vertical clearance - 8 feet minimum. Optimum 12 feet for winter and equestrian users.
- Horizontal clearance - 12 in. beyond trail edge. 24 in. from signs, trees or structures.

Grade

- Target grade < 8%, with grade reversals as needed to control erosion.
- 15% maximum for up to 50 feet.

Cross Slope of Trail

- Target cross slope - 3%, flowing to downside of tread, or to uphill side, if a drainage ditch is provided.
- Maximum - 10%

Signage

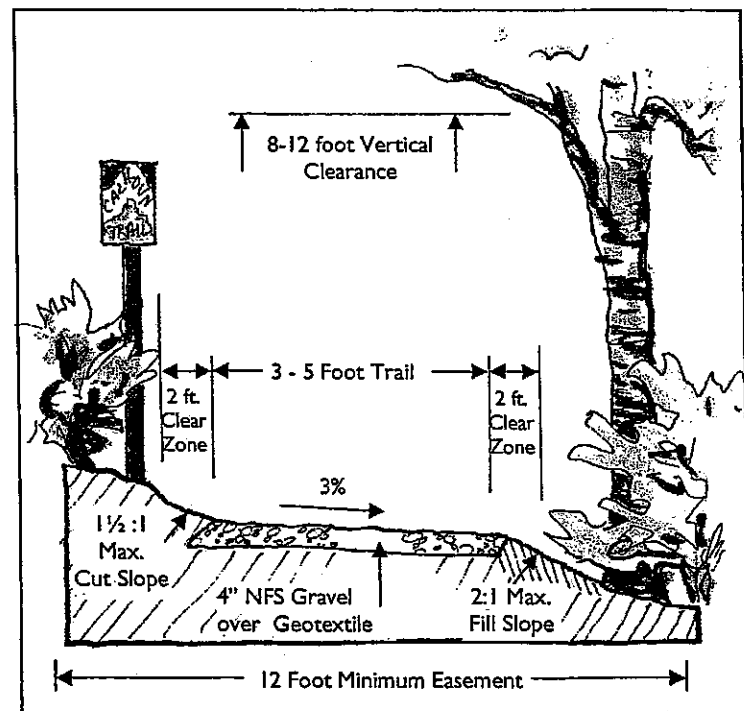
- Trail markers (as needed) to navigate winter use trails.
- Trail information signage posted at each end of the trail: Trail system map (if appropriate), trail name, length, use restrictions or accessibility warnings, and resource protection information.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Few amenities, as approved by City of Homer, such as bear proof trash receptacles, trail heads, benches for rest or viewing, interpretive signs, such as at interesting historic or natural features.

Structures

- Medium duty structures, as needed.
- Elevated plank crossing of wetlands, creeks.
- Few railings or boardwalks.
- Log, timber or rock retaining structures for cut / fill edges, as needed.



CROSS SECTION - LEVEL 3 SEMI-IMPROVED TRAIL

City of Homer
Non-Motorized Trail Planning & Design Criteria Summary
Level 4 - Fully Improved Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

DESIGN CRITERIA

Trail Width & Shoulders 5 - 8 foot wide paved or gravel trail.

- 5 - 6 foot wide trail - for routes with lower volumes of traffic, and fewer recreational users.
- 7 - 8 foot wide trail - for routes with bicycles and/or moderate to high user volumes.
- PAVED TRAILS - where a Level 5 trail is recommended, but topography or other physical conditions prevent construction to Level 5 standards, a paved Level 4 trail is acceptable. Any Level 4 trail can be paved. Provide a minimum 12 in. gravel shoulders on all paved trails.

Surface Firm and stable. Smooth, few or no obstacles. Protrusions <3 in. Steps to 8 in. Remove surface vegetation and organic soils. For gravel trails: 2 in. leveling course over 8 in. NFS gravel over geotextile. For paved trails: 2 in. AC pavement over 2 in. leveling course over 24 in. NFS gravel over geotextile. Alternate surfacing: PPP filled with native or imported material.

Clearance

- Vertical clearance - 9 feet above trail and shoulders, 12 feet for equestrian use.
- Horizontal clearance - Minimum 12 in. beyond trail edge. 24 in. from signs and trees.

Grade & Accessibility

- Accessible trails: Target grade $\leq 5\%$, 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.
- Maximum: 10% for up to 50 feet.
- Stairs used where absolutely necessary and pedestrians are the primary user group.

Cross Slope of Trail

- Gravel trails - 3%
- Paved trails - 2%
- Shoulders - 10% Max.

Signage

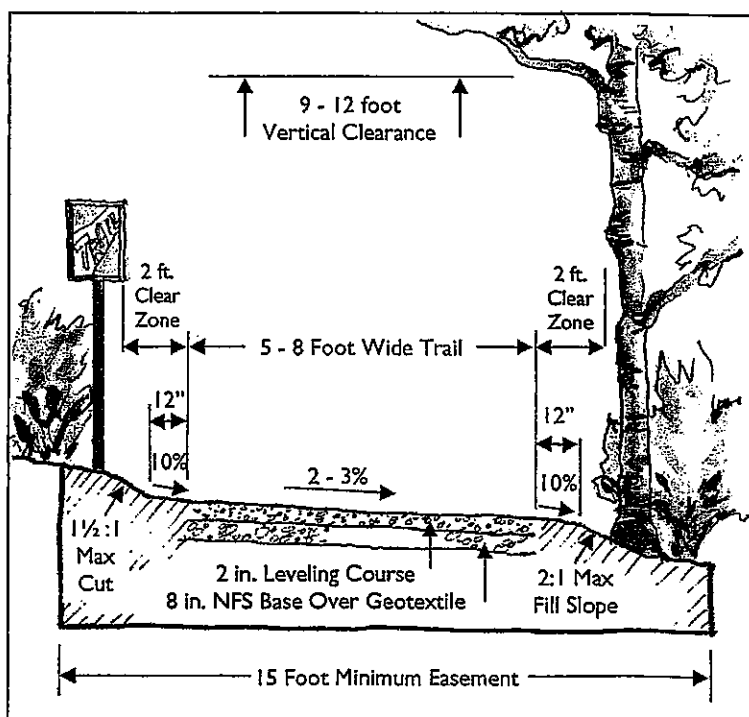
- Trail information signage posted at ends and intersections, as necessary, such as a trail system map, trail name, use restrictions, accessibility warnings, and resource protection information.
- Directional signs for nearby destinations, traffic control and warnings for intersections or other trail conditions.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Amenities common. Lighting, bear proof trash & recycling receptacles, maps, benches for rests or viewing, and interpretive signs, as approved.

Structures

- Heavy duty structures, as needed: bridges, boardwalks, retaining structures, railings.



CROSS SECTION - LEVEL 4 FULLY IMPROVED TRAIL

City of Homer
Non-Motorized Trail Planning & Design Criteria Summary
Level 5 - High Use Trail

NOTE: This is a summary. Refer to Article 5.13 Non-Motorized Trails and Public Access Easements for full description of criteria.

TRAIL DESIGN CRITERIA

Trail Width & Shoulders. 8- 12 foot wide paved trail with 2 foot wide gravel shoulders.

- 8 foot Trail - for routes with lower volumes of traffic, few recreational users, or space limitations.
- 10 foot wide trail sections are the standard.
- 12 foot wide trails are recommended where traffic volumes are high, bicycles and in-line skates are common, near intersections with other trails or streets, as the trail approaches a bridge, where grades exceed 5% and handrails are provided, or near points of interest along the trail.
- **ALTERNATE TRAIL DESIGN** - Where trail is highly recreational, with bicycles, equestrians, joggers, an alternative design of 6 foot wide paved trail with 4 foot shoulders on each side or a trail with one 2 foot and one 6 foot wide shoulder is allowable. Or, provide a separated dual trail, one paved, one gravel, with a vegetated median in-between.

Surface. Uniform, firm and stable. Pavement or boardwalk. Smooth, no obstacles. Protrusions <2 inches. Construct using 2 in. AC pavement over 2 in. leveling course over 24 in. NFS gravel over geotextile fabric.

Clearance.

- Vertical clearance - 9 feet above trail and shoulders, 12 feet for equestrian use.
- Horizontal clearance - Minimum 24 inches beyond trail edge. 36 inches for posts and structures.

Grade

- Accessible Trails: Target grade $\leq 5\%$, 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.

Cross Slope of Trail

- Target cross slope - 2% Shoulders - 10% Max.
- Maximum, where needed for driveway crossings or other intersections - 3%

Signage

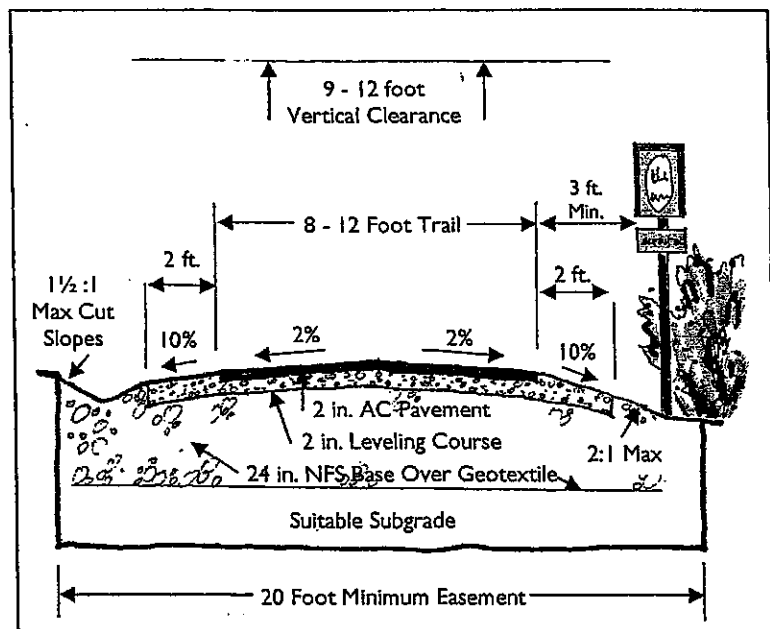
- Trail information signage posted at ends and intersections, as necessary: Trail system map (if appropriate), trail name, use restrictions or accessibility warnings, and resource protection information.
- Directional signs for nearby destinations, traffic control and warnings for intersections or other trail conditions.
- Directional signage with trail name and length, at all trail intersections.

Amenities

- Amenities common. Lighting, bear proof trash & recycling receptacles, maps, benches for rests or viewing, and interpretive signs, such as at historic or natural features.

Structures

- Heavy duty structures, as needed: bridges, boardwalks, retaining structures, railings.



CROSS SECTION - LEVEL 5 HIGH USE TRAIL

D. TRAIL DESIGN CRITERIA

- Compliance would substantially alter the nature of the setting or the purpose of the facility, or portion of the facility.
- Compliance would require construction methods or materials that are prohibited by Federal, State, or Local Regulations or Statutes.
- Compliance would not be feasible due to terrain or the prevailing construction practices.

v) Running Grade Criteria by Trail Level.

LEVEL 1: Maximum grade is based primarily on the ability of the trail to resist erosion caused by trail use, surface water, or wet soils. Target grade <12%. Maximum 20% for trails where underlying soils are sand, silt, or clay. 20%- 30% for gravel or rock base. For grades over 30%, natural trail base and surface must be composed of angular rock, large rock or solid rock. Provide grade reversals every 20-50 feet. Construct steps to minimize erosion.

LEVEL 2: Target grade: <10%. Maximum: 20% for distances up to 50 feet. Use on-site cut and fill to soften dips or peaks in trail corridor.

LEVEL 3: Target grade: < 8%. Maximum: 15% for up to 50 feet.

LEVEL 4/5: Target grade: ≤ 5%. Maximum: 8.33% for up to 200 feet, 10% for up to 30 feet, 12.5% for up to 10 feet. No more than 30% of trail length shall exceed 8.33%.

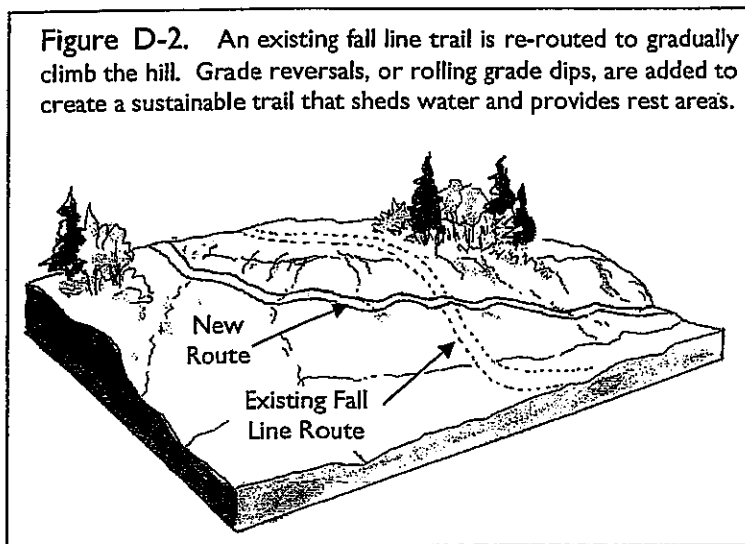
b. Grade Reversals

A grade reversal is a change in the direction of running grade, from an upslope grade to a down slope grade. They are used on unpaved trails to prevent erosion that is caused by water running *along* the surface of a trail versus *across* the trail. They should be provided every 20-50 feet along the trail corridor.

c. Cross-Slope & Cut / Fill

All trails require enough cross-slope to shed water off the trail surface, but not so much that it impacts the comfort or safety for the trail user. Managing surface water drainage along a trail corridor is critical to maintaining a safe and long lasting trail. Poorly managed drainage can erode soils and destroy vegetation. Keeping water moving across the surface of a trail will prevent ponding, erosion, and icing.

Steep side slopes (> 30%) are a common obstacle to the construction of trails on Homer's hillside terrain, and often trigger the need for extensive cut and fill to "fit" a trail into a hillside. Careful planning can minimize expense and environmental damage.



D. TRAIL DESIGN CRITERIA

i) General Cross-Slope and Cut / Fill Criteria:

- All construction-related disturbance, including areas of cut or fill, shall occur within the limits of the easement;
- Limits of cut and fill should be in proportion to the construction level of the trail. For example: low level trails justify very little cut / fill, high level trails may utilize the entire easement for most of the length of the trail;
- Maximum 1½ :1 (75%) cut slopes, maximum 2:1 (50%) fill slopes. Where soils are unstable, sandy, or saturated, 3:1 (33%) max slopes are recommended.
- For trails along side slopes of 30% or greater, construct the trail on the cut bench portion only. Avoid locating the trail on fill portions of the side slope;
- Provide retaining structures, as needed to minimize disturbance and to improve accessibility on Level 3, 4 or 5 trails;
- Construct trails to ensure water flows across or under the trail surface, not along the trail. Where it is necessary to run the water along the trail, it should be contained in a ditch with provisions made to protect against erosion. Ditch length should be minimized by diverting runoff across the trail at the nearest point feasible.
- To accommodate vision-impaired or wheelchair users on Level 4 or 5 trails with an adjacent fill slope, provide a vertical barrier along the cut slope edge of the shoulder, such as vegetation, or a minimum 3 in. curb or barrier.

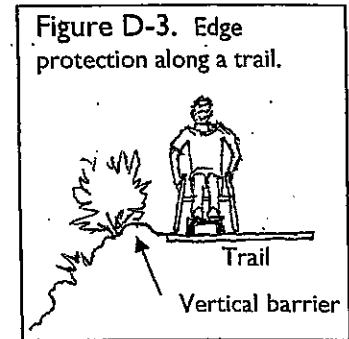


Figure D-3. Edge protection along a trail.

ii) Criteria by Trail Level

LEVEL 1: Target cross slope is 3-10%. Maximum is up to the natural side slope. If the trail is designed for mountain bikes, cross slope maximum is 10%. Very minimal cut and fill. Little or no use of (rustic) retaining methods.

LEVEL 2: Target cross slope: 5%. Maximum: 10%. For ski trails, if bicycles are not allowed, steeper side slopes may be allowed. Minimal cut and fill as necessary to meet criteria and soften dips, ruts, bumps or peaks.

LEVEL 3: Target cross slope is 3%. Maximum is 10%. Cut and fill as needed to meet design criteria. Rock or timber used for most retaining needs.

LEVEL 4: Gravel trails: Target cross slope: 3%, Max.: 4%. Paved trails: target cross slope: 2%, Max.: 3%. Cut and fill may be significant, as needed to meet design criteria. May likely extend to edges of easement for much of the trail length. Imported materials for retaining structures common.

LEVEL 5: Target cross-slope is 2%. Where necessary, such as when crossing driveways, a cross-slope of 3% is allowable. Paved surfaces must be uniform enough to prevent ponding and icing. Shoulders should slope away from the paved sections of the trail with a target slope of 3%, and a maximum of 10%. Cut and fill may extend to the outer edges of the easement. Retaining structures common.

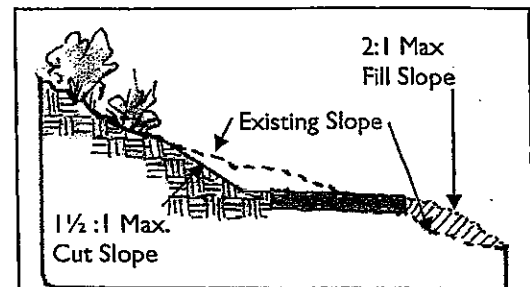


Figure D-4. A Full Bench Trail, placed on cut portion of the slope only, provides a more stable base than a trail placed on fill material.

iii) Re-vegetation. All cut / fill slopes should be vegetated with native species. Attempts should be made to salvage and stockpile existing vegetation for re-use on cut / fill slopes. Avoid reseeding with non-native species.

D. TRAIL DESIGN CRITERIA

4. WIDTHS

The complete trail cross-section is composed of the easement, the trail surface, the shoulders, and the clearance zone. The desired width is primarily related to the volume and mix of users. Secondary considerations include topography, curves, intersections, structures, and amenities.

Table D-5

REQUIRED EASEMENT WIDTHS	
LEVEL 1:	8 Feet
LEVEL 2:	20 Feet
LEVEL 3:	12 Feet
LEVEL 4:	15 Feet
LEVEL 5:	20 Feet

a. Easement Width

The following criteria apply to easement widths:

- A narrower portion of easement may be allowed when available space is limited by existing structures or property boundaries, for a short duration of the trail, and the narrow segment of the trail does not create a safety hazard or an uncomfortable trail segment of trail;
- Vary the easement width as needed to accommodate switchbacks or turns;
- Wider easement sections are allowed where existing side slopes require additional cut and fill, and retaining structures are not feasible, and the widened area is not extensive.

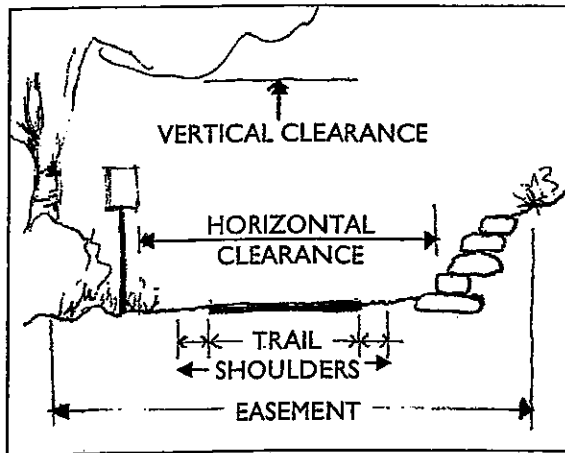
b. Trail Width

The width of the trail surface, or tread, is determined by the volume and type of users, as well as the nature of the terrain and the trail surface. Always provide for the user with the most demanding needs.

LEVEL 1: Trail tread width may range from 6 - 24 inches. Consistent width along the length is preferred, but not required on this level of trail. Natural obstacles and topography may both affect variability of the tread width. Provide 24 in. width when the trail is expected to attract mountain biking, equestrians, snow-shoeing, or skiing.

LEVEL 2: There is typically not a constructed trail tread for recreation corridors. They are a specified width of area that is cleared of woody vegetation and obstacles, mowed (optional), and identified with trail markers for use as a recreation corridor. Minimum width for an un-programmed low use corridor is 6 feet. Groomed ski trail routes require up to a 16 foot wide mowed corridor.

FIGURE D-5 Trail Profile



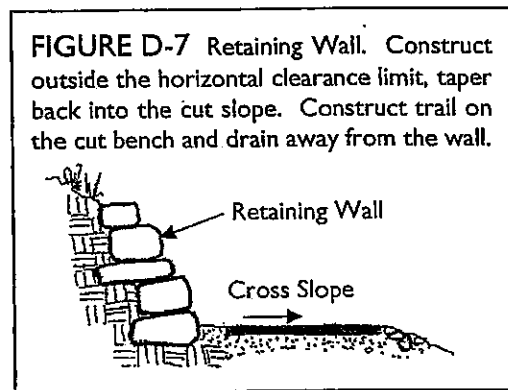
LEVEL 3: Widths may range from 3-5 feet. Safety may be a concern on narrow trails with a mix of pedestrians, bicycles and equestrians, even if the volumes are low. It cannot be expected that bicycles will use these routes as "one-way" trails, or stay off them altogether, so it is imperative that they be designed to mitigate potential hazards. For trails that will expect regular use by bicycles, overall use volumes are moderate, or hills are frequent, the width should be 5 feet. Narrower trails are allowed for lower use trails, but horizontal clearance and sight stopping distance should both be increased, curves widened, and passing areas provided at a minimum of every 1000 feet.

D. TRAIL DESIGN CRITERIA

LEVEL 5: Base: Remove vegetation and organic soils. 24 in. NFS gravel over geotextile over suitable soils. Surface: 2 in. AC pavement over 2 in. leveling course. For bridges and wet crossings: wood, synthetic, recycled plastic, treated wood, or metal.

6. STRUCTURES

Where trails cross creeks or traverse areas where existing grades or side slopes are too steep to construct the trail without excessive disruption to adjacent areas, structures may be necessary.



a. Retaining Walls

Construct all retaining walls outside the horizontal clearance limit of the trail. Retaining walls higher than 24 in. on the down slope side of a trail are discouraged. Where necessary, they should include a railing, for safety. Retaining wall materials vary depending on the level of the trail, with rock, concrete block, or timbers used on higher level trails and on-site materials, such as logs or rocks used on lower level trails. Where seeps occur behind retaining walls, provide method to ensure drainage through and under the wall.

b. Steps or Stairs

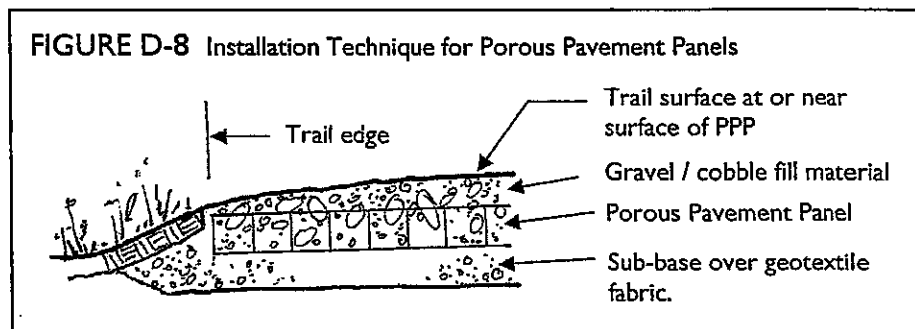
Steps and stairs are obstacles to many trail users, and are to be avoided, where possible. As needed, construct steps on Level 1 trails using on site materials, such as rocks. Only when all other options, including ramps, have been ruled out, are stairs allowed on Level 3, 4 or 5 trails. When stairs are necessary, consider providing long perron style steps, as strollers and wheelchairs can maneuver them easier.

c. Ramps

Along required ADA accessible pedestrian routes, sections of trail greater than 5% may be considered ramps, and are allowed for limited lengths (see section 3. GRADE & CROSS SLOPE).

d. PPP (Porous Pavement Panels)

These are three dimensional structural grids designed to provide a durable wear surface and load distribution system in wetland and other degradable soils



D. TRAIL DESIGN CRITERIA

e. Bridges

Bridges should be designed for pedestrian live loads and for maintenance or emergency vehicles if they may be expected to cross the bridge. Bridge decking should be designed with bicycle safe expansion joints or planks laid perpendicular to the trail direction unless bicycles are not allowed or not expected. Bridge widths should be the same as that of the approach trail plus 2 feet clear area on each side. Bridge decking should be flush with the approaching trail surface.

f. Railings

Railings are provided for safety on elevated trail segments, such as bridges. All railings should be engineered to withstand all loads that may be expected to occur on the bridge. The type of railing that is required is determined by the accessibility level of the trail, and fall into three basic types:

i) **Urban Setting.** Railings in highly pedestrian urban settings must meet International Building Code (IBC) requirements. Railings must be at least 42 inches high with vertical rails to prevent climbing, and be spaced to not allow a 4-inch sphere to pass through. Railings are required on ADA accessible ramps.

ii) **Rural Bridges.** Handrails on bridges or crossings, that are elevated at 30 inches or more, on accessible trails, such as Level 4 & 5 trails, need to meet AASHTO standards for pedestrian highway bridges. These standards require a 6-inch sphere must not pass through the railing in the bottom 27 inches, and an 8-inch sphere must not pass through the area higher than 27 inches. It also requires that the top railing is at least 42 inches for bicycles use, and 54 inches high for equestrian traffic. Rails should also be horizontal to prevent wheels and other objects from catching. All accessible trail bridges that do not have a rail system must have a minimum 3 inch high curb.

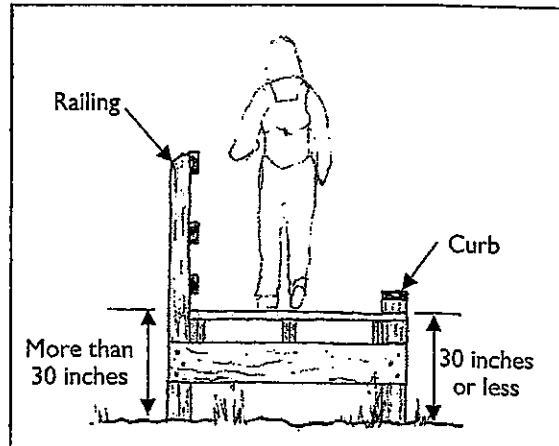
iii) **Remote Bridges.** For bridges in remote areas with a drop of 30 in. or more, railing requirements must meet OSHA standards. For typical crossings along Level 1, 2 & 3 trails, handrails are required to be at least 42 inches high for pedestrian traffic and 54 inches high for bicycle and equestrian traffic. They must include an intermediate rail so that vertical distances between rails do not exceed 15 inches between 2x4 wood rails or 19 inches between steel rails.

iv) **Railing Exceptions.** Not all trail bridges require railings. An analysis should be completed to identify and evaluate the bridge's potential users and the hazards of not having a rail system, including situations where a railing is provided on only one side. As a general rule, a remote trail or bridge with a drop of 8 feet or more, should have a pedestrian railing system.



FIGURE D-9 Bridge, railing and typical warning sign on a Level 5 Trail (Urban setting).

FIGURE D-10



D. TRAIL DESIGN CRITERIA

iv) **Planks with Piles, Cribbing or Bents.** An elevated trail technique where one or more tread planks are laid parallel to the trail corridor, attached to piles, cribbing, or bents. Choice of support method depends on type of wetland, range of water depth, user volumes, size of trail. Piles are not recommended on low level trails, due to the depth needed to prevent frost heaving.

v) **Puncheons.** A crossing technique for low water areas that utilizes sleepers. Some have linear planks, others also have stringers to support perpendicular decking, which is necessary for bicycle travel.

vi) **Boardwalks.** These are the most substantially constructed form of elevated crossings. They use piles, diagonal bracing, stringers, and planking laid perpendicular to the direction of travel. They often include curbed edges or railings, and can be constructed to suit many user groups, including bicycles and wheelchairs.

vii) **Other Techniques.** Avoid using ditches, culverts or other channelization techniques to divert water, as they may create issues with landslides and super-saturation of soils. Corduroy, turnpikes and causeways are all variations of at-grade wetland crossings, each with their pros and cons. Use of these may be appropriate in some situations, but they are typically not the most environmentally friendly.

c. Materials

Choose materials that are long-lasting and environmentally safe. More investment is expected on higher level trails.

FIGURE D-12 Log Cribbing with Two Sleepers

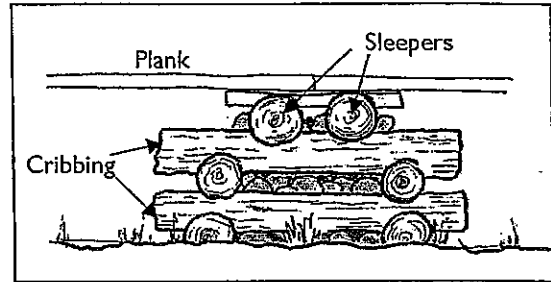


FIGURE D-13 Bog Bridge with Sleepers, or Single Plank Boardwalk

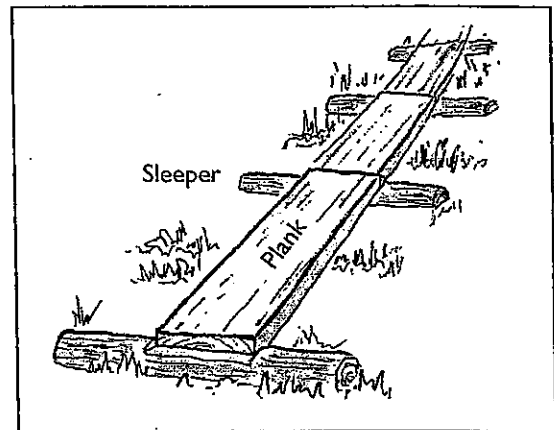


FIGURE D-15 Puncheon

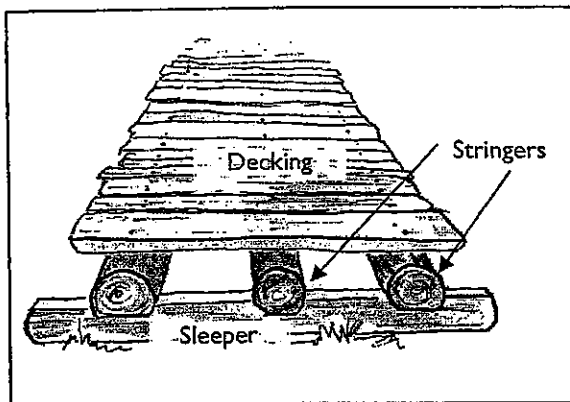
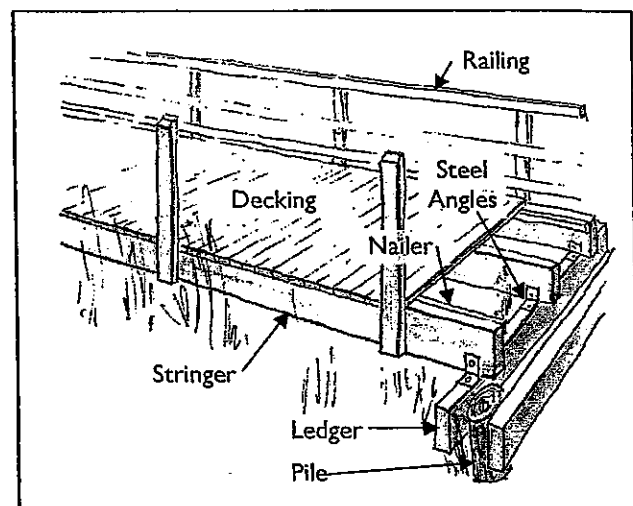


FIGURE D-14 Boardwalk



SOURCE OF DRAWINGS: Wetland Trail Design and Construction, USDA Forest Service, 2007.

D. TRAIL DESIGN CRITERIA

d. Trail Heads & Parking

Provide adequate parking, signage and staging areas as needed to accommodate various recreational activities on trails. Amenities such as maps, educational information, trash receptacles, seating, and other trail information are all possible features found at trail heads. Place trail heads and parking areas at the most logical locations along the trail, typically at ends.

9. AMENITIES

Trails are expected to serve many purposes including transportation, recreation, education and social interaction. Amenities, such as benches, trash receptacles, lighting, interpretive panels, and structures are appropriate and necessary for a trail network that meets these objectives. Generally, the higher level trails require more amenities. All amenities should be located outside the trail's clear zone. All amenities provided on accessible trails must also be accessible.

a. Benches

Benches are integral to recreation facilities, and can be used to provide seating for resting, socializing, or viewing. They should be provided at crests of hills, at midpoints of long inclines, in conjunction with other trail amenities, near recreation areas such as playgrounds, and at overlooks or viewpoints along a trail. All benches should meet ATBCB Guidelines for Recreation Facilities.

b. Trash & Recycling Receptacles

Provide bear proof facilities for trash and recycling along higher level trails in locations such as trail heads, rest areas, & interpretive facilities. Locate these facilities for easy maintenance.

c. Lighting

Lighting provides safety and comfort on trails used for transportation, which is primarily Level 4 and Level 5 trails. Where ambient lighting from nearby areas is not adequate to light the trail, additional pedestrian scale lighting may be advisable on these trails, especially at intersections.

d. Information

Trail maps, interpretive information is useful and appropriate in many circumstances along trails, such as to provide information on nearby historic, cultural or natural features. Such amenities enhance the user experience and also protect those community assets. Provide a minimum 4 feet clearance between informational amenities, such as interpretive signs and kiosks, and the edge of the trail.

e. Bicycle Racks

Provide bicycle racks at trail heads, parking areas, and other destinations along the trail corridor. Provide a minimum 4 feet clearance between bicycle racks and the trail.

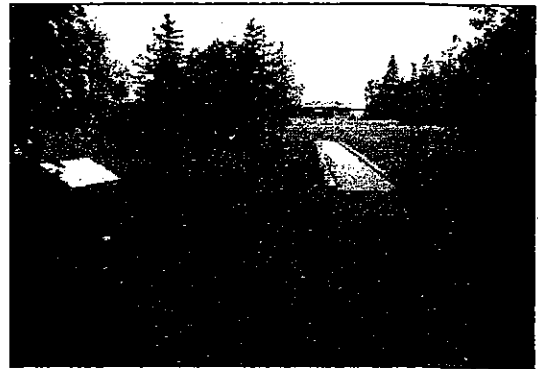


FIGURE D-18 Trail widens to accommodate interpretive signage.

developer shall construct the streets on the alignment adopted in the Master Roads and Streets Plan, and conforming to the respective classification. The developer shall be required to construct the street to a twenty-eight-foot width in accordance with the minimum requirements of a local residential street; provided, however, that the City may, upon direction of the City Council, elect to require construction to the full standards and pay to the developer the cost difference between the required street and the proposed street. (Resolution 07-82)

VII. TRAIL PRIORITIZING CRITERIA AND PLANNING GUIDELINES

A. Trail Prioritizing. The TAC and Parks and Recreation Advisory Commission will review the trail priority list during the annual review of the HART. The list will be presented in a memorandum from staff, and will contain a mix of large and small projects. Generally it will include up to five trail projects that staff has reviewed and found ready for preliminary work. Trails on this list are planned for construction in the near term (one to three year timeframe). Staff will actively work to prepare those projects for construction. (Resolution 07-82)

B. Trail Planning Guidelines

Trail design shall take into account at minimum the following:

1. Use context sensitive design when locating and planning trails to take advantage of scenic resources.
2. Respect the character of trails based on function, setting, and expectation of accessibility.
3. Evaluate the soils, drainage, wetlands, Tsunami zone, flood plain, stream setbacks, historical resources, visual resources, topography, existing and potential land use, zoning and land ownership.
4. Where estimated costs, operating costs and outside funding availability are considerations and important criteria, care should be used to ensure that important trails are not eliminated solely using cost as a determinant.
5. Multi-use trails are encouraged. Design of the trail should include consideration of compatible uses such as pedestrians and bicycles.
6. All trails should be designed to recognize the requirements of ADA standards and guidelines. (Resolution 07-82)

LAND USE/PLANNING & ZONING

Through land use planning, including transportation planning, local government has enormous influence over community energy use and greenhouse gas emissions. Development that reduces the need to drive and encourages non-motorized and public transportation will significantly reduce the use of fossil fuels.

Many of these same strategies (often referred to as “smart growth”) help reduce costs in providing services. Denser “cluster development,” for example, reduces taxpayer expenditures for water and sewer lines, road construction and road maintenance, and street lights. These reductions are tied directly to reductions in energy use and greenhouse gas emissions. Another benefit of more compact development is that less soil is disturbed, which helps prevent the release of CO₂ stored in the soil.

In September 2007, the Urban Land Institute and the National Center for Smart Growth Research and Education at the University of Maryland published *Growing Cooler: The Evidence on Urban Development and Climate Change*. After reviewing dozens of empirical studies, the authors predict that if sprawling development continues to fuel growth in driving, the increase in total miles driven will overwhelm expected gains from vehicle efficiency and low-carbon fuels. Lead author Reid Ewing stated, “The research shows that one of the best ways to reduce vehicle travel is to build places where people can accomplish more with less driving.”

Depending on several factors, from mix of land uses to pedestrian-friendly design, compact development reduces driving from 20 to 40 percent, and more in some instances. Typically, Americans living in compact urban neighborhoods where cars are not the only transportation option drive a third fewer miles than those in automobile-oriented suburbs, the researchers found.

At the same time, the book documents market research showing a shift in future housing demand to smaller homes and lots, townhouses, and condominiums in neighborhoods where jobs and activities are close at hand. Homer planners and developers would be wise to recognize the benefits of such development and encourage these trends.

The City of Homer will develop and implement land use plans that explicitly recognize the urgent need to reduce greenhouse gas emissions.

1. Support current (or stronger) language in the draft Homer Comprehensive Plan Update that calls for denser, more compact development and increased emphasis on developing infrastructure for non-motorized transportation.
2. Update City planning and zoning regulations to promote land use strategies that include compact, mixed-use development, higher density development, and infill.

“Planners in local government...are planning things that will be here in 50 and 100 years. Every single thing should be demonstrating sustainability. Every single one should have significant reductions in greenhouse gases, particularly in transportation, built in. If not, planners will be seen as absolute pariahs by their children and grandchildren. They’ll say ‘How could you have done that when everyone knew?’”

—Urban planner and author
Peter Newman

“Compact development provides an insurance policy against the worst effects of climate change and oil price spikes. In the worst case, current or future residents of compact development will have a variety of viable transportation options, while the residents of sprawl will not.”

—from Growing Cooler: The Evidence on Urban Development and Climate Change



Compact mixed-use development that includes sidewalks and trails creates a pleasant environment and encourages residents and visitors to walk instead of drive. (Photo from “Growing Cooler: The Evidence on Urban Development and Climate Change”)

3. Implement the City of Homer Non-Motorized Transportation and Trails Plan, including construction of specific trails, sidewalks, and safe crossings recommended in the plan, and revisions to Homer City Code, Title 21, to require non-motorized circulation systems.
4. Institute traffic calming measures and “complete street” designs to make bicycling and walking safer and more pleasant. (“Complete streets” are those which are designed to encourage and safely accommodate pedestrians, bicyclists, and transit users as well as automobiles. A number of cities and states have passed complete-street legislation.)
5. Develop Homer’s “Town Center” in line with the guiding principles in the Homer Town Center Development Plan and use this as a model for future development as Homer grows.

The City of Homer will make use of the permitting process to encourage development that helps reduce greenhouse gas emissions.

1. Provide assistance to developers and builders in evaluating plans to increase energy efficiency and promote non-motorized transportation.
2. Adopt building codes and incentives to increase energy efficiency in all new residential and commercial development.
3. Keep abreast of new LEED (Leadership in Energy and Environmental Design) standards for neighborhood development and building remodeling and consider adopting these standards in the permitting process.

1 INTRODUCTION

The Homer Area 2001 Transportation Plan was produced to be a comprehensive transportation-planning tool for the City of Homer. Additionally, the Non-Motorized Trails and Transportation Plan should be considered a companion document to the Transportation Plan, as it is the City's policy document for comprehensive long-range non-motorized transportation and trails system. This study will provide a general guide for planning and funding requests for road and trail transportation capital improvement projects for the next 20 years. This plan is not intended to identify site-specific locations of improvement.

The City of Homer (COH) obtained funding for this study from the Alaska Department of Transportation and Public Facilities (DOT/PF), and has retained Mike Taurainen, P.E., Consulting Engineers, Inc. as the prime consultant to perform this study. Other members of the consultant planning team include Land Design North (trails element), Kinney Engineering (transportation engineering), Bechtol Planning & Development (planning), and Brooks & Associates (transit engineering).

The 2001 Draft Transportation Plan was completed but not adopted. In Fall 2003 the City of Homer Road Standards Committee began reviewing the goals, objectives, and recommendations of the 2001 Draft Transportation Plan. The Homer Advisory Planning Commission took up discussion of the Draft Transportation Plan following the Roads Standards Committee. This plan reflects the plan as recommended by the Road Standards Committee.

The Road Standard Committee and the Homer Advisory Planning Commission further recommend that where ever this plan recommends signalization that alternatives, such as roundabouts, be seriously considered.

3 TRANSPORTATION FUNDING

3.1 Local Funding

The City of Homer has created the Homer Accelerated Roads Program (HARP); the intent of which is to reconstruct local substandard city streets and/or upgrade existing city streets and rights-of-way. Improvements funded under this program reduce maintenance costs, improve access, increase property values and improve the quality of life. The program utilizes dedicated City sales tax not to exceed three quarters of one percent supplemented by assessments against benefited properties. The program provides for the sale of revenue bonds to cover the cost of the projects. Abutting property owners generally share the cost of upgrading a street to residential standards by paying \$30/front foot for gravel improvements and \$17/front foot for paving. There has been some discussion about revising the program to more closely meet the current needs of the community.

3.2 State of Alaska

The State of Alaska Department of Transportation and Public Facilities publishes a State Transportation Improvement Program (STIP) on a three-year cycle. For roads and highways, the STIP funds projects under 4 categories:

- National Highway System, NHS (Includes Sterling Highway);
- State Highway System, SHS (established for this current cycle, no SHS roads in Homer);
- Community Transportation and Economic Development Program, CTP (any street or road nominated by local and borough governments); and,
- Trails and Recreational Access for Alaska, TRAAK (Trails, enhancements).

The 1998 to 2000 STIP and the 2001 to 2003 STIP has identified projects for the Homer area. Figure I-4 shows approximate total Statewide STIP projects, and those within Homer as listed in the 1998-2000 and 2001 to 2002 STIPs.

Figure I-4: Statewide STIP and Funding Dedicated to Homer Area

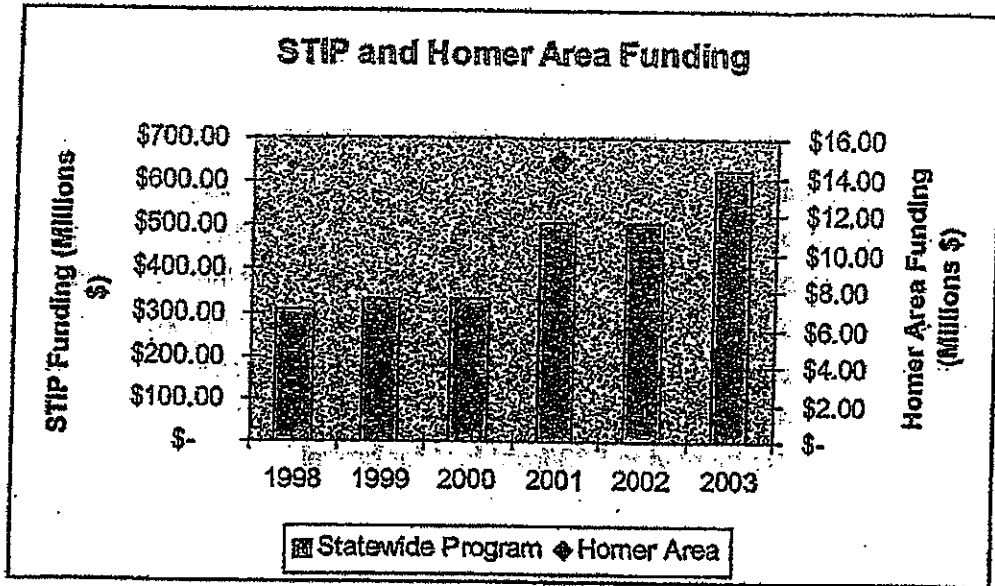


Table I-7 presents the street and roads needs for Homer, and is from the DOT/PF website (http://www.dot.state.ak.us/cgi-bin/projneeds.d/need_list).

Table I-7: State of Alaska Road and Trail STIP Needs for Homer

Name	Description	Estimate	STIP Category
Bartlett/Hohe Street	Rehab Bartlett St. from Pioneer St. to Fairview Ave. (2000'). Rehab Hohe St. from Fairview Ave. to the South Peninsula Hospital (1000'). Work on both streets includes lowering grade, minor realignments within existing ROW, widening driving lanes to current standards, water/sewer stubouts, storm drains, curb & gutter, sidewalks, and lighting at street intersections.	1,000,000	CTP
Baycrest/Homestead Ski/Trailhead Rehabilitation	Construct a year round trailhead that provides off-street parking on Rogers Loop Road.		TRAAK
Bunnell St/Olsen Lane Rehabilitation	Reconstruct and pave 1 mile of urban streets within the Homer City limits. Project scope and cost estimate are modified to include lighting and sidewalks.	1,100,000	CTP
East End Road Improvements	Rehabilitate and resurface Homer East End Road.		
East End Road Safety Trail MP 9.5 to MP 11.9	Construct a separated pedestrian/bicycle facility along East End Road from MP 9.5 to MP 11.9		TRAAK

Name	Description	Estimate	STIP Category
East End Road: MP 0.0-3.6	Rehab from Lake Street (MP 0.0) to the Kachemak Drive (MP 3.6). Reconstruct the road base, drainage, vertical and horizontal curves, pedestrian facilities and resurface. Sidewalks on both sides will be constructed to the elementary school from that point a separated trail will be constructed on the uphill side with four shoulders on both sides of the road to Kachemak Drive.	9,600,000	CTP
East End Road: MP 3.6 to 12.5	Rehabilitate from the intersection with Kachemak Drive (MP 3.6) to the McNeil Canyon School (MP 12.5). Rehabilitate the roadway; provide shoulders; improve drainage; provide pedestrian facilities; and address safety concerns as warranted. Pedestrian facilities will be considered between MP 9.5 and MP 11.8. Also includes separated path pathway from Kachemak Drive to Chelsea Street and from Hunter Road to McNeil Canyon School.	9,850,500	CTP
End of the Road Wayside Park Construction	Construct a highway wayside at the beginning of the Sterling Hwy and adjacent to the AMHS terminal and dock which consists of a rest and viewing area, toilet facilities, parking, pathway and interpretive signs.		TRAAK
Freight Dock Road	Upgrade and pave Freight Dock Road from Sterling Highway to Homer Deep Water Dock (4,000').	200,000	CTP
Homer Mooring Improvements	Replace the existing Homer ferry terminal marine structures.	3,500,000	NHS
Homer Scenic Overlook	Construct a scenic overlook adjacent AMHS deck/parking area on Homer Spit.	600,000	TRAAK
Homer: East End Road: MP 14.2 to 22.0 Rehabilitation - Phase II	Rehabilitate roadway from the McNeil Canyon School (12.5) to the vehicle parking turnaround at Vosnesenka (MP 22). The project will include widening, realignment, drainage improvements and resurfacing.	12,765,000	CTP
Kachemak Drive	Rehabilitate Kachemak Drive from the Sterling Hwy to East End Road, 3.5 miles in length. Work includes improving and raising the embankment, surfacing, widening and drainage improvements. Pedestrian facilities will be evaluated.		CTP
Kachemak Drive Pathways	Construct a pathway along Kachemak Drive from East End Road to the Sterling Highway (approximately 3.5 miles).	1,000,000	TRAAK
Kenai Peninsula Road and Trail Improvements	Rehabilitate, resurface, and pave approximately 3 miles of Skyline Drive from the end of pavement on West Hill Road to the intersection with East Hill Road in Homer. Construct a trail connection and landscaping at the intersection of Lake Street and the Sterling Highway.		
Mission Avenue	Improve drainage, replace soft areas in the road base and hard surface approximately one mile.	750,000	CTP

5 EXISTING STREET CLASSIFICATION

Streets provide dual functions of access and mobility. The degree to which these functions are served determines the street classification. The primary reference for street classification is the American Association of State Highway and Transportation Officials "A Policy on Geometric Design of Highways and Streets". In this reference, AASHTO uses the broad classifications of arterials, collectors, and local streets as a hierarchical system.

Arterial Streets emphasize mobility. Although, arterial streets can provide access, the street is usually designed to carry higher volumes at higher speeds, attributes that usually conflict with safe access.

Local Streets are those streets, which emphasize access and penalize mobility. These will have frequently spaced driveways, will be designed for low speeds and low volumes, and may have a high degree of pedestrian or bicycle use, parking, on-street delivery, and other landside functions that spill into the street and street right-of-way.

Collector Streets provide an intermediate link in the hierarchy between local streets and arterials. Typically, many local streets will connect with a collector, and many collectors to an arterial. Collectors often provide a mixed function of both access and mobility.

The study area roads and streets are presented on Plate 1. Arterial and collector streets are identified. All other streets are local streets.

AASHTO further defines the demographic environment for streets and roadways as urban (population areas > 50,000 people), small urban areas (between 5,000 and 50,000 people), and rural areas (those outside of the urban areas). Street classifications are modified with the adjective of either "rural" or "urban". Based on a population of about 4,000, the streets in Homer will be rural. However, as a practical matter, the roadways serve an area population that extends far beyond the City of Homer boundaries, and this area-wide population is well above 5,000. It should be noted that the DOT/PF has categorized all of their roads in the study area as rural.

AASHTO also subdivides arterials into "principal arterials" and "minor arterials", and subdivides collectors into "major collectors" and "minor collectors". The reader is directed to AASHTO for these definitions. DOT/PF adheres to AASHTO sub-classes for their streets.

The State of Alaska DOT/PF Central Region publishes the Annual Traffic Volume Report that lists functional classifications and Annual Average Daily Traffic (AADT) for selected streets and roads. Table I-8 shows the data from the 1999 report.

Table I-8: Homer Area Roads

CDS Route	Name	Class	1999 AADT	AADT Location
110000	Sterling Highway	Rural Principal Arterial	8,300	Lake St.
110000	Homer Spit Rd. (Sterling Hwy)	Rural Principal Arterial	4009	Between End of the Spit and Kachemak Bay Drive.
110100	Pioneer Avenue	Rural Major Collector	7,300	Lake St. and East End Rd.
110150	Lake Street	Rural Major Collector	5,550	Sterling Hwy.
110200	Kachemak Bay Drive	Rural Major/Minor Collector	1,720	Sterling Hwy.
110300	East End Road	Rural Major Collector	7,700	Lake St. / Pioneer Ave.
110305	East Hill Road	Rural Minor Collector	1,800	East End Rd.
110500	Bartlett Street	Rural Minor Collector	1,270	Pioneer Rd.
110610	Bunnell Avenue	Rural Minor Collector	1,930	Olson Lane
110615	Olson Lane	Rural Minor Collector	470	Sterling Hwy.
110625	Main Street	Rural Major Collector	2,770	Sterling Hwy.
110716	Mission Road	Rural Minor Collector	125	East Hill Rd.
110800	West Hill Road	Rural Minor Collector	1,400	Sterling Hwy.
110900	Skyline Drive	Rural Minor Collector	460	East Hill Rd.
111300	Diamond Ridge Road	Rural Minor Collector	250	Sterling Hwy.

6 TRAFFIC MODELING AND FORECASTS

6.1 Methodology

This study uses the Quick Response System II (QRS II) for transportation models. The Quick Response System (QRS) was formulated in the 1970s as a set of manual techniques that could be used on planning problems too small for the computer technology of the time. These methods are documented in the National Cooperative Highway Research Program Report #187. It has evolved into the Windows-based QRS II, which is packaged with the General Network Editor (GNE), which serves as a data input module to QRS II. QRS II software employs all four steps of the modeling process and uses the inputs of development and land use, economic information, and transportation system attributes.

The existing network system of collectors and arterials was entered into a 1999 baseline model. This network system describes speeds, lanes, approach configurations, type of intersection control, and inherent delays. The year 1999 was selected because there is good roadway volume information that can be used for calibration. Only the arterials and collectors were entered into the network because the low volumes of local streets are not modeled well. In addition, it has been found that local streets are usually unaffected by area growth, and local traffic will be determined and limited by the site-specific development. Development is modeled by creating centroids that contain residential, business, and institutional data indicating location and levels of development.

QRS II's primary centroid inputs for traffic generation are dwelling units and employees. Appendix B contains information about housing, lots, schools, employment, and businesses, which were used for input into a baseline model. The baseline employment information was further adjusted using some of the data in Table I-5, above.

In addition, other land uses, such as industrial and institutional uses, require special Production/Attraction tags to describe unique trip generators. This model was built using the detailed schema. With this schema trip ends for special generators were modeled, such as post offices, schools, docks, and other facilities that require Production/Attraction tags. The ITE Trip Generation Manual as the basis of trips at special generators.

External stations are nodes that define the boundary conditions, and require information about travel patterns at the ends of the model. Types of trips were estimated (e.g. home to work, home to other, tourism) to and from the external stations.

In order to have confidence in the model, the base model was calibrated to 1999 (AADT) volumes published by DOT/PF for the Homer Area Roads shown in Table I-8. Calibration was a highly iterative process, where centroids, network attributes (e.g. delay at intersections) and external station trip types are adjusted until the model link volumes agree reasonably well with the published volumes. At the end of the calibration phase the 1999 calibrated base model was within 5% to 20% of the 1999 AADT, for most of the roadways. There were roads, most notably West Hill Road, Main Street and Bartlett Street, which did not calibrate very well with the 1999 AADT. However, Main and Bartlett are parallel streets, and together the model shows that they carry about the combined volume of the 1999 AADT. As such, the model as calibrated to the 1999 volumes was accepted (see Table I-9 in Section 6.3 and compare volumes in "1999 DOT/PF AADT" column to "1999 Base Model ADT" column).

Once the model was calibrated, it became the basis of future development and system activities. Residential dwelling units were assumed to grow at the same forecasted rate as the population. As discussed, under Subsection 2.4 Population Forecasts, the future annual population growth rate was estimated to be about 2%, and therefore the dwelling units will increase at about 2% per year. Using Equation I-1, it was estimated that the total number of dwelling units in the study area will increase about 49 to 50 percent over the next 20 years. Economic growth was assumed grow at 2.4% annually as developed under Subsection 2.5 Local Economy. Total economic growth, primarily stated as employees, is expected to increase about 60 percent in the next 20 years.

To model the locations of the future dwelling unit growth, future generators were placed in the model consistent with current development plans and patterns. An algorithm was developed to equally assign new residences to subdivisions. It was assumed that existing unsubdivided areas would be developed to some extent in the future, with lot sizes similar to surrounding subdivisions, and that growth was included in the model. Once a subdivision was full (dwelling units equal lots), no further units were added. Future commercial development was treated in a similar manner.

The modeled 2021 transportation network included those improvements listed in Table I-7. The one improvement that seemingly would have the most effect on this system is paving Kachemak Drive. Presently, the gravel surface limits speeds. The model does not have surfacing parameters, but the travel speed was raised by 10 mph as a means of estimating the travel benefit. Once paved, the road will provide an attractive alternative to traveling through the CBD and using East End Road for the entire trip. It was found that the volumes did increase on Kachemak Drive by 10 to 15% once the road is paved. However, other street volumes, especially within the CBD, were not reduced by an appreciable amount by the paving of

Kachemak Drive. However, the volumes on Sterling Highway were increased. (It should be noted that Kachemak Drive was paved in 2002, after the model was run, but before the adoption of this plan.) In addition to Kachemak Drive pavement, the new dock facility on the spit was included in separate model runs to ascertain the impact of the dock on roadway operations.

Homer has distinct seasonal variations in traffic flow. DOT/PF maintains a permanent traffic recorder on the Homer Spit Road south of Kachemak Drive. Average summer daily traffic is about 7,800 vehicles, which is about 95% higher than the AADT. It was decided to develop models based on the summer months as well because of these huge summer increases over the average model. Most traffic engineering analysis and decision processes use the peak hour of traffic flow as the design hour, therefore summer peak hour models were developed to review measures of effectiveness.

6.2 Recent Studies

The Boutet Company (TBC) prepared a Traffic Study (Draft) for Homer (January 2, 1999). Pertinent to this plan, TBC's study recommended a street classification system, and prepared capacity analysis of key intersections during summer peak hours. Their capacity analysis showed that all intersections, except the Sterling Highway and Lake Street operated at level of service "C" or better. Sterling and Lake Street operated at level of service "D".

The State of Alaska Central Region Traffic and Safety Section performed a traffic signal warrant analysis for the Sterling Highway (By-Pass) and Lake Street intersection. They found that signalization is warranted, even in the lower volume winter season.

TBC also prepared the Kachemak Bay Multi-Purpose Dock Traffic Impact Analysis, March 7, 1999. Trip generation data from this report for this model. They found that signalization, or other means of traffic control is warranted, even in the lower volume winter season.

6.3 Results

Several model runs were prepared for this transportation plan to describe the present and future operations of the existing system (with near-term known improvements). The following models are included under Appendix C.

- 1999 Base Model- This model was the basis of further work and was calibrated to the published 1999 AADT in the Central Region Traffic Volume Report.
- 2021 Base Model- This run includes future population and economic data, and known future street improvements. It shows AADT in 2021, without the Dock
- 2021 Base Model with Dock (both daily traffic and peak hour traffic)- These models superimposed the Dock volumes on the 2021 Base Model. This represents the future system with known improvements.
- 1999 Summer Model (both daily traffic and peak hour traffic)- These models show the 1999 summer volumes that are estimated to occur now.
- 2021 Summer Model (both daily traffic and peak hour traffic)- These models show the summer volumes in 2021, without the dock.
- 2021 Summer Model with Dock (both daily traffic and peak hour traffic)- These models include dock traffic, and will show the level of future dock impacts on the system when compared to the summer models without the dock. These models represent the design condition in the future.

Each model diagram has volumes shown on links. These volumes are directional, and the total segment volume is the sum of the two volumes. Table I-9 presents 1999 actual DOT/PF AADTs for key street segments, and the model outputs for those segments under various time and development scenarios. Table I-10 presents design hour volumes (DHV) for those segments.

Table I-9: Selected Average Daily Traffic (ADT)

State System Highway / Street	Segment Location	1999 DOT/PF AADT	1999 Base Model ADT	2021 Base Model w/Dock	2021 Summer Model w/Dock
Sterling Highway	Lake Street	8,300	7,277	11,919	14,441
Homer Spit Rd. (Sterling Hwy)	Between end of the Spit and Kachemak Bay Drive	4,009	4,161	7,468	13,719
Pioneer Avenue	Lake St. and East End Rd.	7,300	6,820	10,046	13,428
Lake Street	Sterling Hwy.	5,550	4,450	6,871	8,043
State System Highway / Street	Segment Location	1999 DOT/PF AADT	1999 Base Model ADT	2021 Base Model w/Dock	2021 Summer Model w/Dock
Kachemak Bay Drive	Sterling Hwy.	1,720	1,936	3,666	4,665
East End Road	Lake St./Pioneer Ave.	7,700	7,704	9,856	10,505
East Hill Road	East End Rd.	1,800	2,134	3,403	3,701
Bartlett Street	Pioneer Ave.	1,270	2,234	3,205	3,683
Bunnell Avenue	Olson Lane	1,930	1,218	1,902	2,073
Olson Lane	Sterling Hwy.	470	717	1,097	1,221

Main Street	Sterling Hwy. to Pioneer Ave.	2,770	1,480	2,883	3,919
Mission Road	East Hill Rd.	125	173	300	323
West Hill Road	Sterling Hwy	1,400	2,378	3,446	3,696
Skyline Drive	East Hill Rd.	460	385	625	671
Diamond Ridge Road	Sterling Hwy.	250	826	1,220	1,422

Table I-10: Selected Design Hour Volumes (DHV)

State System Highway / Street	Segment Location	1999 Base Model DHV	2021 Base Model w/Dock	2021 Summer Model w/Dock
Sterling Highway	Lake Street	635	990	1,308
Homer Spit Rd. (Sterling Hwy)	Between end of the Spit and Kachemak Bay Drive	356	641	1,158
Pioneer Avenue	Lake St. and East End Rd.	590	902	1,147
Lake Street	Sterling Hwy.	391	633	690
Kachemak Bay Drive	Sterling Hwy.	167	332	402
East End Road	Lake St./Pioneer Ave.	573	841	857
East Hill Road	East End Rd.	196	299	326
Bartlett Street	Pioneer Ave.	195	273	300
Bunnell Avenue	Olson Lane	105	155	178
Olson Lane	Sterling Hwy.	61	85	103
Main Street	Sterling Hwy.	137	250	503
Mission Road	East Hill Rd.	15	26	28
West Hill Road	Sterling Hwy	210	305	328
Skyline Drive	East Hill Rd.	33	54	62
Diamond Ridge Road	Sterling Hwy.	71	107	121

In evaluating the performance of the existing system, QRS II reports for delay were used to estimate intersection levels of service. Level of Service (LOS) is a quantitative description of roadway facility operations. In this project, LOS analysis is considered for unsignalized intersections. Levels of Service and capacity analysis methods are developed under the Highway Capacity Manual, Transportation Research Board Special Report 209 (HCM). Unsignalized intersection LOS is defined by control delay. Control delay includes initial deceleration delay, queue move-up time, stopped delay, and final acceleration delay. However, the methodology only presents LOS for the minor movements of the intersection, which include the minor street approaches under sign control, or major street movements that must yield to oncoming traffic such as left-turning traffic. There is no overall unsignalized intersection LOS. Unsignalized LOS is defined in Chapter 10 of the HCM as follows:

LOS A: ≤ 10 seconds per vehicle

LOS B: > 10 and ≤ 15 seconds per vehicle

LOS C: > 15 and ≤ 25 seconds per vehicle

LOS D: >25 and ≤35 seconds per vehicle

LOS E: >35 and ≤50 seconds per vehicle

LOS F: >50 seconds per vehicle

QRS II calculates approach delay for intersections. Table I-11 presents the approach delay for the summer evening peak hours in 1999 and 2021 at the model's intersections. We have assigned LOS to these approaches based on the delay. This table presents delay with and without the multi-use dock. This enables the reader to determine the system impacts of the new facility. Those approaches that have LOS "D" or worse are shown in bold.

Table I-11: Delay and LOS for Model Intersections

Intersection	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
Diamond Ridge Road and Sterling Hwy	Southbound	1.6	A	0.6	A	0.6	A
	Westbound	13.5	B	18.3	C	18.5	C
	Northbound	0	A	0	A	0	A
Skyline Drive and East Hill Road	Westbound	5.2	A	8.6	A	8.6	A
	Eastbound	0	A	0	A	0	A
	Northbound	11.8	B	11.5	B	11.5	B
East End Road and Kachemak Drive	Westbound	9.5	A	4.8	A	4.6	A
	Eastbound	0	A	0	A	0	A
	Northbound	14.2	B	19.7	C	20.0	C
Fairview Street and Bartlett Street	Eastbound	11.0	B	11.5	B	10.9	B
	Southbound	4.5	A	4.3	A	2.7	A
	Eastbound	11.2	B	11.5	B	11.3	B
Fairview Street and Main Street	Northbound	2.2	A	3.1	A	3.1	A
	Eastbound	10.5	B	9.9	A	10.7	B
	Southbound	0	A	0	A	0	A
	Westbound	11.4	B	11.1	B	10.9	B
Svedlund Street and Fairview Street	Northbound	4.2	A	1.3	A	1.5	A
	Southbound	0	A	0	A	0	A
	Eastbound	9.4	A	10.3	B	9.6	A
Pioneer Avenue and Heath Street	Northbound	3.1	A	6.1	A	5.7	A
	Northbound	12.4	B	56.6	F	94.8	F
	Westbound	1.2	A	2.7	A	3.1	A
Kachemak Way and Pioneer Avenue	Eastbound	0	A	0	A	0	A
	Southbound	11.1	B	15.9	C	16.8	C
	Westbound	0.1	A	0	A	0	A
	Eastbound	0.5	A	1.3	A	1.0	A
Svedlund Street and Pioneer	Northbound	11.1	B	13.7	B	14.3	B
	Southbound	11.7	B	19.4	C	23.3	C

Intersection Avenue	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
	Westbound	0	A	0	A	0	A
	Eastbound	0.6	A	0.2	A	0.2	A
	Southbound	12.1	B	18.0	C	18.0	C
East Hill Road and East End Road	Westbound	0	A	0	A	0	A
	Eastbound	5.7	A	3.7	A	3.7	A
	Southbound	9.7	A	14.4	B	14.4	B
Fairview Drive and East End Road	Westbound	0	A	0	A	0	A
	Eastbound	8.4	A	1.3	A	1.4	A
	Eastbound	0.4	A	0.4	A	0.4	A
Sterling Hwy and West Hill Road	Southbound	18.0	C	35.1	E	35.8	E
	Westbound	0	A	0	A	0	A
	Southbound	12.8	B	59.2	F	18.1	C
Main Street and Pioneer Avenue	Westbound	4.2	A	4.5	A	3.6	A
	Eastbound	1.2	A	0.8	A	1.0	A
	Northbound	12.7	B	25.9	D	17.8	C
	Southbound	10.8	B	18.0	C	18.6	C
Bartlett Street and Pioneer Avenue	Westbound	0	A	0	A	0	A
	Eastbound	3.9	A	4.2	A	4.7	A
	Southbound	13.9	B	99.0	F	983.9	F
Heath Street and Sterling Hwy	Westbound	0	A	0	A	0	A
	Eastbound	3.9	A	4.2	A	4.7	A
	Eastbound	5.6	A	4.2	A	4.0	A
Pioneer Avenue and Sterling Hwy	Southbound	14.3	B	63.1	F	48.7	E
	Westbound	0	A	0	A	0	A
	Westbound	0	A	0	A	0	A
Ohlson Lane and Sterling Hwy.	Westbound	0	A	0	A	0	A
	Northbound	13.9	B	28.3	D	25.1	D
	Eastbound	0	A	0	A	0	A
Main Street and Sterling Hwy	Southbound	13.9	B	53.3	F	37.4	E
	Westbound	1.4	A	1.4	A	1.5	A
	Eastbound	0	A	0	A	0	A
	Northbound	11.9	B	19.8	C	17.2	C
Lake Street and Smokey Bay Way	Southbound	3.5	A	3.0	A	3.5	A
	Westbound	10.6	B	13.4	B	13.1	B
	Northbound	0	A	0	A	0	A
Thompson Court and Lake Street	Westbound	13.7	B	15.2	C	15.2	C
	Southbound	1.3	A	1.7	A	3	A
	Northbound	0	A	0	A	0	A
Sterling Hwy and Lake Street	Eastbound	6.1	A	7.6	A	10.7	B
	Westbound	0	A	0	A	0	A
	Southbound	14.2	B	1462.3	F	522.7	F
Sterling Hwy and Kachemak Drive	Southbound	7.8	A	3.1	A	3.1	A
	Northbound	0	A	0	A	0	A
	Westbound	15.3	C	52.8	F	71.3	F
Diamond Ridge Road / West Hill Road / Westwood Drive / Skyline Drive	Eastbound	11.7	B	11.4	B	11.4	B
	Southbound	11.7	B	11.5	B	11.5	B
	Westbound	11.6	B	11.3	B	11.3	B

Intersection	Approach	Year	LOS	Year	LOS	Year	LOS
		1999 Summer		2021 Summer		2021 Summer w/New Dock	
East End Road / Pioneer Avenue / Lake Street	Northbound	11.7	B	11.4	B	11.4	B
	Westbound	10.4	B	15.7	C	17.3	C
	Eastbound	10.9	B	17.9	C	15.9	C
Ben Walters Street / Smokey Bay Way / Timmerman Court	Northbound	11.0	B	15.9	C	16.9	C
	Southbound	10.3	B	10.2	B	10.1	B
	Eastbound	10.6	B	11.1	B	11.1	B
	Northbound	10.2	B	10.9	B	10.9	B

Table I-11 shows that the following intersections will have poor LOS and undesirable delays by the planning year.

- Pioneer Avenue and Heath Street
- Sterling Hwy and West Hill Road
- Main Street and Pioneer Avenue
- Heath Street and Sterling Hwy
- Pioneer Avenue and Sterling Hwy
- Main Street and Sterling Hwy
- Sterling Hwy and Lake Street
- Sterling Hwy and Kachemak Drive

In addition, TBC's analysis shows that the Sterling Highway/Lake Street intersection already operates at an undesirable LOS, and the DOT/PF shows that a signal is warranted at this intersection. It should be noted that the 1999 summer model shows a LOS "B" for the minor movement. The RSC recommends that traffic control alternatives to signalization be considered.

The models also show that the summer peak hour volumes for the Spit Road and East End Road will increase to levels where a two-lane facility may be over capacity, especially considering the volumes of left-turning vehicles that will use the road. Through past work, the consultant has found that two lane roadways with moderate speeds (40 to 45 mph) can accommodate through volumes of around 10,000 vehicles per day, or 1,000 to 1,300 per hour, as long as there is not a substantial number of left-turn vehicles. Lower speed roads (25 to 30 mph) can accommodate higher flows, because desirable headways (time between vehicles) can be maintained at a reduced distance.

I. PURPOSE and INTENT

1. The H.A.R.T. is a combined local funding source of dedicated sales tax and assessments to upgrade city streets, new city streets and new city non-motorized trails.(Ordinance 06-42(S); Resolution 88-47 #1)
2. The intent of the program is to reconstruct local substandard city roads and/or upgrade existing city roads, construct new city streets and non motorized trails, thereby reducing maintenance cost, improving access, increasing property values and improving the quality of life. (Ordinance 06-42(S); Resolution 88-47 #2)
3. Reconstruction and new construction shall be to City Standards. (Ordinance 06-42(S) Resolution 88-47 #19)
4. The City will not accept a street for full time maintenance until it meets city standards and is shown on the official maintenance map.¹ (Ordinance 85-14 07/01/85; Resolution 88-47 #8)
5. When practical, the intent of the program is to preclude the destruction of existing property improvements in built up areas. (Resolution 88-77(A), be it further Resolved clause.)
6. State maintained roads are not part of this program. (Resolution 88-47 #7)
7. The criteria for the H.A.R.T. shall be reviewed annually by the Transportation Advisory Committee, with recommendations reported to the Homer City Council. (Resolution 88-47 #22)
8. Annexed roads are included as newly eligible roads, as listed on the Official Road Maintenance Map. (Resolution 03-116, 08/25/03)
9. New roads shall be listed on the Official Road Maintenance Map. (Resolution 07-82)
10. New trails shall be listed on a map in the City Clerk's Office. (Resolution 07-82)

II. DEFINITIONS

- A. Sidewalk- the term "sidewalk" means a pedestrian facility associated with a road and generally within a street right of way. (Resolution 07-82)
- B. Trail – a pedestrian facility detached from a road, or not within a street right of way. (Resolution 07-82)

¹1.Clerk's Note: Done by Ordinance

- a. Connectivity to existing road(s), for example completes a traffic pattern.
- b. Arterials or thoroughfares;
- c. Existing utilities;
- d. Contributing funds such as property owner assessments, loans, grants, etc;
- e. Level of need. (Resolution 07-82)

B. Trails

New local non-motorized trails shall be prioritized according to the following:

- a. Project is listed in the HNMTTP or furthers a stated goal of that plan;
- b. Solves a safety concern;
- c. Creates connectivity to existing trail(s), completes pattern or provides access to a point of interest;
- d. Protects an established trail;
- e. Creates or improves a trailhead;
- f. Has significant scenic or aesthetic value;
- g. Existence or potential for contributing funds;
- h. Property owner participation. (Resolution 07-82).

IV. FINANCING and ASSESSMENTS

1. The program will utilize an additional dedicated City sales tax not to exceed three quarters of one percent (¾%) supplemental with assessments against adjacent benefited properties. (Ordinance 06-42, Resolution 87-61(S), Resolution 88-47 #3).
2. A three quarters of one percent (¾) dedicated sales tax and will be collected for up to twenty years expiring December 31, 2007 and reauthorizing up to an additional twenty years expiring December 31, 2027 to participate in funding the accelerated roads and trails program (Ordinance 06-42, Resolution 87-61(S), Resolution 88-47 #4). Reauthorized twenty additional years at the October 3, 2006 election (Resolution 06-145(S)) to expire December 31, 2027. Ten percent of the annual revenue shall be used for trail projects.
3. The road improvements will be financed on a combined pay as you go basis as well as sale of revenue bonds in a fifty-fifty ratio. There may be future bond sales as revenues increase. (Resolution 87-47 #6)
4. The City will attempt to obtain long term financing for up to ten years for the private share of funding. (Resolution 88-74 #12, bond change Ordinance 89-17, regarding ten years financing.)

5. Interest, if any, generated from the program will remain with the program funds. (Resolution 88-47 #18)
6. Abutting property owners will share the cost of upgrading a street to residential standards by paying \$30 per front foot for gravel and \$17 per front foot for paving.² (Resolution 87-61(S), Resolution 88-47, Resolution 94-50, Resolution 95-97)
7. The City will pay all costs for any additional improvements required when deemed necessary by the City. Other improvements requested by the benefited property owners will be paid by those same property owners. (Resolution 88-47 #11)
8. The \$30/\$17 split in front foot assessment stands unless there is 100% agreement on a revised formula for a specific project or the amount is adjusted by the City Council. (Resolution 87-61(S), Resolution 88-47; #21; Resolution 95-97)
9. Road Reconstruction assessment payment date, penalty and interest shall be set as soon as the reconstruction project has been accepted by the Public Works Department regardless if the LID/Assessment district wherein reconstruction has been completed is also scheduled for paving as part of the same LID/Assessment District. Paving assessment payment date, penalty and interest will be set as soon as the paving project has been accepted by the Public Works Department. HCC 17.04.070--110. (Resolution 96-73)
10. New Local Roads may be constructed by 100% program funds when the construction thereof benefits the entire City or when the City owns the property wherein the road is to be constructed. The Road to be constructed must meet the qualifying criteria and be recommended by the Transportation Advisory Committee to the City Council. This expenditure must be approved via Ordinance with justification noted within the body of the Ordinance. Whenever possible, New Local Roads will be constructed using the LID process HCC 17.04 and the assessment methodology as noted in item 6. and 8. (Resolution 07-82)
11. HART funds may be used to leverage outside funds for New Local Roads and Trails. (Resolution 07-82)
12. New Local Trails may be constructed using 100% program funds and follow the procedures listed in item 10. (Resolution 07-82)
13. Sidewalks shall be paid for out of road funds, and trails shall be paid for out of the 10% allocated to trails. (Resolution 07-82)

² Danview/Svedlund and Sabrina/Mark White are grandfathered in at the \$20/\$11 split per Council action. (Resolution 94-52)

developer shall construct the streets on the alignment adopted in the Master Roads and Streets Plan, and conforming to the respective classification. The developer shall be required to construct the street to a twenty-eight-foot width in accordance with the minimum requirements of a local residential street; provided, however, that the City may, upon direction of the City Council, elect to require construction to the full standards and pay to the developer the cost difference between the required street and the proposed street. (Resolution 07-82)

VII. TRAIL PRIORITIZING CRITERIA AND PLANNING GUIDELINES

A. Trail Prioritizing. The TAC and Parks and Recreation Advisory Commission will review the trail priority list during the annual review of the HART. The list will be presented in a memorandum from staff, and will contain a mix of large and small projects. Generally it will include up to five trail projects that staff has reviewed and found ready for preliminary work. Trails on this list are planned for construction in the near term (one to three year timeframe). Staff will actively work to prepare those projects for construction. (Resolution 07-82)

B. Trail Planning Guidelines

Trail design shall take into account at minimum the following:

1. Use context sensitive design when locating and planning trails to take advantage of scenic resources.
2. Respect the character of trails based on function, setting, and expectation of accessibility.
3. Evaluate the soils, drainage, wetlands, Tsunami zone, flood plain, stream setbacks, historical resources, visual resources, topography, existing and potential land use, zoning and land ownership.
4. Where estimated costs, operating costs and outside funding availability are considerations and important criteria, care should be used to ensure that important trails are not eliminated solely using cost as a determinant.
5. Multi-use trails are encouraged. Design of the trail should include consideration of compatible uses such as pedestrians and bicycles.
6. All trails should be designed to recognize the requirements of ADA standards and guidelines. (Resolution 07-82)



City of Homer
Planning & Zoning
491 East Pioneer Avenue
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E-mail Planning@ci.homer.ak.us
Web Site www.ci.homer.ak.us

MEMORANDUM

TO: Mayor Hornaday and Homer City Council
THRU: Rick Abboud, City Planner
FROM: Julie Engebretsen, Planning Technician
DATE: November 2, 2011
SUBJ: Homer Advisory Planning Commission comments on the Kachemak Drive Pathway

At the September 12, Homer City Council meeting, the Council considered Resolution 11-90, brought forward by the Parks and Recreation Advisory Commission. The resolution supported the concept and construction of a pathway or other non-motorized improvement along Kachemak Drive. The Council referred the matter to the Planning Commission.

At the September 21st HAPC meeting, the Commission made and approved the following motions:

- THE ADVISORY PLANNING COMMISSION SUPPORTS THE CONCEPT OF A NON MOTORIZED ACCESS ALONG KACHEMAK DRIVE.
- A LARGE PART OF THIS PROJECT IS A PRIVATE PROPERTY RIGHTS ISSUE THAT SHOULD BE ADDRESSED CAREFULLY FROM THE ONSET. THE UTILITY EASEMENTS ARE PRIVATE PROPERTY.
- THE ADVISORY PLANNING COMMISSION RECOMMENDS THE CITY ADD THE KACHEMAK DRIVE PATH IMPROVEMENTS TO THE STIP NEEDS LIST AS AN AVENUE FOR STATE FUNDING.
- THE COMMISSION APPRECIATES THE EFFORTS OF THE PARKS AND RECREATION ADVISORY COMMISSION AND ENCOURAGES THEM TO CONTINUE WITH THIS GRASS ROOTS EFFORT.

Kachemak Drive Path Committee

Thursday, February 23, 2012, 5:30 p.m.

Bumppo Bremicker, Chairman, Beth Cumming, Lynn Burt, Lindianne Sarno, recording, Dave Brann, David Clemens, Kevin Walker, visitor.

Motion to Approve Agenda: Beth moves to put New Business B. before New Business A. No objections, we approve this change. Beth feels it's important to get our resolution done and ready to go to Parks and Rec.

Dave Brann moves to approve, Beth Cumming seconds, no objections, approved.

Motion to approve minutes: Beth moves. Dave seconds, no objections, minutes approved.

Public comments: Kevin Walker, when shall we discuss my packet of info? Beth thinks this packet should accompany the resolution to City Council, and it will be discussed then, including Kevin in the discussion.

Pending Business: Continuing Discussion and Planning for Path Design, page 7. Kevin Walker at the microphone. Regarding providing public with access to and from the airport for pedestrians who get off planes. Kevin's letter to Jennifer Bailey cleared up; we are not proposing to trespass on airport property, but rather to provide safe non-motorized transportation corridor for people coming to and from the airport. Dave Brann reports that Jocelyn Biloon and Scott Thomas, say DOT is working on this. They recognize there is a committee, and Dave is their contact. Walt Wrede, City Manager will get in touch with Ms. Biloon regarding a traffic count. Jennifer Bailey is our contact at DOT Public Transportation Facilities, Aviation Leasing. She is in Anchorage. Kevin Jones is the airport manager.

Bumppo points out that if we are in the DOT right of way we have to follow their requirements, regardless of whether DOT is paying for it. (1) Survey (2) Engineered drawings (3) ADA design specs (4) erosion control (5) contract with DOT to provide maintenance (6) liability and insurance coverage. City of Homer sponsorship is crucial, especially for ongoing maintenance.

Dave Brann points out most of the easements are private property easements. They are not a big stumbling block, especially with approval of City Council. Utility easements are outside DOT right of way. Until we survey each section on the ground we won't know for sure but huge section of road is outside DOT right of way.

Dave Clemens: suggests we send DOT a package about the DOT right of way on the sections that are in DOT right of way. Kevin Walker says Jennifer Bailey says this is already in DOT planning. Next step is contacting the City Manager, the City Manager contacting state planning, and getting back to us.

Bumppo: what we need is for city to officially to buy into this and deal with DOT.

New Business: page 67, reformulating the Resolution on the Kachemak Drive pathway. City Council sent it back to Zoning and Planning, who sent it back to us. It was too muddy last time.

Add to the resolution; include a funding plan mentioning that there will be \$305,000 by the end of 2012.

Beth suggests we send to the city this resolution, the Kachemak Drive Path specifics packet created by Kevin Walker, and a funding plan.

FUNDING PLAN: the city can borrow money at good rates, against the HART money, need not be bonded, borrow against the revenue source for 10 years. Julie Engebretsen, Planning Department, says this is a common practice. Funding plan would include private donations and HART funding. Page 95 of our packet gives an outline of the Planning and Approval process.

WHAT WE MUST ACCOMPLISH: Bumppo: We want City Council to direct city administration to start focusing on this pathway. Dave Brann commented, to direct city staff to do what they can to keep this moving along. Give the city a plan for maintenance. Bumppo interjected that we still need to contact landowners, how will they feel about this. Dave Brann: this is on top line of things to talk about with the City Manager.

Beth: Two City Council members commented, "We want to know what you have in mind."

DOCUMENTS WE NEED: (1) resolution (2) packet from Kevin is ready. Complete these two items tonight. Then Renee can advise us on form of (3) funding plan (not ready) and (4) a letter to private landowners will come from the City. (5) Maintenance plan.

We revise the resolution. See resolution on next page.

Beth moves that we approve Resolution 11-090 as revised and send it on to Parks and Recreation and the City Council; Dave seconded. Unanimous approval. See resolution on next page.

Bumppo moves that we assign a council member to each committee member, and well in advance of the City Council meeting bring to them the packet including: resolution, Kevin Walker's packet, and funding plan. Dave Brann says we'll not have a projected cost until the survey. Dave Clemens suggests we mention that we've scaled back from the East End Bike Path to less than \$1.5 million per mile.

Beth proposes changing our Committee's mission statement: "to build a safe non-motorized separate path along Kachemak Drive, connecting East End Road to Spit Road via the airport. Kevin's packet is called our preliminary engineering packet.

Beth proposes a fund-raising event to raise \$5,000 to \$10,000 petty cash. She has engaged a professional event producer, Sally Oberstein, to produce the event, and is proceeding with these plans as a private citizen. Funds would be used for chain saws, Surveying, signage, and other uses.

Dave Clemens mentions that the Homer Cycling Club approves increased signage to increase safety on Kachemak Drive for those commuting cyclists who will continue to use the road. If Kachemak Drive is improved, the cycling club would support widening the shoulder for the safety of commuting cyclists.

Beth wonders if the Mud Bay Trail is included. We see the Mud Bay Trail is included in Kevin Walker's pre-engineering packet.

We discuss the plastic walkway and decide a request has gone to the city for the walkway.

Report on December 10 Field Trip: Dave Brann said it was valuable, going from the airport lot, down, looked at possibility of cut-off trail. It will be challenging. Walked along the beach to the road. Site visitation was valuable. Clarified and provided yellow line and blue line on the pre-engineering packet. Lynn agrees with Dave. Beth says the foundation is there on Aviation Leasing land for a full quarter mile, bridge would be used to cross the gully, then pleasant going among the trees. Yes, it drops steeply and bicyclists could walk there. Bicycle trail would be fairly easy along the Mud Bay Trail.

Kevin Walker: revised mission statement as noted. Will include resolution. Lindianne will e-mail resolution to Kevin as well as to Renee. Will leave off last three pages.

homerkev@gmail.com, clerk@ci.homer.ak.us,

Re funding and maintenance: Dave, Kevin, Renee, will put those together, and Kevin will send them out to the Committee via e-mail. HART funds: \$234,719 at end of 2011, \$305,000 by end of 2012.

Dates: Next Parks and Rec is 15th of March, City Council will be 26th of March. Possible for this resolution and preliminary engineering packet to go to these meetings. Renee, please put the Kachemak Drive Path on the agendas for both those meetings. Bumppo will call or e-mail each Committee member to assign a City Council member.

Our next meeting: March 22, fourth Thursday in March.

We adjourn. 9:05 p.m.

CITY OF HOMER
THE REVISED RESOLUTION:

City Clerk/Parks and
Recreation Advisory Commission

RESOLUTION 11-090

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA,
SUPPORTING THE CONSTRUCTION OF A NON-MOTORIZED
PATHWAY TO INCREASE SAFETY FOR MOTORIZED AND NON-
MOTORIZED USERS ALONG KACHEMAK DRIVE LOCATED
WITHIN HOMER CITY LIMITS, FROM THE BASE OF THE HOMER
SPIT TO EAST END ROAD

WHEREAS, The Parks and Recreation Advisory Commission established the Kachemak Drive Path Committee to specifically address possible solutions to the hazards presented to non-motorized and motorized users of Kachemak Drive; and

WHEREAS, The Kachemak Drive Path Committee received substantial public input on safety concerns; and

WHEREAS, The Homer City Council has shown support for this non-motorized pathway by inclusion of the Kachemak Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the Homer Non-Motorized Transportation and Trail Plan; the Homer Area Transportation Plan; the Climate Action Plan; and the Homer Accelerated Roads and Trails (HART) Policy Manual; and

WHEREAS, Increasing non-motorized transportation improves public health and safety; encourages tourism; cleans the environment; reduces transportation costs; and enhances livable communities; and

WHEREAS, The City of Homer has available HART trail funds that can, including other sources, form a basis for funding this project; and

WHEREAS, Utility easements can be employed with the permission of property owners.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, hereby supports the construction of a non-motorized pathway along Kachemak Drive within the City of Homer, and that said improvement will enhance safety and benefit the public; and

BE IT FURTHER RESOLVED that the City Council of Homer, Alaska further supports actions increasing safety for motorized and non-motorized users of Kachemak Drive in the following ways:

1. Increasing the usage of signage warning drivers of bicycles and pedestrians on roadway.
2. Build a separated path paralleling Kachemak Drive using utility easements and public property, and increase the use of signage.

Jo Johnson

From: David Clemens [clem-19@hotmail.com]
Sent: Monday, May 07, 2012 6:09 PM
To: Department Clerk
Subject: Resolution 11-090(S)

My name is David Clemens and I am a resident of the City of Homer. I have been a member of the Kachemak Drive Path Committee that forwarded this resolution to council, but the thoughts here are my own.

I would like to support passage of Resolution 11-090(S), to build a non-motorized trail along Kachemak Drive between East End Rd. and the base of the Homer Spit. This stretch of road is the perfect candidate for a non-motorized path as it connects two very well used paths in town. In addition, the safety of all users, motorized and non-motorized alike, would be greatly improved by such a path. As someone who both drives and rides bike along Kachemak Drive, it is clear the road as presently constructed cannot safely accommodate cars and trucks along with walkers, runners, and bikers -- of which there are many. A separated path off the road could solve this not only for those currently using the road, but would, I believe, see a dramatic increase of use along this corridor from those who do not feel comfortable on the road as of now. This path, I'm sure, would be welcomed as both a transportation alternative for non-motorized traffic and as recreational opportunity for all. I know my family would use it as such.

It is my understanding that this type of trail has been talked about and wished for in town for some time and passing this resolution would be a good step towards realizing it. There is substantial support and excitement for this project to happen sooner rather than later. I urge the council to pass Resolution 11-090(S).

In addition, I feel it would be good for Council to keep in mind for the future that when the State decides it is time for Kachemak Drive to receive major maintenance, that the road would benefit greatly from having shoulders added on both sides. There will be some people who continue to use the road even if a path is constructed, and shoulders would help facilitate this.

Thank you, David Clemens
1605 Eagleview Drive
Homer AK 99603

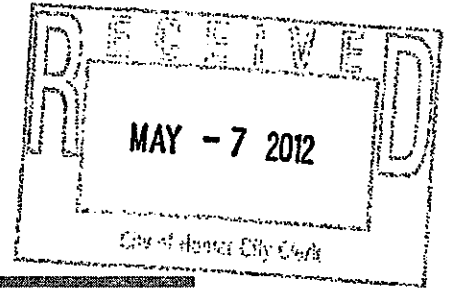
Jo Johnson

From: Hayley Norris [hayleybird412@gmail.com]
Sent: Monday, May 07, 2012 1:26 PM
To: Department Clerk
Subject: Support for Resolution 11-090(S)

I am a local cyclist, pedestrian, and vehicle driver and I support Resolution 11-090(S). Homer needs more places for cyclists and pedestrians to travel. This only makes our community safer and healthier. By encouraging people to walk or ride a bike, you are also helping the environment because less fossil fuels are being burned in our communities. We should strive to have sidewalks and bicycle lanes on all major roads in Homer. This is a step in that direction. Please support making Homer a better place to live!



2181 Kachemak Drive
Homer, Alaska
Phone (907) 399-2174
chucklindsay@gmail.com



May 6, 2012

Dear Homer City Council:

I am writing to express my support of the construction of the proposed Kachemak Drive Path (Resolution 11-090(S)). I work daily at an office located on Kachemak Drive. Although I live 10 miles east of this location, I ride my bike to work an average of once/week, year-round. I think that a non-motorized path would increase the safety of both non-motorized and motorized users of Kachemak Drive. I frequently see people walking to air taxi operator businesses, riding bikes, jogging, and walking their dogs along this road. Although the speed limit is 35 mph, moderately heavy traffic along this road often includes oversize vehicles like large boats being towed to and from the Homer Spit, which challenge the safety of non-motorized transportation along this road. Construction of a non-motorized path along Kachemak Drive would provide increased safety for non-motorized users to commute and recreate. A path along Kachemak Drive would also provide a critical non-motorized transportation link connecting the Homer Spit Path and the East End Road Path. Construction of the Kachemak Drive Path would likely increase non-motorized traffic along Kachemak Drive, potentially increasing business opportunities along this road and tourism in the city of Homer. I request that the Homer City Council pass resolution 11-090(S). If this trail is constructed, I will personally support the project by donating funds and volunteer hours for construction and maintenance.

If you have any questions, please feel free to call or email me at your convenience. Thank you.

Respectfully,

/s/ Chuck Lindsay

Renee Krause

From: Beaver & Jessie Nelson <bjnelson@alaska.net>
Sent: Tuesday, September 06, 2011 9:25 PM
To: Renee Krause
Subject: Kachemak Drive Bike Path

Sept 7, 2011

City of Homer

ATTN: Rene Krause

RE: Kachemak Bike Path

I am commenting on the various proposals to turn Kachemak Drive into a very unfriendly road for the working people of Homer to gain access between the Spit/Boat Harbor and the businesses along Kachemak Drive and the Gear Shed on East Road.

This road is not a scenic byway - it is an industrial road used to transport boats, trailers with fishing gear, freight hauling 18-wheelers and for airport activities. The entire fishing population east of Kachemak Center, which includes the Russian villages, uses it to access the Spit. At times, several trips are made per day between the Spit and the Gear Shed or other businesses along the road.

Do we really want all that traffic going through town? Narrowing the lanes and implementing a 25 mph speed limit would be ridiculous! There is a mile-long straight stretch with a double yellow line - to go 25 mph with not another car in sight is just not going to happen. This would be like an office worker's computer running at half speed. Incredibly infuriating. Do we want our police force enforcing this limit? I don't. Actually, the speed limit between the Bay Club and Northern Enterprises should be 40-45 mph.

I like bike paths. It just needs to be along the utility right-of-way so the working people do not bear the burden for other peoples' pleasure. Leave Kachemak Drive alone.

Jessie Nelson
Mile 5 East End Rd.

From: "Thomas, Scott E (DOT)" <scott.thomas@alaska.gov>
Subject: RE: signage on Kachemak Drive, Homer
Date: August 26, 2011 11:08:44 AM AKDT
To: Molly Brann <brann@alaska.net>
Cc: "Jones, Kevin L (DOT)" <kevin.jones@alaska.gov>, "High, Carl S (DOT)" <carl.high@alaska.gov>, "Vanderwood, Randy D (DOT)" <randy.vanderwood@alaska.gov>, "Huling, Lawrence K (DOT)" <larry.huling@alaska.gov>, "Rader, Val S (DOT)" <val.rader@alaska.gov>, "Morton, Kenneth M (DOT)" <ken.morton@alaska.gov>

Mr. Brann,

As we discussed, I recommend the following:

1. SHARE THE ROAD signs.

A. A State perspective: DOT/PF has no policy/rules, program, or resource on where best to do this or proof they are effective. If we did, we would likely choose higher priority roads on a southcentral Region basis. Kachemak Bay Drive has visibility, and it is our practice to limit the use of warning signs to things people need to know because they can't see the problem. Bicyclists can be seen on this route and sharing the road should be obvious to the driver due to the lack of shoulders and other facilities.

B. A local perspective: State priorities are not the same as local priorities. What I can recommend and support is a local government plan for signing for SHARE THE ROAD or BIKE ROUTE[s] or pathways. This is the approach we are taking in the Municipality of Anchorage, where the city creates a priority plan for bike routes and pathway circulation regardless of road ownership. They identify in a plan the routes and connectivity in an area. Then they are responsible for signing those routes. We would entertain an agreement for route signing that is conducted at the local level with continuity in mind. That will get you away from a Regional priority system for investing in signing and into more of a system that makes sense and is easier to coordinate/plan at the local level.

2. SPEED signs - driver feedback, battery operated.

A. DOT/PF is not opposed to these, but again, is only prioritizing these and maintaining these at the Regionwide level - with our highest priority being to test the effectiveness of these in our Safety Corridors - high speed, high volume highways with a crash problem. For the same reasons as above, (resources, funding, priority) we are not doing this on other roads.

B. Similar to bicycling signs, we have agreed to allow the City of Anchorage to test driver feedback signs on state roads. We do require they take speed measurements and document effectiveness before they are allowed to remain indefinitely. I would recommend a local level approach to the use of these signs for education and policing information to the driver, with a local priority system. Same as Anchorage and local use of speed carts. We would entertain an agreement led by the City of Homer to have a local effort in speed feedback signs, as long as it is part of a prioritized plan, funded by and operated by the City.

Thank you for calling.

Scott Thomas
Traffic Safety Engineer
DOT/PF Central Region
907-269-0633
scott.thomas@alaska.gov

-----Original Message-----

From: Molly Brann [mailto:brann@alaska.net]
Sent: Tuesday, August 23, 2011 1:57 PM
To: Thomas, Scott E (DOT)
Subject: signage on Kachemak Drive, Homer

Hello Mr. Thomas,

I was referred to you by Kevin Jones, DOT here in Homer. I am working on various ideas to increase safety for pedestrians, bicyclists and motorists along Kachemak Drive in Homer. I am writing as a concerned citizen and one that supports the concept of a bike/pedestrian path along Kachemak Drive. Until all the questions related to a possible path can be answered, possibly years down the road, I think a few things can be done to increase safety for users of the road.

I would like to be able to add a couple of Bicycles on the Road or Share the Road signs to existing signposts, the yellow advisory signs, not to the black and white regulatory signs. This would avoid (I hope) having to get permits to add new sign posts in the ROW. I think this would help make drivers aware there may be bicycles on the roadway, especially since there is a blind hill and some blind corners along the road. Without paved shoulders bicyclists do ride in the road-this summer has seen an increased number of commuter type bicyclists.

I have also been researching driver feedback / radar signs that are small and alert drivers when they go over the posted speed limit. The speed limit on Kachemak Drive is 35 but many, if not most, drive 45mph. The radar signs are battery operated and can be placed on existing sign posts. The signs are about \$3000 and I think I could raise the funds for two if permission could be obtained to mount them on existing signs. The local police chief and the city manager support the concept as far as slowing traffic and increasing safety.

Any advice, information or perhaps permission you could send my way would be greatly appreciated.

Sincerely,
Dave Brann

Jo Johnson

From: Renee Krause
Sent: Friday, July 29, 2011 7:50 AM
To: Melissa Jacobsen; Jo Johnson
Subject: FW: Request for Resolution from Councilmember Zak

See below. I can draft one if you are too swamped. I believe the goal is to have it on the agenda for August 22 cc meeting since Francie was gone for the first meeting.

Renee Krause, CMC
Deputy City Clerk I

-----Original Message-----

From: bryanzak@aol.com [mailto:bryanzak@aol.com]
Sent: Thursday, July 28, 2011 10:22 PM
To: Renee Krause
Subject: Re: Excerpt from the Parks and Recreation Minutes of July 21 2011

I will sponsor a resolution to this effect.

-----Original Message-----

From: Renee Krause <RKrause@ci.homer.ak.us>
To: Bryan Zak <BryanZak@aol.com>; Francie Roberts <roberts2@alaska.net>; David Lewis <davelyn@gci.net>
Cc: Beth Cumming <listentothewaters@yahoo.com>
Sent: Thu, Jul 28, 2011 12:05 pm
Subject: Excerpt from the Parks and Recreation Minutes of July 21 2011

The below is an excerpt from the Meeting Minutes of the Parks and Recreation Advisory Commission regarding recommendations submitted on a proposed Kachemak Drive Pedestrian and Bike Path this was requested from Commissioner Cumming to be emailed to you seeking support for the recommendation and a resolution from Council to continue/proceed with the concept.

I believe this may be brought up at the Council Meeting August 8th.

COMMITTEE REPORTS/STAFF REPORTS

- C. Kachemak Drive Path Committee - Commissioner Harrald
1. Synopsis of the June 17, 2011 Meeting
 2. Synopsis if Surveys Received as of July 13, 2011
 3. Survey received as of July 1, 2011
 4. Recommendations to the Commission
 5. Memorandum dated July 14, 2011 Discussion on Recommendations to Council from the Committee

Chair Bremicker requested a motion and read the motion submitted by the Committee into the record as follows:

THE KACHEMAK DRIVE PATH COMMITTEE OF THE HOMER PARKS AND RECREATION ADVISORY COMMISSION RECOMMENDS THAT THE FOLLOWING BE PASSED AND SUBMITTED TO CITY COUNCIL:

TO SUPPORT THE ACTIONS INCREASING THE SAFETY FOR MOTORIZED AND NON-MOTORIZED USERS ALONG KACHEMAK DRIVE IN ANY OR ALL OF THE FOLLOWING WAYS:

- ALTERATION OF THE EXISTING KACHEMAK DRIVE AND SHOULDER
- A SEPARATED PATH PARALLELING KACHEMAK DRIVE USING THE UTILITY EASEMENTS
- LOWERING THE SPEED LIMIT AND INCREASING THE USE OF SIGNAGE
- THE PARKS AND RECREATION ADVISORY COMMISSION WILL WORK IN COOPERATION WITH THE CITY OF HOMER IN THE PLANNING AND IMPLEMENTATION PHASES.

BRANN/ARCHIBALD - SO MOVED.

CUMMING/LILLIBRIDGE - MOVED TO AMEND THE MOTION TO READ "TO SUPPORT DEVELOPMENT OF PATHS ALONG KACHEMAK DRIVE FOR NON-MOTORIZED USERS INCLUDING WALKERS, RUNNERS AND BICYCLISTS TO INCREASE SAFETY FOR BOTH MOTORIZED AND NON-MOTORIZED USERS IN THE FOLLOWING WAYS.

Discussion proceeded on the wording changes and that the word flow and intent of the motion was the same and the original recommendation was appropriate. The maker and second pulled the amendment from consideration. Chair Bremicker commented that the motion was acceptable as written.

Commissioner Lillibridge re-read the motion as written for clarification.

VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

Ms. Krause responded to questions regarding the process and what happens when they submit the recommendations to City Council. She informed the commissioners that since this item was not and has not been on the council agenda, the recommendation can be verbally presented at the August 8, 2011 Council meeting during Commission Reports, a memorandum from the Commission could be submitted for inclusion in the packet. It will be up to the Council if they would like to proceed further on the recommendation and it must have Council support for the recommendation to have anything further done. The commissioners must realize that the recommendation could be presented to council and that is it, nothing more happens even with the support of councilmembers. The issue will then be finished and the job of the Committee may then be complete. She recommended contacting council members to try to gain support for further action and implementation of the recommendations that will be forwarded by this commission. She confirmed that Council has the option to request a resolution to express support also.

Commissioner Cumming commented that she would like to do as much as possible to ensure that the council supports the recommendation. She expounded on her reasons for having this approved by council.

Renee Krause, CMC
Deputy City Clerk I
City of Homer
491 E. Pioneer Avenue
Homer, Alaska 99603
Ph. 907-235-8121 ext. 2224
Fax. 907- 235-3143
rkrause@ci.homer.ak.us

"Successful is the person who has lived well, laughed often and loved much, who has gained the respect of children, who leaves the world better than they found it, who has never lacked appreciation for the earth's beauty, who never fails to look for the best in others or give the best of themselves."

The information contained in this message is proprietary and/or confidential. If you are not the intended recipient, please: (i) delete the message and all copies; (ii) do not disclose, distribute or use the message in any manner; and (iii) notify the sender immediately. In addition, please be aware that any message is subject to archiving and review by persons other than the intended recipient. Thank you.

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

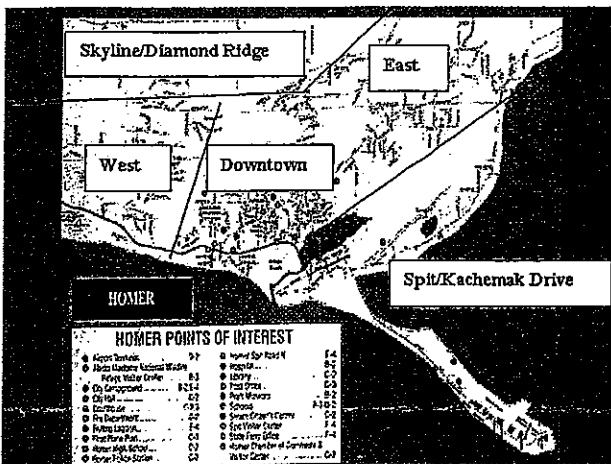


Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Name (Optional) Address: (Optional)
 City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|--|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input checked="" type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

Support +

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

strongly support
safer for all

support -
safer for all

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

no

safety +
preserving scenic byway drive option

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

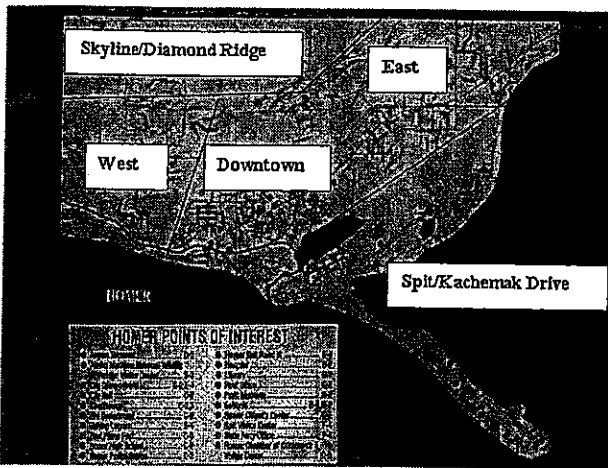
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
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Name (Optional)

Address: (Optional)

City, State, Zip



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3. How often do you ride a bicycle or walk along Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
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- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike or walk, which of the following describes why you use Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

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- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|---------------------------------------|--|--|---------------------------------------|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

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10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

The ability to safely ride a bike or run on the road. Additionally, it should NOT be commercialized

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

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YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

Renee Krause

From: chase warren <likes_to_roam@yahoo.com>
Sent: Wednesday, July 06, 2011 11:56 AM
To: Renee Krause
Subject: path survey
Attachments: survey.kdpc_06.21.11.xml

Excellent idea to put this online!

<?xml version="1.0" encoding="UTF-8"?>

<form1>

<TextField3>41347 mclay rd</TextField3>

<TextField4>kachemak city, ak, 99603</TextField4>

<Answer>East Homer</Answer>

<Answer/>

<Answer/>

<Answer/>

<Answer>Downtown Homer</Answer>

<Answer/>

<Answer>Occasionally, about once or twice a month</Answer>

<Answer>Regularly, once or twice a week</Answer>

<Answer>Routine Errands</Answer>

<Answer/>

<TextField1/>

<TextField1/>

<TextField2>Chase Warren</TextField2>

<Question11>How about 30? Or, and I know the committee has little to do with this but, what if we actually enforced the speed limit? Make it a double fine zone. 25 is kinda pushy, considering nobody wants to go 35.</Question11>

<Question10>Good luck. If the cost, right of way issues, dealing with the airport, dealing with the bog, cooperation with HEA, and the time it would take to deal with all those things were not issues, then yes I am for it. Create a shoulder first, then tie yourselves up with this project for the next 10 years. </Question10>

<Question5>I am in full support of this option. Although it is not an ideal solution, as it does not safely open up the shoulder to children/families, or persons with special needs. However, the cost/effectiveness of this option for out way the others. A consistent shoulder width is not necessary to immediately alleviate the dangers of riding on the current uneven and rock/sand riddled shoulders. No fill is needed, which dramatically reduces the cost of creating a shoulder. In addition, I would propose more signs, both speed limit and be aware of pedestrian/cyclists signs.</Question5>

<Question3>The trail would promote tourism, the shore bird festival would benefit greatly. Perhaps increased traffic would cut down on blatant littering. Also the trail seems to cut right through areas currently being used as squatter camps, the remnants of these camps are evident by the bags of trash and weather logged materials scattered along the bluff. I'd rather see people appreciating the area, not abusing it. The downside of this option is that it really doesn't address the inherent safety hazards of riding and/or walking on K drive, and could distract from the real objective of the committee. </Question3>

<Question1>Ideally, a separated ADA approved path connecting E end road to the spit, with alternate nature trails headed north and through the bog that could be used as ski trails in the winter</Question1>

<Question2>Every year since I have lived here I have seen more cyclists and higher gas prices. If you want people to be able to afford to live here, something that has always been an issue, the city design should be able to accommodate alternative modes of transportation. </Question2>

<Question13>considerations made by the state/city/drivers who speed/drivers who pass a pedestrian or cyclists with another car coming in the other lane at the same time also, the condition or lack of shoulder. the lack of considerations by others could be alleviated by a sufficient shoulder</Question13>

<Question6>This would be absolutely negligent, which is my current opinion of the situation. K dr. has become a major through route for those working/ travelling to the spit or ocean drive. For everyone's safety there needs to be a shoulder. If nothing is done now, when there is the opportunity, any accidents in the future would reflect quite clearly that the state and the city has not been concerned enough about the safety of persons to do anything. </Question6>

<RadioButtonList>1</RadioButtonList>

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<RadioButtonList>1</RadioButtonList>

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<RadioButtonList/>

<RadioButtonList>1</RadioButtonList>

<Question1>pave out to the edge of the dirt, re-stripe the road, narrow the lanes if at all possible, put up signs, enforce the current speed limit. any or all of these I feel would dramatically improve the situation as is</Question1>

<Question1>Your dealing with a situation that places people within 12-24" of being struck by a moving vehicle. I feel that no matter how difficult or significant the investment that the city has made some major oversights in letting this go on for so long and must do something before someone is hurt. I'm all for the narrow lanes and re-paving option!</Question1>

</form1>

Renee Krause

From: Hayley Norris <hayleybird412@gmail.com>
Sent: Wednesday, July 06, 2011 11:16 AM
To: Renee Krause
Subject: Kachemak Drive Survey
Attachments: Survey Kachemak Drive 07.06.11.xml

attached.

<?xml version="1.0" encoding="UTF-8"?>

<form1>

<TextField3>41347 McLay Road</TextField3>

<TextField4>Homer, AK 99603</TextField4>

<Answer>East Homer</Answer>

<Answer/>

<Answer/>

<Answer/>

<Answer>Downtown Homer</Answer>

<Answer/>

<Answer>Occasionally, about once or twice a month</Answer>

<Answer>Occasionally, about once or twice a month</Answer>

<Answer>Routine Errands</Answer>

<Answer>Routine Errands</Answer>

<TextField1>and 'Trips to the Spit.'</TextField1>

<TextField1>and 'Trips to the Spit.'</TextField1>

<TextField2/>

<Question11>I think 25 mph is too low of a speed limit. I'd support 30 mph.</Question11>

<Question10>I do support this trail, but only after the narrower lane/shoulder option. This trail would accommodate pedestrians, older and younger, but not cyclists. I think cyclists must be accommodated for because the mode of travel is so beneficial to our environment and society. So, I only support this option if it is included with a plan to put in shoulders along Kachemak Drive for cyclists.</Question10>

<Question5>This is my favorite option and I whole-heartedly support it. There are so many benefits that could be realized with the implementation of this option. For one, the speed that drivers drive will be less with the narrower lanes. This makes things safer first off. Next, cyclists and pedestrians would have a designated spot to walk and ride if a shoulder is painted. This improves safety and encourages residents to get out and walk/ride that beautiful road. Also, this option seems that monetarily it would be relatively cheap compared to a separated bike path. The only thing not good about this option is that some kids and older people might not feel safe on a shoulder alone and this is why a separated path should still be considered as a long-term goal. This option could be a shorter-term goal.</Question5>

<Question3>I do support this option because it would be the beginning or end of a full-length Kachemak Drive pedestrian path. Also, locals and tourists could more easily enjoy the beautiful views and wildlife in the area. That stretch of Kachemak Drive is one of the most dangerous spots for walking and cycling on the road; therefore, a trail could make that safer for pedestrians at least.</Question3>

<Question1>I'd like to see bike lanes and a separated path for other pedestrians. This will mean there is a loop of trails: the Spit, East End Road and Kachemak Drive. Maybe even some camp grounds and other nature, bird viewing trails.</Question1>

<Question2/>

<Question13>It is unsafe to walk or cycle along Kachemak Drive and it is an important road in our town.</Question13>

<Question6>I do not support this option. Kachemak Drive is an important road in our community and it needs to be safer. Kachemak Drive being safer makes this a better community.</Question6>

<RadioButtonList>1</RadioButtonList>

<RadioButtonList/>

<RadioButtonList/>

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<RadioButtonList>1</RadioButtonList>
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<Question1/>
<Question1/>
</form1>
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PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

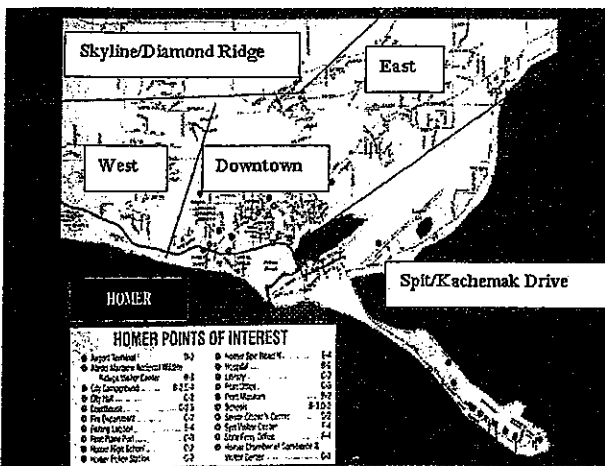


Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
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Cowles Council Chambers City Hall
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Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Name (Optional) Address: (Optional)
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- Skyline/Diamond Ridge

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4. How often do you drive a car on Kachemak Drive?

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|--|--|--|--|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input checked="" type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?
 I can't find where Airport Access Road is.

A better connection to FAA road would be good, esp. the crossing from the W-side of Ocean Drive (heading out the spit) onto the spit trail (E-side) could be improved. A round-a-bout would be a really good idea here.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?
 Yes, I'd support this option. This is an economical and effective solution. By keeping bicyclists on the road, they are easier for motorists to see, making it safer for everybody.

When going for this option, it is important to implement it on both sides. Cycling in the opposite direction from car traffic is very dangerous because bikes are not expected to come from this direction and their speed is generally underestimated.

As with any of the other solutions, snow plowing and dirt can be

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?
 Motorists tend to like this option because it gets bikes off "their" road, and inexperienced bikers like it because it gives them a (false!) sense of security.

Unfortunately, this is actually a bad idea in terms of bike safety. Drivers cannot see or do not notice bicycles that far off the road. Riding this kind of trail is often less safe than riding on the road

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?
 Good idea.

Why limit this to summer months? Driving conditions don't get better during the winter.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?
 There's a need to improve Kachemak Drive. That said, I would not have given it the highest priority. Main Street, of all places, is an embarrassment for our town. Please go there, get out of the car, and take a walk. Now imagine pushing a baby stroller through the dirt. Now imagine doing this in winter with a foot of snow. It's downright scary.

13. What do you feel are the most pressing issues facing Kachemak Drive?
 Intersection with Spit Rd is dangerous.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
 Maintain the rural/wilderness character of Kachemak Drive. Widening this drive would be a sad mistake. This is a great scenic drive through wetlands, past baby moose, with views of the bluff and the the bay, and all that right next to town.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?
 A speed limit along with a narrower road for cars would be a great start. One without the other would be dangerous.

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.
 Round-a-bout at the base of the spit, intersection Kachemak Drive and Spit Rd. Not that big of an investment, really. Pay-off in smoother traffic and considerable fuel savings compared to the current stop-signs. This would also improve safety for motorists, cyclists and pedestrians.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

```

<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3>41640 Gladys Ct</TextField3>
  <TextField4>Homer</TextField4>
  <Answer>East Homer</Answer>
  <Answer/>
  <Answer>Kachemak Drive and Spit</Answer>
  <Answer/>
  <Answer/>
  <Answer/>
  <Answer>Never</Answer>
  <Answer>Regularly, once or twice a week</Answer>
  <Answer> Other </Answer>
  <Answer>Trips to the Library, Museums, Spit, Parks, etc.</Answer>
  <TextField1>To access the spit.</TextField1>
  <TextField1/>
  <TextField2>Duane Howe</TextField2>
  <Question11>That would be too confusing to motorists to have changing speed limits. They
    already drive 50 mph in the 25 zone. </Question11>
  <Question10>That could work if the easements are far enough from the road to allow several feet
    of separation from the roadway. I walk frequently on the spit and along East Road, and I can
    tell you I would not feel safe with any less space than there is along those paths, and there is
    several feet most of the time. I have known two people who were killed trying to walk or ride
    a bike too close to traffic. I would not risk doing it myself.</Question10>
  <Question5>That would not allow enough space to walk or ride a bike safely. </Question5>
  <Question3>No. It would not be of much use just to go that far. </Question3>
  <Question1>Pretty much like it is now.</Question1>
  <Question2>That road is primarily to access the homes along it, and it should stay that way. There
    is little place for businesses along it because it is mostly wetland which should not be
    disturbed or filled in.</Question2>
  <Question13>Surface water drainage and domestic water supply.</Question13>
  <Question6>Since there is already a path on East End Road we may not need another one to go
    essentially to the same place. Extending the present one farther east would make more
    practical sense to me.</Question6>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
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  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>
  <Question1>Improve the drainage and rezone the commercial portion to conservation. There
    should be no development in the wetlands. This would add to the recreational value of a path,
    but the cost would be questionable. I have seen groups of bikers that ride out via one route and

```

return via the other route, so this might be an excuse to improve Kachemak Drive to accommodate that sort of recreation. </Question1>

<Question1>Little investment should be made here because most of that land will eventually be eroded away along with the houses on it. There is no way to prevent it as we are learning in other parts of town.</Question1>

</form1>

<?xml version="1.0" encoding="UTF-8"?>

<form1>

JUL 01 2011 PM01:53

<TextField3/>

<TextField4/>

<Answer>East Homer</Answer>

<Answer/>

<Answer/>

<Answer/>

<Answer>Downtown Homer</Answer>

<Answer/>

<Answer>Infrequently; maybe every few months</Answer>

<Answer>Frequently, several times a week or every day</Answer>

<Answer> Other </Answer>

<Answer>Other</Answer>

<TextField1>exercise and to go birding</TextField1>

<TextField1>to go to the spit or businesses that are closer that way for me</TextField1>

<TextField2>Lani Raymond</TextField2>

<Question11>I definitely think you need to acknowledge that there is lots of bike and pedestrian traffic in winter also!! Not so much as summer, but still quite a bit. AND, it is a more dangerous situation because of ice on the road. This road is often quite icy and with the curves and limited visibility, can be challenging for drivers. Please also note that if you reduce the speed limit but don't enforce it, the situation will still be very dangerous.</Question11>

<Question10>This might be another good idea. Possibly safer but more costly.</Question10>

<Question5>I think there should be a narrowing of the road and make a walking/biking lane on ONE SIDE ONLY. The speed limit should be reduced and enforced. The speed limit should also be lower in winter because there is still lots of use (biking and walking) even in winter and with icy roads, the lower limit would still be needed. I think also some sort of "watch for pedestrians and bikers along this road" caution signs should be in place at each end.</Question5>

<Question3>It's a good idea. I walked it last week and there were several people camping down there. Concern about it being a possible homeless area? Also how maintained in winter?</Question3>

<Question1>Not built up with lots of commercial stuff. It is beautiful as it is now. Hopefully there won't be too much more erosion. If it had a bike/walking path somewhere along it to use safely, it would be a fantastic asset. Plus being able to go from town out East End Rd, down Kachemak Dr then back into town as a circle (or triangle).</Question1>

<Question2>More use of Kachemak Drive for driving and walking and biking. More people wanting to use bikes and walk for various reasons--save gas, exercise, etc.</Question2>

<Question13>Safe travel for: bicyclists, walkers and vehicles. I want to say somewhere that your Question #3 might be going to give you a false sense of amount of use this road gets for walking and biking. AS IT IS NOW, I and many others I know avoid using Kachemak Drive. But if it were safe, there would be way, way more use!!!!</Question13>

<Question6>Obviously something needs to be done!!</Question6>

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<RadioButtonList/>

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<RadioButtonList/>
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<RadioButtonList>1</RadioButtonList>

<Question1>Figure out what is most affordable and weigh the safety. I also think that you need to do something SOON!! Not just have an elaborate plan for 5 years down the road (pardon the pun there). Reducing the speed limit for vehicles is easy and cheap. People may complain but taking a little bit more time is not the end of the world. (But would have to be enforced.)
</Question1>

<Question1>Ideally there would be a bike path along the whole way but that might not be possible due to cost, land owners not wanting to cooperate, and the fact it is wetland part of the way. If enough people think about it and help plan, there will be a way found to accomplish this.</Question1>

</form1>

Renee Krause

From: Adam Bauer <abauer@bauerhaus.ws>
Sent: Friday, July 01, 2011 6:13 PM
To: Renee Krause
Subject: Kachemak Drive survey
Attachments: survey.kdpc.pdf

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

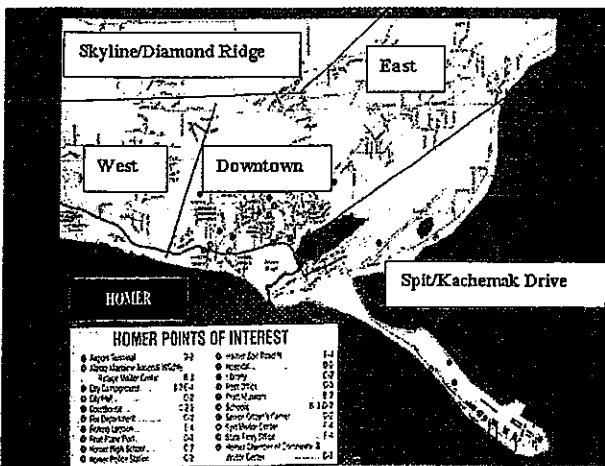


Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Name (Optional) Address: (Optional)
City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle or walk along Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike or walk, which of the following describes why you use Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

6. If you drive a car, which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|--|
| <input checked="" type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input checked="" type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
 The area will most likely lose more coastline to erosion there will most likely be fewer residents. I doubt there will be any more businesses. The road will become an important transportation link diverting traffic from East Road to Ocean Drive both of

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. FRONTIER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

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<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3/>
  <TextField4>Homer, AK 99603</TextField4>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer>Kachemak Drive and Spit</Answer>
  <Answer/>
  <Answer/>
  <Answer/>
  <Answer>Frequently, several times a week or every day</Answer>
  <Answer>Regularly, once or twice a week</Answer>
  <Answer>Commuting - Work or School</Answer>
  <Answer>Commuting to Work or School</Answer>
  <TextField1/>
  <TextField1/>
  <TextField2>Ryan Briscoe</TextField2>
  <Question11>Yes, as long as the speed limit is enforced. Although, you will still have cars that go
    around bicyclists/walkers in unsafe locations.</Question11>
  <Question10>Yes, this would be the ideal situation and would encourage more user groups to use
    the trail.</Question10>
  <Question5>If there is actually enough room to do this safely, then I would support it. I am
    skeptical that there is enough room with the current width of the road to make this
    feasible.</Question5>
  <Question3>Yes, this seems to be the most dangerous spot since cars frequently go around me on
    my bicycle in the middle of the hill where they cannot see what is coming the other
    direction.</Question3>
  <Question1>Kachemak drive should have a trail that connects the Spit trail to the East End Road
    trail. </Question1>
  <Question2>I don't know the history beyond three years, so I can't speak to long term changes,
    but the city should provide safe travel options for multiple user groups.</Question2>
  <Question13>Providing a safe road for all user groups (cars, walkers, bicycles,
    etc.).</Question13>
  <Question6>I do not support taking no action.</Question6>
  <RadioButtonList/>
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  <RadioButtonList>1</RadioButtonList>
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  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>
  <Question1>For starters, you could add the half mile trail from the spit to the airport access road
    and widen the shoulders on the remainder of the road. </Question1>

```

<Question1>Ultimately, it would be best to have a trail connecting the spit to East End Road.</Question1>

</form1>

Renee Krause

From: Ryan & Erin Briscoe <rebriscoe602@hotmail.com>
Sent: Thursday, June 30, 2011 10:38 AM
To: Renee Krause
Subject: Kachemak Drive Path survey
Attachments: survey.kdpc_.06.21.11_0.xml

Hello,

I filled out the survey online and clicked on submit via email. It said to save file and attach to an email, so I did that. It saved it as an .xml file instead of a .pdf file. Hopefully, you can read the .xml file and see my comments.

Thanks,
Ryan

Dear Homer Community Member,

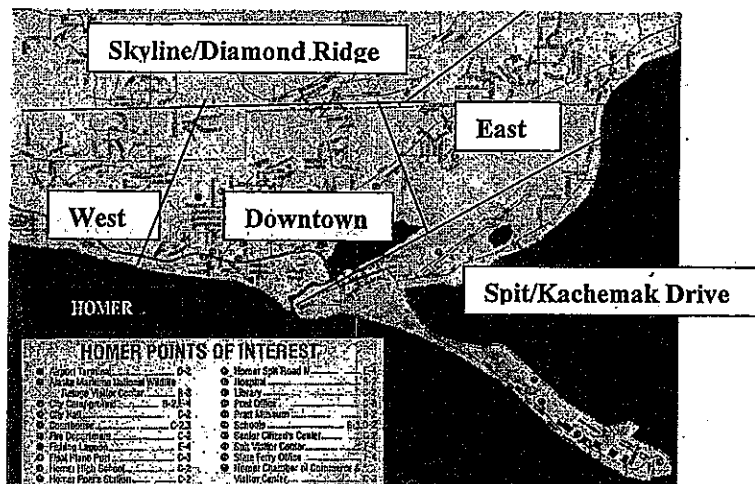
Thank you for attending the Kachemak Drive Proposed Bike Path Open House.

Background: Earlier this year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented tonight are ideas created and supported by the subcommittee, but not put forth as concrete plans. Your input and opinions will help us formulate further actions, if any, that should occur along this issue.

Please help our efforts by completing the survey form below.

1. Please refer to the map below and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge



Please refer to the map and indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

How often to do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently: maybe every few months
- Occasionally: about once or twice a month
- Regularly: once or twice a week
- Frequently: several times a week to every day

If you ride a bike, which of the following describes why you use it on Kachemak Drive. Check all those that apply.

- Regular exercise or workout
- Commuting to work or school
- Routine errands
- Trips to the library, museums, parks, and similar places
- Family outings or touring
- Other (please indicate)
- I do not ride a bike

How often do you drive a car on Kachemak Drive?

- Never
- Infrequently: maybe every few months
- Occasionally: about once or twice a month
- Regularly: once or twice a week
- Frequently: several times a week to every day

If you drive a car, which of the following describes why you use it on Kachemak Drive. Check all those that apply.

- Commuting to work or school
- Routine errands
- Trips to the library, museums, parks, and similar places
- Other (please indicate)

If you could re-allocate space on the road for the following user groups, circle the action you would take:

Space For	Less Road Space	Same Road Space	More Road Space
Cars	<input checked="" type="radio"/>	<input type="radio"/>	<input type="radio"/>
Pedestrians	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
Cyclists	<input type="radio"/>	<input type="radio"/>	<input checked="" type="radio"/>
Buses	<input checked="" type="radio"/>	<input type="radio"/>	<input type="radio"/>

The following page refers to the ideas presented at the Kachemak Drive Open House:

1. Half Mile Trail from base of the spit to the airport access road.

Do you have any comments or concerns about this option?

I LIKE IT CAN BE A NICE WALKABLE/BIKEABLE RECREATIONAL ROUTE.

Do you support this option? Why or why not?

YES, VERY APPEALING GREAT EASY WALK CLOSE TO SPIT PARKING, A WALKABLE ACCESS TO GRAVEL BAR - FOR BIRDING

2. Narrowing width of road on Kachemak Drive and adding widened painted shoulder for bike lane.

Do you have any comments or concerns about this option?

I THINK IT CAN/SHOULD BE DONE, EASY TO ACCOMPLISH, RELATIVELY INEXPENSIVE

Do you support this option? Why or why not?

YES, INCREASES SAFETY, CONNECTS EXISTING BIKER PATHS.

3. Creating a separated recreational trail following current water and sewer right of ways.

Do you have any comments or concerns about this option?

BEST OPTION UNTIL POWER LINE EASEMENT COULD BE UTILIZED.

Do you support this option? Why or why not?

YES, MAKES GOOD USE OF DISTURBED GROUND/PROPERTY

4. Decreasing speed to 25 mph during summer months (defined as non-studded tire months) and making it a "scenic byway" during this time.

Do you have any comments or concerns about this option?

A GOOD OPTION - WOULD BE MORE SCENIC WITH 2-3 POCKET PARKS/COURT YARDS

Do you support this option? Why or why not?

YES, INCREASES SAFETY, EASY TO IMPLEMENT

5. Take no action on Kachamak Drive.

Do you have any comments or concerns about this option?

NOT A VIABLE OPTION FOR ME, SOMETHING NEEDS TO BE DONE.

Do you support this option? Why or why not?

NO. IT MEANS GIVING UP, ACCEPTING NO FOR AN ANSWER

In addition, we would appreciate your thoughts on the following questions:

What do you feel are the most pressing issues facing Kachamak Drive?

SPEEDING, LACK OF BICYCLE/PEDESTRIAN PATH

How are the community and the uses of Kachamak Drive changing?

BICYCLE USE INCREASING, VEHICLE USE INCREASING, MORE DEVELOPMENT IN GENERAL

How should we respond to these changes?

SLOW VEHICLE TRAFFIC PROVIDE SAFE OPTIONS FOR BICYCLES + PEDESTRIANS

What do you want Kachamak Drive to look like 10 and 20 years into the future?

A SCENIC BYWAY WITH A MIX OF BUSINESS, INDUSTRY, RESIDENTIAL - ACCESSED BY IMPROVED ROAD/BICYCLE/PEDESTRIAN PATH

4. Please identify any actions on Kachamak Drive that seem easy, affordable and effective.

WEEKLY PLACEMENT OF DIGITAL SPEED CAP, RADIO/NEWSPAPER EDUCATIONAL SPOTS, SHARE THE ROAD SIGNS, SPEED LIMIT ENFORCEMENT, OTHER SIGNS, PEDESTRIAN'S BICYCLES, DOUBLE FAN

5. Please identify any actions on Kachamak Drive that you feel we need to take even though they seem hard, but worth it. These actions may be difficult to achieve or may require a significant investment.

PURCHASE TRAIL EASEMENT(S), PURCHASE PROPERTIES FOR TRAILHEADS, PARKING, SCENIC OVERLOOKS ENCOURAGE BUSINESSES/HOMESOWNERS TO CLEAN UP PROPERTIES.

7/10/11

```

<?xml version="1.0" encoding="UTF-8"?>
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  <TextField4>Homer, AK 99603</TextField4>
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  <Answer/>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer>Never</Answer>
  <Answer>Occasionally, about once or twice a month</Answer>
  <Answer/>
  <Answer>Routine Errands</Answer>
  <TextField1/>
  <TextField1/>
  <TextField2>Ryjil Christianson</TextField2>
  <Question11>No, if anything the speed limit should be lowered during the winter time when it's
    icy. I think the current speed limit is fine. I do think this would be a wonderful spot to make a
    Scenic Byway though. A trail or walking/bike lane should also be added. </Question11>
  <Question10>On Kachemak Drive? I think this would be wonderful. Kachemak Drive is a lovely
    stretch of road to walk or bike. Unfortunately, there is no room right now for walkers and
    bikers to safely travel. </Question10>
  <Question5>What road are you talking about? The base of the Spit or Kachemak Drive?
    Kachemak Drive is already a narrow road. I would be strongly in favor of adding a bike/
    pedestrian lane. But not if it means narrowing the roadway. The road at the base of the Spit
    may be wide enough to accommodate the a bike/walking lane. </Question5>
  <Question3>Extending this trail would be wonderful. Right now there is a narrow point between
    the Airport Access Road and the beginning of the Spit trail. I often feel uncomfortable walking
    or cycling this stretch. </Question3>
  <Question1>I would like to see turn out spots along to road for trucks towing boats. In the winter
    these areas could also be used as parking for people skating on the lake. </Question1>
  <Question2>When the road was paved the traffic speed seemed to increase drastically. This made
    it even more terrifying to walk or bike along the road in some respects. More people have
    discovered the lake as a great skating area in the winter. It is difficult to park though. More
    homes are being built along the roadway. </Question2>
  <Question13>Safety of walkers and bikers! This is a very scary roadway to travel if you are not in
    a car. </Question13>
  <Question6>NO Something should be done on Kachemak Drive. I have never felt safe walking or
    biking on that road. </Question6>
  <RadioButtonList/>
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  <RadioButtonList>1</RadioButtonList>
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  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>

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<RadioButtonList/>

<RadioButtonList/>

<RadioButtonList/>

<Question1>We could lower the speed limit until we build a seprate walking/ biking lane. I would not be thrilled by this but it would be an affordable change. </Question1>

<Question1>put in a walking/ biking lane that is set back from the main roadway and make the road a scenic byway (like the done along East End Road)</Question1>

</form1>

```

<?xml version="1.0" encoding="UTF-8"?>
<form1>
  <TextField3/>
  <TextField4>Homer, Alaska 99603</TextField4>
  <Answer>Downtown Homer</Answer>
  <Answer/>
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  <Answer/>
  <Answer>Downtown Homer</Answer>
  <Answer/>
  <Answer>Never</Answer>
  <Answer>Infrequently; maybe every few months</Answer>
  <Answer/>
  <Answer>Other</Answer>
  <TextField1/>
  <TextField1>Occasionally to get from East End Road to the Spit or vice versa, or to Ocean
    Drive</TextField1>
  <TextField2>Anne Marie Holen</TextField2>
  <Question11>I would love to see traffic slow down but I don't think this is a realistic option.
    Narrowing the lanes would help (with visual cues). Scenic Byway status requires an
    application to Alaska DOT. I have never heard of a seasonal Byway designation. I doubt such
    a short section would be granted Scenic Byway designation. (I know something about this
    topic.)</Question11>
  <Question10>Clearly the best option in terms of non-motorized user experience. Undoubtedly
    more expensive. However, I think we need to quit thinking of non-motorized transportation
    infrastructure as "optional." For years, handicapped access to buildings was thought of as
    "optional" until enough pressure was put on public officials to make it mandatory. It should be
    the same with non-motorized transportation, for many reasons.</Question10>
  <Question5>Not as good/safe/pleasant as a separated trail, but better than current
    situation.</Question5>
  <Question3>I support all trail development in the Homer area, including this
    proposal.</Question3>
  <Question1>It should have a separated bike/walking/running trail along the entire
    length.</Question1>
  <Question2>not sure</Question2>
  <Question13>not sure</Question13>
  <Question6>I am glad that the Parks and Rec Commission has not let this issue (biking/pedestrian
    pathway) die. It would be a shame if that happened.</Question6>
  <RadioButtonList>1</RadioButtonList>
  <RadioButtonList/>
  <RadioButtonList/>
  <RadioButtonList>1</RadioButtonList>
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```


PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

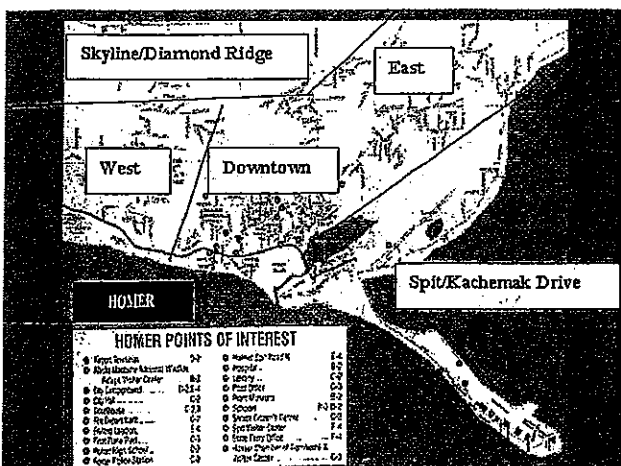
Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

Name (Optional)

Address (Optional)

City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

CARS	PEDESTRIANS	CYCLISTS	OTHER VEHICLES
<input checked="" type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input checked="" type="radio"/> Same Road Space
<input type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input type="radio"/> More Road Space
<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

This doesn't make sense...
 Why would anyone just want to bike to the airport.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

Yes, concern is road is small to begin w/

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

Possibly depending on how far.

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

No! Will just create more traffic.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

No - the shoulder needs to be paved at least!

13. What do you feel are the most pressing issues facing Kachemak Drive?

No bike lane, or at least no paved shoulder

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

Bike commuters are more common - also tourist bike trips create hazard in summer - need safe path for the loop

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

Much the same but w/ bike path.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

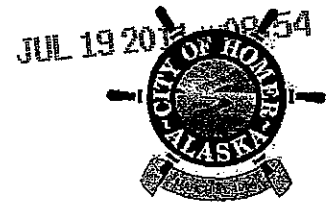
Paving shoulders

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.



PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

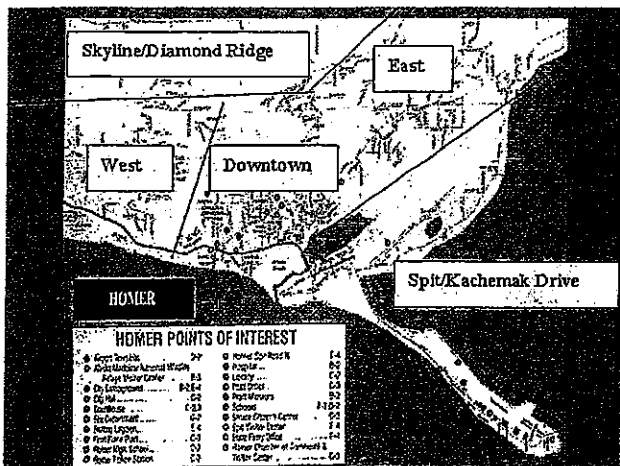
Cowles Council Chambers City Hall
491 E. Pioneer Avenue
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Name (Optional) Tom & Sandy Early

Address: (Optional) _____

City, State, Zip _____



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

running several times per week

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other _____

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other _____

go to boat harbor, athletic club, Starvin Murrins, beach

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|---------------------------------------|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

would be nice as continuation of spit bike trail- Homer needs to add bike paths whenever possible

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

The road is not very wide now- but this would be better than nothing

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

Yes- this would be best

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

No- drivers don't usually observe the speed limit now- but would be better than nothing also

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

Nope- needs something

13. What do you feel are the most pressing issues facing Kachemak Drive?

more use- needs to be upgraded

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

More bikers, runners- they need safer accomodations

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

Wider, with room for everyone

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

Bike/pedestrian path

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW

JUL 19 2011 AM 08:54



Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

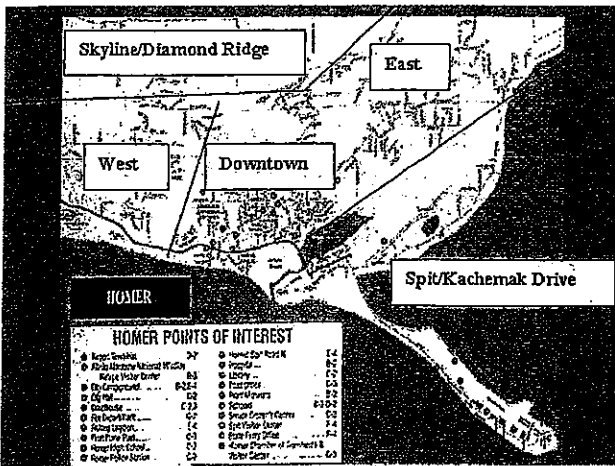
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Bruce Bezon

Address: (Optional) 304 W. Pioneer

City, State, Zip 99603



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike

Other Bird watching

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|---------------------------------------|--|--|---------------------------------------|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

Yeah right!

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

13. What do you feel are the most pressing issues facing Kachemak Drive?

a bike path

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

more cyclists, & also more car traffic w/ little space. Bad mix!

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PEPPER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

Submit by Email

Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW



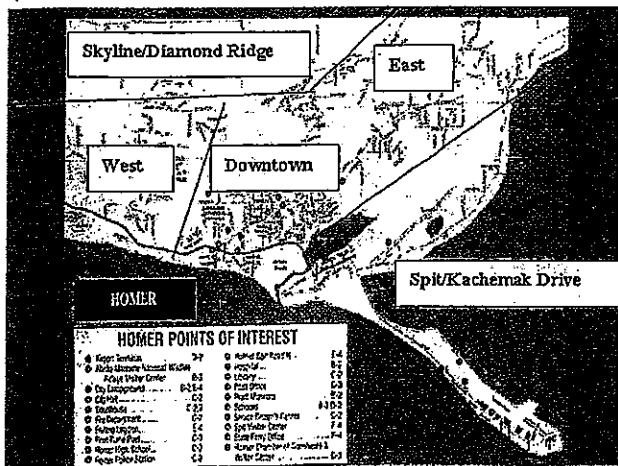
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E. Pioneer Avenue
Homer, Alaska
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Name (Optional) Address: (Optional)

City, State, Zip



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle on Kachemak Drive?

- *CURRENTLY TOO DANGEROUS*
- Never
 - Infrequently; maybe every few months
 - Occasionally, about once or twice a month
 - Regularly, once or twice a week
 - Frequently, several times a week or every day

5. If you ride a bike, which of the following describes why you use it on Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

6. If you drive a car which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

CARS	PEDESTRIANS	CYCLISTS	OTHER VEHICLES
<input checked="" type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space	<input type="radio"/> Same Road Space
<input type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input checked="" type="radio"/> More Road Space	<input type="radio"/> More Road Space
<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input type="radio"/> Less Road Space	<input checked="" type="radio"/> Less Road Space

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option. Do you support this Option? Why or Why Not?
 YES, FOR PEDESTRIAN/BYCECLE ACCESS TO LOCAL/CHARTER AIR SERVICES.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?
 MAYBE, NOT CERTAIN IT WOULD IMPROVE SAFETY FOR ANYONE.

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?
 YES, A PEDESTRIAN/BYCLE PATH IS NEEDED TO COMPLETE THE LOOP BY TIEING EAST END RD TO SPIT TRAILS.

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?
 YES, WOULD IMPROVE SAFETY FOR ALL USERS.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?
 NO

13. What do you feel are the most pressing issues facing Kachemak Drive?
 SAFETY & DRAINAGE.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?
 MORE & FASTER TRAFFIC, ESPECIALLY IN SUMMER - BOAT/TRAILOR TRAFFIC TO BOAT YARD + RVs.

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?
 MORE LIKE PORTION OF EAST END ROAD THAT HAS BIKE PATH.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?
 ?

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.
 SEPARATED BIKE/PEDESTRIAN LANE

YOU CAN MAIL THE COMPLETED SURVEY TO: CITY OF HOMER, KDPC ATTN. RENEE KRAUSE, CITY CLERK'S OFFICE 491 E. PIONEER AVENUE HOMER, ALASKA 99603 OR SUBMIT VIA EMAIL

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Print Form

PROPOSED KACHEMAK DRIVE NON-MOTORIZED PATH

PLEASE HELP OUR EFFORTS BY COMPLETING THE SURVEY
QUESTIONS BELOW.

AUG 30 2011 10:02:06 De



Background: Late last year it was brought to the attention of the Parks and Recreation Advisory Commission that the current state of Kachemak Drive is unsafe for the traffic and recreation occurring there. A committee was formed to look at the safety issues on this road and to come up with creative ideas to increase safety. All four scenarios presented are ideas created and supported by the Committee members but are not to be considered concrete plans. Your input will help the Kachemak Drive Pedestrian Bike Path Committee present recommendations to the Parks and Recreation Advisory Commission and ultimately City Council for further action, if any, in the future.

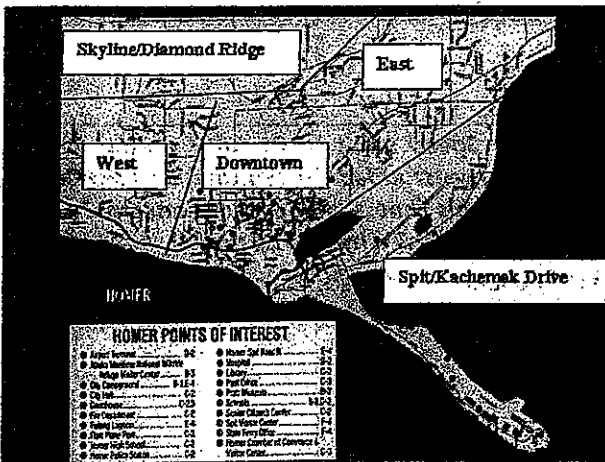
Kachemak Drive Pedestrian Bike Path
A Committee of the Parks and Recreation Advisory
Commission

Cowles Council Chambers City Hall
491 E Pioneer Avenue
Homer, Alaska
www.cityofhomer-ak.com

Name (Optional) Kevin Walker

Address (Optional) 59975 Glden Plover

City, State, Zip Kachemak City, AK 99603



1. Please refer to the map and indicate the area of Homer where you live.

- West Homer
- Downtown Homer
- East Homer
- Kachemak Drive and Spit
- Skyline/Diamond Ridge

2. Referring to the map, indicate the area of Homer where your most frequent destination is located (workplace, office, school, etc.)

- West Homer
- Downtown Homer
- East Homer
- Skyline/Diamond Ridge
- Kachemak Drive and Spit

3. How often do you ride a bicycle or walk along Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

4. How often do you drive a car on Kachemak Drive?

- Never
- Infrequently; maybe every few months
- Occasionally, about once or twice a month
- Regularly, once or twice a week
- Frequently, several times a week or every day

5. If you ride a bike or walk, which of the following describes why you use Kachemak Drive. If more than one please note in "Other".

- Regular Exercise or Workout
- Family Outings or touring
- Commuting - Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- I do not Ride a Bike
- Other

6. If you drive a car, which describes why you use Kachemak Drive. If more than one please note in "Other".

- Commuting to Work or School
- Routine Errands
- Trips to the Library, Museums, Spit, Parks, etc.
- Other

7. If you could re-allocate space on the road for the following user groups, circle the action you would take:

- | CARS | PEDESTRIANS | CYCLISTS | OTHER VEHICLES |
|--|--|--|--|
| <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space | <input type="radio"/> Same Road Space |
| <input type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input checked="" type="radio"/> More Road Space | <input type="radio"/> More Road Space |
| <input checked="" type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input type="radio"/> Less Road Space | <input checked="" type="radio"/> Less Road Space |

8. Half Mile Trail from the Base of the Spit to the Airport Access Road. Comments and Concerns about this Option.

Do you support this Option? Why or Why Not?

Is this the beach trail option? This is a good pedestrian option, but only mountain bikers with lots of time would find cycle on this trail. I would strongly support a paved or crushed gravel surface parallel to the road - this is a very dangerous section as cars are going fast with limited visibility over the hill, and uphill bike riders may be going very slow and weaving a bit.

9. Narrowing the Width of Road and Adding Widened Painted Shoulder for Bike/Walking Lane. Comments or concerns about this Option? Do you Support this Option? Why or Why Not?

I strongly support this option as an immediate, low cost solution where the right of way and topography permit.

10. Creating a Separated Recreational Trail following Current Water and Sewer Easements or Right of Ways. Comments or Concerns? Do you Support this Option? Why or Why Not?

This is an excellent long term solution. Act fast, the easements on the west end are dug up and construction equipment is on site!

11. Decreasing the Speed Limit to 25 MPH during Summer Months (defined as non-studded tire months) and Making it a Scenic Byway during this Time. Comments or Concerns? Do you Support this Option? Why or Why Not?

I support this if it could be enforced. Making 9.5-10' lanes would help slow down traffic.

12. Take No Action on Kachemak Drive. Comments and Concerns? Do you Support this Option? Why or Why Not?

I travel this road frequently, both on a bike and by car. There is often a lot of car / truck traffic and they are going too fast. I often see 4-8 bicyclists on my trips along Kachemak Drive. I can not support no action.

13. What do you feel are the most pressing issues facing Kachemak Drive?

Safety, safety, and safety.

14. How are the Community and the Uses of Kachemak Drive changing? How should the City respond to the changes?

With the rising price of gas, more people are riding bikes.

15. What do you want Kachemak Drive to look like in the next 10 or 20 years?

I would like to see a separate, paved bike and pedestrian trail, similar to the spit trail. I would discourage an adjacent trail where possible, as the proximity of speeding cars and trucks detracts from a pleasant commuting / traveling experience.

16. Please identify any actions on Kachemak Drive that seem easy, affordable and effective?

Take advantage of the current construction activity at the west end of Kachemak Drive to eliminate mobe - demobe costs. Repaint the lines to allow up to 3' on each side for a paved shoulder for bikes and walkers.

17. Please identify any actions on Kachemak Drive that you feel the City needs to take even though they appear hard but worth it? These actions may be difficult to achieve or may require a significant investment.

Acquire necessary easements for trails under the power lines and over the sewer and water lines.

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VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE PRELIMINARY ENGINEERING PLAN FOR KACHEMAK DRIVE PATH TO CITY COUNCIL AS AMENDED.

There was no discussion.

VOTE. YES. LILLIBRIDGE, BRANN, BREMICKER, LOWNEY.

Motion carried.

Chair Bremicker read the Funding plan into the record and opened discussion on the item.

BREMICKER/LILLIBRIDGE - MOVED TO INSERT TRUCKING IN THE THIRD PARAGRAPH BEFORE GEOBLOCK, INSERT THE LAST NOTE BEFORE OTHER SOURCES OF FUNDING AND ADD OTHER POSSIBLE SOURCES OF FUNDING AS DISCOVERED OR AVAILABLE.

There was a brief discussion on including trucking since it was one of the highest costs in a project, deleting the reference to hiring a contractor. It was noted that the trail will not necessarily be built all at one time and may be built in phases and designating crossing(s) for the path.

VOTE. YES. NON-OBJECTION. UNANIMOUS CONSENT.

Motion carried.

BRANN/LILLIBRIDGE - MOVED TO FORWARD THE AMENDED FUNDING PLAN FOR KACHEMAK DRIVE PATH TO COUNCIL FOR APPROVAL.

There was no further discussion.

VOTE. YES. BREMICKER, LOWNEY, LILLIBRIDGE, BRANN

Motion carried.

Chair Bremicker introduced and read the Maintenance plan into the record and opened discussion by commented on dragging the path versus plowing the path during winter.

A discussion ensued on ownership of the path would depend on the location which would be easements on private property so the trail would be the city but the land is the property owners. The city would have the easement and maintain the path.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE MAINTENANCE PLAN FOR KACHEMAK DRIVE PATH TO CITY COUNCIL.

There was no further discussion.

VOTE. YES. BRANN, BREMICKER, LOWNEY, LILLIBRIDGE.

Motion carried.

The Commissioners agreed to schedule a Special Meeting April 10, 2012 at 5:30 P.M. in the Conference Room Upstairs at City Hall. The content of the agenda to focus on Karen Hornaday Park – What and Where Do We Go from Here?

The commission requested the presence of the following staff and vowed to take up only 30 minutes of their time acknowledging the constraints of the departmental budget restrictions: Public Works Director, Recreation Coordinator, Parks Maintenance Coordinator, Julie Engebretsen in Planning and if the City Manager would like to attend his input would be greatly appreciated. The commissioners had questions on the budgeted work items when they would be scheduled, application and funding for the trails within the park especially the Woodard Creek trail, engineering and items approved last year.

C. Discussion and Recommendations on the Kachemak Drive Proposed Non-motorized Pathway and Revised Resolution

Chair Bremicker introduced the item by reading of title then recommended that the commission review and address each item - Resolution, Preliminary Engineering Plan, Funding Plan, Maintenance Plan separately.

Chair Bremicker then read the Revised Resolution 11-090 into the record.

BREMICKER/BRANN – MOVED TO CHANGE LAST TWO LINES IN THE LAST PARAGRAPH TO READ "...THE FOLLOWING WAYS:

1. INCREASING THE USAGE OF SIGNAGE WARNING DRIVERS OF BICYCLES AND PEDESTRIANS ON ROADWAY.
2. BUILD A SEPARATED PATH PARALLELING KACHEMAK DRIVE USING UTILITY EASEMENTS AND PUBLIC PROPERTY.

There was a brief discussion on a proposed revision by Ms. Cumming a member of the committee. Staff was unable to locate the submitted document at the time for review by the commission and was not present. Commissioner Brann commented that he believed she was addressing a change to "contingent on available funding but the remaining committee members were satisfied with the changes made.

VOTE. YES. LOWNEY, LILLIBRIDGE, BRANN, BREMICKER

Motion carried.

BRANN/LILLIBRIDGE – MOVED TO FORWARD THE AMENDED RESOLUTION 11-090 TO COUNCIL FOR APPROVAL.

There was a brief discussion on the clarity of the resolution was adequate.

VOTE. YES. BRANN, BREMICKER, LOWNEY, LILLIBRIDGE

Motion carried.

A brief discussion on the Proposed Preliminary Engineering Plan ensued. The Commissioners requested colored maps for City Council, it was noted that these are generalized, not detailed.

BREMICKER/BRANN – RECOMMEND ADDING ON THE GENERAL NOTES PAGE CONSTRUCTION OF THE PROPOSED PATHWAY BE CONDUCTED IN SUCH A MANNER THAT IT CAN BE UPGRADED TO A HIGHER LEVEL OF TRAIL INCLUDING PAVING AND DELETE THE LAST SENTENCE.

A brief discussion on the time frame for submittal to council at the same time as the resolution.

STAFF AND COUNCIL REPORTS/COMMITTEE REPORTS/BOROUGH REPORTS

A. Community Recreation Report – Mike Illg

Mr. Illg reported that they are working with some groups to sponsor some recreational programs for youth, and requests to use the HERC for private parties.

B. Spit Parks and Recreation Committee Report – Tricia Lillibridge

Commissioner Lillibridge distributed a Draft Flyer for the proposed "Meet the Trucks" event at Mariner Park and the next Spit Parks and Recreation meeting is scheduled for February 28th, 2012 at 3:30 p.m. in the conference room at City Hall. She welcomed the audience to attend as they were looking for additional members for the committee.

C. Kachemak Drive Path Committee Report – Dave Brann

Commissioner Brann reported that there was still work being done on preparing for a path design and information packet that he is working on with Kevin Walker; he is still pursuing signage along Kachemak Drive and permission to cross airport property. He is working with the City Manager on getting letters sent to the Property Owners regarding input on the proposed path. He has had a teleconference with Ms. Biloon on the proposed path.

D. Karen Hornaday Park Committee Report – Robert Archibald

Commissioner Archibald reported that at the recent meeting the discussed the improvements and needs of Little League, Vice Chair, Ralph Broshes was elected; discussed the Camp Host Program that there are negatives and positives; the Committee passed a recommendation which is included in the packet tonight, that there should be no additional improvements until engineering and design services are procured specifically for the road, parking, creek trail and drainage issues directly affecting those areas. The committee did discuss funding and the participation of all the user groups it was noted the funding is out there and managing the parks takes money;

Commissioner Lillibridge requested the Little League submit their stats on letterhead so it can be used to by the commission in support of the needed funding.

Commissioner Lowney recommended a copy of the Park Master Plan be displayed on a bulletin board during the HoPP build week, this will let the public know what needs to be done, what benefits there are, and what more is planned for the park.

PUBLIC HEARINGS

No public hearings were scheduled.

PENDING BUSINESS

A. Recommendation to City Council to Establish a Campground Host Program

Chair Bremicker introduced the item into the record.

The commission entertained a brief discussion on the benefits of having this program and getting it implemented this year at Karen Hornaday Park.

There was a brief discussion on the appearance that it was owned by the adjacent property owner and that no contact was made with the owner. The commission thanked him for bringing this idea forward. Mr. Aderhold offered any assistance he could provide if the commissioners decided to look further into this idea.

There was no further discussion.

B. Miranda Weiss, Homer Playground Project (HoPP)

Miranda Weiss, acknowledged members of the group and their roles they have within the project. Ms. Weiss stated that the group intends for a long term commitment, that as a whole they are supportive of the entire Master Plan for the park, and HoPP is supportive of ongoing maintenance. Ms. Weiss further explained the enormity of the proposed playground project and what the recommendation from the commission to City Council would provide for the proposed project and community alike. She emphasized that Build Week would be a kick-off for implementing the Master Plan and it would be an asset to the commission in the future to be in the forefront of this project today.

There were no further questions or comments.

RECONSIDERATION

There were no items scheduled for reconsideration.

STAFF AND COUNCIL REPORTS/COMMITTEE REPORTS/BOROUGH REPORTS

A. Community Recreation Report – Mike Illg

There were no discussions or questions on the information provided in the packet.

B. Spit Parks and Recreation Committee Report – Tricia Lillibridge

Commissioner Lillibridge reported that the committee has not met due to personal schedules and the holidays. She explained to those present the purpose of the committee and named the current recreational areas and parks that are located on the Homer Spit and pointed out a few issues with some of the more used areas.

There was a brief discussion on relocating the access to Marina Park due to line of sight issues and that Carey Meyer is currently drafting some numbers on relocating the entrance.

Commissioner Lillibridge announced the plan to hold a Meet the Trucks event on May 5, 2012 at Mariner Park which will be discussed in more detail at the next meeting.

C. Kachemak Drive Path Committee Report – Dave Brann

Commissioner Brann reported the progress made so far by the Committee in getting a proposed pedestrian bicycle path along Kachemak Drive. He has received a call from AKDOT personnel and will be talking with the City Manager on that discussion that was held.

D. Karen Hornaday Park Committee Report – Robert Archibald

Commissioner Archibald reported that the committee has met and discussed several issues and recommended removal of the red shed and several other issues that are before the Commission this

There was no further discussion.

F. Kachemak Drive Path Committee Report

Chair Bremicker reported on the progress of the committee during the last month. He stated that they had a visitor at the most recent meeting, Lynn Whitmore who represents the Moose Habitat and were concerned what the committee was proposing and what the design would be through the property. Chair Bremicker stated that the information and advice that the committee received from Mr. Whitmore who is experienced with the different state agencies was very helpful and especially the best types of paths to put through that property that will not deter or hamper a moose passage through the area.

It was noted that the committee made progress on defining the proposed path and where it should be located along Kachemak Drive. They have received very helpful advice and efforts from Kevin Walker also. An option that was discussed was to use an established easement for a road that goes from East End Road in front of Redden Marine to Kachemak Drive. The committee has reached consensus on the proposed location and the next steps will be the design cross sections and funding.

The members of the committee responded to questions from the commission on portions at the easterly end of the proposed path and connection to the beach access.

The committee will have a meeting on November 22, 2011 at 5:30 p.m.

There was no further discussion.

PUBLIC HEARINGS

No public hearings were scheduled.

PENDING BUSINESS

A. Transient Camping Along Trails in City Limits – Discussion and Recommendations

Chair Bremicker opened discussion by asking clarification on the Staff recommendation.

A lengthy discussion covered the following topics:

- offering a place where someone could camp for free for a limited time
- to prevent garbage and debris being left behind from transient campers

It was noted that the information provided by Ms. Krause did contain several simple suggestions such as clearing brush and shrubbery to make the problem areas more open to visual inspection, bring problems to the attention of local police enforcement, etc. It was discussed in depth that the police department needs to respond and react to complaints that are filed by residents. This is an issue that affects all areas of trails and the like. Including advertisement or notice encouraging reporting incidents of unlawful camping in a resolution forwarded to council.

BRANN/ARCHIBALD - MOVED TO DIRECT STAFF TO SEND A MEMORANDUM TO THE HOMER POLICE CHIEF MARK ROBL THROUGH THE CITY MANAGER WALT WREDE TO ENFORCE CITY CODE, 19.08.030 AND 19.08.070 AS APPLICABLE TO THE AREAS NOTED FOR TRANSIENT CAMPERS AND THE ASSOCIATIVE PROBLEMS AND TO INSTALL ADDITIONAL SIGNAGE PROHIBITING CAMPING.

There was a brief discussion.

BREMICKER/BRANN - MOVED TO OFFER FRIENDLY AMENDMENT, AND TO ISSUE PUBLIC SERVICE ANNOUNCEMENTS ENCOURAGING RESIDENTS TO REPORT ILLEGAL CAMPING TO THE POLICE DEPARTMENT AND MAKING SURE OFFICERS FOLLOW-UP ON THE ALLEGED VIOLATIONS.

concern Commissioner Cumming expressed regarding work to be completed in the area of the catch basins and that this work has been completed. The contractor did a great job too. He stated that the preliminary engineering to establish the cost of the road relocation would be completed in November, he cited a "cup runneth over" the past few months but assured the commission that he felt he was capable of performing the valuation and there was no need to expend monies unnecessary for engineering work to be done by an outside firm. Commissioner Cumming commented on terracing the parking lots. Mr. Meyer did acknowledge that there would be more earth-work required in relation to the road but that is not the most expensive component of the work required and he believed that there was enough talent with Public Works to handle that aspect of the job.

Commissioner Lilibridge would like to propose a recommendation to remove the red shed at Karen Hornaday Park. She stated that the committee discussed it during the site visit and the HoPP committee has reviewed the possible uses and voted that it is not needed and it needs to be removed this winter if possible but definitely before build week in May and re-purposed as the city sees fit.

Mr. Meyer agreed that they have reviewed what is being stored and if any of the items can be relocated or permanently disposed of and they are in the process of looking for a group that would be interested in the wood from the building. He did not believe that they needed a recommendation from the commission but if it came down to needing a minimal amount of funding approved then it may be good to have the support of its removal from the commission to include with the request from council.

There was a brief discussion on the possible uses and how it would distract from the beautiful new design being created for Karen Hornaday Park playground. There were further comments on showing the state that they are spending the money but that the City has something to show for the work.

Commissioner Archibald thanked Mr. Meyer for coming to the meeting and clearing the air over a few issues. He further noted for the record that he was supportive in re-purposing the red shed if possible.

Mr. Meyer praised Ms. Otteson for her efforts and work done since assuming the position of Parks Maintenance Coordinator.

There was no further discussion.

C. Kachemak Drive Path Committee – Commissioner Harrald

1. October 6, 2011 Meeting Minutes
2. Draft property Owner Notices
3. Memorandum and Attachments on Recommendations from Planning Commission
4. HEA Response

Commissioner Brann spoke about addressing the transient camping along the portion of the Mud Bay Trail along Kachemak Drive.

Commissioner Cumming reported that the committee is arriving at the point that a difficult decision requires a vote. She hopes that the other commissioners will comment on her comments. She was concerned about the content of another resolution submitted to City Council. Chair Bremicker noted that the committee had an in-depth discussion on narrowing the lanes and it was determined that due to public input this option was not recommended and will not be included on the revised resolution.

Chair Bremicker stated that there was a consensus in the committee that an off road trail was needed along the road. He stated that they had the Finance Director attend the last meeting and explain the process pertaining to use of HART funds and the amount that may be available at the end of 2012. Chair Bremicker noted that there is two separate funds – one for roads and one for trails.

The discussion by the committee centered on the final points to be addressed in the revised resolution which will be the following:

- What do we need to build?
- Where do we need to build it?
- How much do we spend to build it?

It is hoped that there will be a specific plan with specific numbers and funding to present to this commission and city council.

Commissioner Brann reported that the committee also discussed about drafting letters to be distributed to the property owners that will be affected by the proposed trail to see how many would be for or against the proposed project. Ms. Krause responded to the commissioner's inquiry that the letter once agreed upon by the commission will have to be forwarded to the City Attorney for review and drafting the formal document that can be distributed.

Chair Bremicker requested the sample letters to be reviewed by the committee since not all the members have seen it to comment on and then at the November meeting the commission can put forth their recommendations and then forward to the City Attorney.

Commissioner Cumming inquired when they can start re-working the resolution. Ms. Krause stated that the information included in the packet has not been before Council however in the meantime before officially being remand from City Council. It was commented that it may be as late as January or February before resubmittal to City Council. Further observations were made on the time that has been expended on this committee and the task they have undertaken.

Commissioner Archibald left the meeting due to a prior commitment at 7:30 p.m.

Commissioner Brann responding to Commissioner Lillibridge' question regarding the placement of the path along Kachemak Drive stated that there are still discussion on all options; because of the work being done with the sewer and water easement currently in progress would be ideal but there are several proponents to use that easement including property owners permission; conservation issues; feasibility of placement. The trail will be constructed from one end to the other but it may be along different avenues for different sections. That is one of the reason to sending a letter to the property owners so that they can voice concerns they may have regarding the trail. He further noted that Homer Electric Association has no objection to a trail placed in the utility easement.

Commissioner Brann briefly explained the plans to establishing the Mud Bay Trail and where it would travel. He briefly described the path starting at the end of the existing Spit Trail progressing up the hill; once on top of the hill there has been cleared out a path to the airport access road. He described further more details in placement and thoughts on locating portions of the trail from the portion of the path known as Mud Bay Trail up to the Bay Club. He commented that this portion may be pedestrian friendly only.

There was no further discussion.

D. Parks & Campgrounds Report – Angie Otteson

Ms. Otteson reported that camping season was closing at the end of the month. She said that the revenues were down for this year compared to 2010. The revenues collected were:
\$125,818.00 Spit Campgrounds
\$19,390.00 Karen Hornaday Park Campground

Ms. Otteson stated what she believed some of the circumstances causing the shortages were economy, lack of fish at the Fishing Hole, and the weather was not the best. There were brief comments about dealing with the conditions of the Fishing Hole. The last of the seasonal workers are done and completing

Commissioners commented on the efforts and work done at Karen Hornaday Park by the various groups and organizations. Commissioner Cumming gave a detailed explanation on the history and efforts by the Friends of Woodard Creek; the effect of the fill that was dumped on the one parking area; the requirement to attend to the priorities as already outlined for the park; the monies as promised by various groups and persons for the betterment of the park. She further elaborated on the hundreds of hours also put into the plan for the park. She was afraid that the funds would be nickel and dimed away.

Chair Bremicker questioned the monies promised by the Little League and if it may be "in kind" services.

There was \$9,000.00 being held by the Kachemak Bay Conservation Society according to Commissioner Cumming which encompassed the funds "promised" by Little League.

There was more discussion on the use of funds currently available for work on Karen Hornaday Park. Chair Bremicker asked if this discussion could be postponed until the October meeting.

There was no further discussion.

C. Kachemak Drive Path Committee – Commissioner Harrald
1. Synopsis of the September 8, 2011 Meeting (Laydown)

Commissioner Harrald did not attend the last committee meeting she reported the response on the Resolution presented at the meeting on Monday by the members of City Council. She personally spoke to many members to get their reasoning for not approving the resolution was due to the wording included "Action".

Chair Bremicker reported that he spoke with Julie Engebretsen in Planning since City Council remanded this resolution to the Planning Commission. He opined that they gave the Council too many options and that the Commission should present to Council one plan for approval. He was told that once a project has been approved by the State it still takes 5-9 years to complete. He opined that HART funds should be used to complete the trail, they have a revenue source, and he believes that the community is agreeable that this trail is a priority. Ms. Engebretsen stated that a bond was not needed that the City could even get a loan. He believed that a single plan design option etc., real clear specifics that council can vote on. Commissioner Harrald agreed but the one red flag she heard was the council being very protective over the HART funds and something about using those funds to extend Waddell Avenue. She stated that they will have to be really clear on the funding options.

Chair Bremicker explained that the HART Funds covered Roads and Trails. There was specific percentage for roads and separate percentage for trails. The Reber Trail was built with HART funds. Chair Bremicker stated that if they decide to use the funds for 10 years to pay off the trail along Kachemak Drive, well with Council approval. Commissioner Cumming noted a report by Lynn Burt regarding a review of the HART funds that could be done annually.

Ms. Krause outlined the steps that will need to be taken regarding the Resolution that was presented to Council on September 12, 2011 for the proposed path/trail along Kachemak Drive.

Ms. Krause called a point of order since the discussion really should be continued in committee not commission and was more appropriate for the committee to have this discussion regarding the proposed resolution.

Ms. Krause will extend an invitation/request to Ms. Mauras, Finance Director to attend the October 6, 2011 committee meeting to explain how the HART funds are used, etc. Ms. Krause will include a copy of the HART in the next committee packet.

Commissioner Harrauld reported that comments received from cyclists feel that everyone's solution to safety is to get them off the road and the reality is they belong on the road and it should be addressed in a safe manner.

Commissioner Brann stated that two recommendations from the committee that will be presented at the next commission meeting. One recommendation was to install Share the Road signage and the Radar Feedback Speed signage. This will aid in increasing awareness and following the speed limit.

There was no further discussion.

D. Parks & Campgrounds Report – Angie Otteson

Ms. Otteson was unable to attend this meeting as planned. She related that she did not have anything of dire necessity to speak about that could not wait until the October meeting.

There was no further discussion.

E. Community Schools & Recreation Report – Mike Illg

Mike Illg reported that he recently attended the Alaska Recreation and Parks Conference in Seward, Alaska. This encompasses all facets of recreation from maintenance to citizen volunteers. It offers a great opportunity to network with others, however it does show him how far behind in recreation that Homer is. He strongly supported and recommended the commissioners attend a conference. They offer scholarships and you can also volunteer to help at the conference. Mr. Illg reported that the conferences for the next three years will be 2012 Anchorage; 2013 Juneau; 2014 Unalaska; 2015 Fairbanks and he has suggested Homer for 2016. He noted that there is a lot of positive ideas and interaction. He will be attending the quarterly meeting with the School Board on using their facilities with the City Manager. He will keep them updated. They have some new activities to offer table tennis, Zumba dance class, herbalist classes, and youth running program and possible running club soon. He has a strong solid support of the camp host program and believes it will help protect against vandalism.

In response to a question from Chair Bremicker he informed the commissioners that the conference is normally held in September normally after moose season.

Commissioner Cumming inquired if there was any idea on the time frame a campground host would be needed at the park. Mr. Illg did not consider a time frame in the prospect of having a camp host he was just expressing his support of the concept and figures the commissioners can flesh out the idea under the pending business item.

In response to a question about drainage Mr. Illg responded that Commissioner Cumming would have to check with Mr. Meyer or Angie Otteson.

Chair Bremicker stated that Commissioner Harrauld's inquiry was out of order regarding the award of a contract on drainage.

Mr. Illg stated that the benefits attending the conference for the commissioners would be very good. In response to a final question on the status of the HERC building and he stated there is currently no update on the status of the building. He would really love to have the building for the community recreation program. He will keep them informed. Mr. Illg left the meeting at 6:23 p.m.

There was no discussion.

F. Staff Report – Renee Krause

Ms. Krause inquired if there were any questions. She described the progress with the work being done on the City Hall Renovations. The Budget is on her Staff Report and under the section for Parks and Campgrounds.

Minutes 22 March 2012, K-Drive Path Committee

Conference Room Upstairs at City Hall

Attending: Bumpo Bremicker (chair), Dave Brann, Dave Clemens, Beth Cummings, Lindianne Sarno (recording), Mike Stockburger (new member); Absent: Lynn Burt

Call to order, 5:40 p.m.

Dave Brann moves; Beth seconds approval of agenda, agenda approved.

Dave Brann moves Beth seconds approval of minutes, with this addition: regarding Beth's fundraiser idea, Dave Brann has doubts (1) because of what happened at Karen Hornaday Park, (2) is concerned that privately raised petty cash would be used for items that should be paid from HART funds. Minutes approved.

No public comments, no reconsideration, no visitors, no committee reports, no hearings.

Pending Business: Continued Discussion and Planning. Dave Brann hands out and explains the draft path maintenance plan (see attached handout). Summer and winter maintenance differ. Pack snow or plow it? Dave Clemens advises winter bicyclist's bike on studs. Path in winter will be multi-use: ski, snowshoe, bike, run. If plow in winter, melts quicker in spring. Regarding maintenance, we need to research: who owns the trail? The City? Don't make an assumption. Trail would be maintained at a high level, like a multiuse trail. City plows East End Road path, which is paved. K Drive Path will not be paved and plow vehicle could deform gravel structure.

Dave Brann is working on proposed funding plan. HART funds (Homer Accelerated Roads and Trails), \$304,554 in that account. \$100,000 added annually. Can be used for initial survey, materials, engineering, building, hiring contractor. Bond issue not needed, since HART funds are a reliable income stream, City can borrow against that and repay over time. STIP estimate was \$35-40 million, way too high. K Drive Path Committee is asking for city staff time, not funding. Other potential sources: Homer Foundation, donations, fundraisers, STIP, grants.

We describe path to Mike to get him up to speed. Mike owns Homer Boat Yard on Kachemak Drive and drives heavy rigs on K Drive frequently.

Packet that will go to Parks and Recreation and City Council: Make sure pages 57, 58, and 59 are removed from packet.

Private landowners on Kachemak Drive: will need each individual's permission. City Manager Walt Wrede will write the letter asking for right of way. . It's on his desk.

We agree to lobby City Council. Beth Cummings > Beth Wythe, Dave Brann > David Lewis, Dave Clemens > Mayor Hornaday, Lindianne Sarno > Brian Zak, Mike Stockburger > Barbara Howard, Bumpo Bremicker > Barbara Howard. We will wait till we have the whole packet to complete our lobbying assignments.

Walt Wrede joins us.

Beth Cummings wants to see this summer a retaining wall planned at the west end of the proposed trail, cut trees, get plans going.

Walt comments that Dave should bring a plan. Dave plans this summer to concentrate on making Mud Bay Trail a four foot wide walkable path.

Beth: Mud Bay Trail is inappropriate for bicyclists, OK for pedestrians. Find a way to build a terraced walkway or retaining wall on south side of drive. Also, have a bridge made beyond the west end of Aviation Leasing, over the culvert.

Bumppo: will need real engineering on that section of trail.

Dave Brann: This summer, signage. Small speed feedback signs. Need DOT permission? Letter from city not necessary. Dave will photograph existing signs and include with map to DOT. Share the Road program.

Digital signs are \$3,000 each, differing degrees of information can be harvested, depending on cost: count vehicles, record speeds.

Signs go up on Mud Bay Trail when snows melt. "No Camping." Sign on two 4 x 4 posts, "Mud Bay Trail." Lynn and Beth will arrange this.

We choose meeting dates: April 5, May 24 (no Dave Brann), June 14.

Comments: Thanks to Mike Stockburger for joining the committee. We are making good progress.

Adjourn: 8 p.m.

Kachemak Drive Path Committee

Thursday, February 23, 2012, 5:30 p.m.

Bumppo Bremicker, Chairman, Beth Cumming, Lynn Burt, Lindianne Sarno, recording, Dave Brann, David Clemens, Kevin Walker, visitor.

Motion to Approve Agenda: Beth moves to put New Business B. before New Business A. No objections, we approve this change. Beth feels it's important to get our resolution done and ready to go to Parks and Rec.

Dave Brann moves to approve, Beth Cumming seconds, no objections, approved.

Motion to approve minutes: Beth moves. Dave seconds, no objections, minutes approved.

Public comments: Kevin Walker, when shall we discuss my packet of info? Beth thinks this packet should accompany the resolution to City Council, and it will be discussed then, including Kevin in the discussion.

Pending Business: Continuing Discussion and Planning for Path Design, page 7. Kevin Walker at the microphone. Regarding providing public with access to and from the airport for pedestrians who get off planes. Kevin's letter to Jennifer Bailey cleared up; we are not proposing to trespass on airport property, but rather to provide safe non-motorized transportation corridor for people coming to and from the airport. Dave Brann reports that Jocelyn Biloon and Scott Thomas, say DOT is working on this. They recognize there is a committee, and Dave is their contact. Walt Wrede, City Manager will get in touch with Ms. Biloon regarding a traffic count. Jennifer Bailey is our contact at DOT Public Transportation Facilities, Aviation Leasing. She is in Anchorage. Kevin Jones is the airport manager.

Bumppo points out that if we are in the DOT right of way we have to follow their requirements, regardless of whether DOT is paying for it. (1) Survey (2) Engineered drawings (3) ADA design specs (4) erosion control (5) contract with DOT to provide maintenance (6) liability and insurance coverage. City of Homer sponsorship is crucial, especially for ongoing maintenance.

Dave Brann points out most of the easements are private property easements. They are not a big stumbling block, especially with approval of City Council. Utility easements are outside DOT right of way. Until we survey each section on the ground we won't know for sure but huge section of road is outside DOT right of way.

Dave Clemens: suggests we send DOT a package about the DOT right of way on the sections that are in DOT right of way. Kevin Walker says Jennifer Bailey says this is already in DOT planning. Next step is contacting the City Manager, the City Manager contacting state planning, and getting back to us.

Bumppo: what we need is for city to officially to buy into this and deal with DOT.

New Business: page 67, reformulating the Resolution on the Kachemak Drive pathway. City Council sent it back to Zoning and Planning, who sent it back to us. It was too muddy last time.

Add to the resolution; include a funding plan mentioning that there will be \$305,000 by the end of 2012.

Beth suggests we send to the city this resolution, the Kachemak Drive Path specifics packet created by Kevin Walker, and a funding plan.

FUNDING PLAN: the city can borrow money at good rates, against the HART money, need not be bonded, borrow against the revenue source for 10 years. Julie Engebretsen, Planning Department, says this is a common practice. Funding plan would include private donations and HART funding. Page 95 of our packet gives an outline of the Planning and Approval process.

WHAT WE MUST ACCOMPLISH: Bumppo: We want City Council to direct city administration to start focusing on this pathway. Dave Brann commented, to direct city staff to do what they can to keep this moving along. Give the city a plan for maintenance. Bumppo interjected that we still need to contact landowners, how will they feel about this. Dave Brann: this is on top line of things to talk about with the City Manager.

Beth: Two City Council members commented, "We want to know what you have in mind."

DOCUMENTS WE NEED: (1) resolution (2) packet from Kevin is ready. Complete these two items tonight. Then Renee can advise us on form of (3) funding plan (not ready) and (4) a letter to private landowners will come from the City. (5) Maintenance plan.

We revise the resolution. See resolution on next page.

Beth moves that we approve Resolution 11-090 as revised and send it on to Parks and Recreation and the City Council; Dave seconded. Unanimous approval. See resolution on next page.

Bumppo moves that we assign a council member to each committee member, and well in advance of the City Council meeting bring to them the packet including: resolution, Kevin Walker's packet, and funding plan. Dave Brann says we'll not have a projected cost until the survey. Dave Clemens suggests we mention that we've scaled back from the East End Bike Path to less than \$1.5 million per mile.

Beth proposes changing our Committee's mission statement: "to build a safe non-motorized separate path along Kachemak Drive, connecting East End Road to Spit Road via the airport. Kevin's packet is called our preliminary engineering packet.

Beth proposes a fund-raising event to raise \$5,000 to \$10,000 petty cash. She has engaged a professional event producer, Sally Oberstein, to produce the event, and is proceeding with these plans as a private citizen. Funds would be used for chain saws, Surveying, signage, and other uses.

Dave Clemens mentions that the Homer Cycling Club approves increased signage to increase safety on Kachemak Drive for those commuting cyclists who will continue to use the road. If Kachemak Drive is improved, the cycling club would support widening the shoulder for the safety of commuting cyclists.

Beth wonders if the Mud Bay Trail is included. We see the Mud Bay Trail is included in Kevin Walker's pre-engineering packet.

We discuss the plastic walkway and decide a request has gone to the city for the walkway.

Report on December 10 Field Trip: Dave Brann said it was valuable, going from the airport lot, down, looked at possibility of cut-off trail. It will be challenging. Walked along the beach to the road. Site visitation was valuable. Clarified and provided yellow line and blue line on the pre-engineering packet. Lynn agrees with Dave. Beth says the foundation is there on Aviation Leasing land for a full quarter mile, bridge would be used to cross the gully, then pleasant going among the trees. Yes, it drops steeply and bicyclists could walk there. Bicycle trail would be fairly easy along the Mud Bay Trail.

Kevin Walker: revised mission statement as noted. Will include resolution. Lindianne will e-mail resolution to Kevin as well as to Renee. Will leave off last three pages.

homerkev@gmail.com, clerk@ci.homer.ak.us,

Re funding and maintenance: Dave, Kevin, Renee, will put those together, and Kevin will send them out to the Committee via e-mail. HART funds: \$234,719 at end of 2011, \$305,000 by end of 2012.

Dates: Next Parks and Rec is 15th of March, City Council will be 26th of March. Possible for this resolution and preliminary engineering packet to go to these meetings. Renee, please put the Kachemak Drive Path on the agendas for both those meetings. Bumpo will call or e-mail each Committee member to assign a City Council member.

Our next meeting: March 22, fourth Thursday in March.

We adjourn. 9:05 p.m.

CITY OF HOMER
THE REVISED RESOLUTION:

City Clerk/Parks and
Recreation Advisory Commission

RESOLUTION 11-090

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA,
SUPPORTING THE CONSTRUCTION OF A NON-MOTORIZED
PATHWAY TO INCREASE SAFETY FOR MOTORIZED AND NON-
MOTORIZED USERS ALONG KACHEMAK DRIVE LOCATED
WITHIN HOMER CITY LIMITS, FROM THE BASE OF THE HOMER
SPIT TO EAST END ROAD

WHEREAS, The Parks and Recreation Advisory Commission established the Kachemak Drive Path Committee to specifically address possible solutions to the hazards presented to non-motorized and motorized users of Kachemak Drive; and

WHEREAS, The Kachemak Drive Path Committee received substantial public input on safety concerns; and

WHEREAS, The Homer City Council has shown support for this non-motorized pathway by inclusion of the Kachemak Drive Rehabilitation/Pathway on the Capital Improvement Plan; and approving the Homer Non-Motorized Transportation and Trail Plan; the Homer Area Transportation Plan; the Climate Action Plan; and the Homer Accelerated Roads and Trails (HART) Policy Manual; and

WHEREAS, Increasing non-motorized transportation improves public health and safety; encourages tourism; cleans the environment; reduces transportation costs; and enhances livable communities; and

WHEREAS, The City of Homer has available HART trail funds that can, including other sources, form a basis for funding this project; and

WHEREAS, Utility easements can be employed with the permission of property owners.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, hereby supports the construction of a non-motorized pathway along Kachemak Drive within the City of Homer, and that said improvement will enhance safety and benefit the public; and

BE IT FURTHER RESOLVED that the City Council of Homer, Alaska further supports actions increasing safety for motorized and non-motorized users of Kachemak Drive in the following ways:

1. Increasing the usage of signage warning drivers of bicycles and pedestrians on roadway.
2. Build a separated path paralleling Kachemak Drive using utility easements and public property, and increase the use of signage.

Minutes, Kachemak Drive Path Committee, November 22, 2011

Attending: Beth Cummings, Kevin Walker (visitor), Bumpo Bremicker (chair), Dave Brann, Lindianne Sarno (recording), Lynn Burt, David Clemens

Call to order, 5:30 p.m. by Bumpo

Agenda approval: Beth moves to approve, Dave Brann seconds, passed.

Minutes approved: Dave Brann moves, Lynn seconds, passed.

Pending Business:

Meeting dates: December 15, 2011, Thursday, 5:30 p.m.

January 11, 2012, Wednesday, 5:30 p.m.

Continuing discussion, planning for path design

Dave Brann shows us a guide to path design from Minnesota Department of Natural Resources and other sources. We are aiming for a ten foot gravel path with sections of wetland and water crossings which will require other techniques. Techniques we are examining are all city approved. Page numbers here refer to hid guide to path design.

Page 27 - bridging, grading discussed. Page 29. NFS means non-frost susceptible. P 6.46, p 6, p. 6.58 boardwalks and bridges, ways to cross drainages. Bridge would be needed to go down to Spit from airport parking lot. That grade is very steep on the road (12%). Regarding airport leasing, there is a 50 foot x 2 DOT right of way.

Dave Brann recommends we use these materials to develop a final packet to present to City Council. Dave will ask Renee if she can create a packet for the committee. He suggests we draw a line on the map and ask Renee to copy it for the committee. Kevin Walker discusses with committee the segment from airport to Spit.

Dave Brann suggests we meet as an announced group and walk that section and other sections. We select December 10, 11-1 p.m. We invite Kevin to join us. Meet at airport parking lot and go to wetlands at other end of path.

Kevin discusses the sheet he created, accurate to +/- 100 feet. We use these numbers to identify suggested areas for trail types. Dave wants to correlate these numbers to the map.

Bring to field day: 100' tape, GPS device, range finder binoculars.

We break for five minutes and reconvene around visitor table. We correlate numbers to map. We will generate even more detail during field trip.

We return to U-shaped table and continue meeting. City council meeting, November 28, Dave Brann and Bumpo will attend, and will advise City Council of level of detail we have attained.

Visitor comments: Kevin is glad to participate

Beth: specifics are wonderful. Is writing to Jennifer Bailey about Aviation Leasing.

Lynn: Excited about specifics

Lindianne: will e-mail dates to Renee, then notes on Monday

David: Excited about field trip

David: Ditto

Bumppo: Ditto

Bumppo adjourns meeting at 8:10 p.m.

Kachemak Drive Path Committee

Meeting, November 7, 2011, 5:30 p.m.

1. Call to Order – 5:30 by Chairman Bumpo Bremicker

Attending, Beth Cummings, Bumpo Bremicker, Dave Brann, David Clemens, Lindianne Sarno, new member. Ingrid has resigned. Bumpo appoints Lindianne Sarno, who is recording minutes.

Agenda approval. Dave moves to approve. Beth seconds. No objections. Agenda approved.

Motion to approve minutes from last meeting. Dave moves, Beth seconds. Minutes approved. Beth wishes to discuss Mud Bay Trail.

Public Comment on Agenda Matters: Lynn Whitmore, Chair, Kachemak Bay Moose Habitat, visitor.

Lynn Whitmore: Lived here forty-odd years. Seven years Homer Fish and Game Advisory Committee. President. Kachemak Drive Moose Habitat, mitigation funds as result of lake of Bradley Lake hydro project. Moose and black bear and goat affected by that project. Ended up with \$187,000 to work with, now have about \$1,000,000 property, \$250,000 in bank. Fairly successful. At request of Fish & Game, trying to acquire habitat around Beluga Lake to save Homer Bench herd. I like to eat moose, I put time in to give them food, get food back from them. Fundamental. Understand a little of what you propose, put trail through our property, how can we help, need to hear what you guys have in mind. Trail design, trail size. My job is to feed moose or give them a quiet resting spot. Beluga land isn't so much food as quiet resting place. They get stressed in March and April, die off occurs then. F&G says not better buying property in lowlands, moose have been coming down to wetlands. Need quiet place to sit and rest. Trails, OK, through good habitat, trails going along right of way, OK.

Bumpo: Conservation easements – is that what your land is called?

Lynn: No. Land Trust deals with conservation easements. We have notes on some our property, in perpetuity agreements. Private donations and parcels, our stuff, some has easements. Around Beluga Lake.

Beth: Three pieces, west of Arctic Tern Road, is Moose Habitat Incorporated willing to let this city-owned trail go across?

Lynn: How wide? Type? Quality? Any place where there is potential for water/sewer services or HEA events, you want to be aware those services could bisect trail. My board would not be glad to have trail in moose habitat. But it would be OK if you keep trail close to Kachemak Drive.

Dave Brann: have not drawn line on map. Starting from Airport Manager's office, keep under power line, all the way to Arctic Tern, that looks do-able. HEA has no objections.

We don't want to do this without full support of landowners. Contacting Moose, Inc is part of education process for us.

Lynn: I recommend you come up with statement of trail width and materials. Do something right on foundation, in case it will have to be paved someday. If I go to talk to my board, they will want to know how big this is. Some landowners have been whacked with assessment and the take. Some people won't be thrilled to have someone else come along. Bisect some other way, may not take it very well. Path of least resistance. Say to folks, look they're changing your front yard anyway, won't plant native vegetation, we'd like to put trail through. Folks may not want trail in front of or behind their house. Easement for water and sewer is already there, concentrate there.

My board will be interested in uninterrupted quiet space, as you get away from road, power line has cushion of trees between road and power line. Keep it quiet. That's what you'd be up against.

Dave: Could you provide us with map of properties you own.

Lynn: City Planning department. On maps. Figure on a ten foot footprint. Water and sewer project. My board includes chair of Alaska Energy Association. Other is in charge of all refuges, McNeil River sanctuary. Both concerned with water flow on surface of Beluga wetland. Water sewer project changed the flow of things. Fish and wildlife made a fuss with city. If you do anything to further change that flow, you'll run into further headwinds. Again, keep it close to water and sewer project.

Bumppo: Most of trail, drains away from the bluff. Most folks didn't get that. It drains towards slough. Put in culverts to deal with that afterwards. Water and sewer takes that into account and is sloped in that direction. Trail on top could interrupt that flow. Needs to be way to let flow get across that trail.

Lynn: Landowners won't soon forget that impact on bay side. Catch it early and design it properly.

Dave: Geoblock. Familiar with? Inch and half thick in delicate areas.

Lynn: Is it a foundation? Can you pave on top of it? PSI? You'll be compressing material.

Bumppo: This helps us a lot. Drainage issues, quiet zones.

Lynn: We've all watched moose die near trees. Need to not get disturbed so they don't burn energy. In a lot of cases, you'll be dealing with already stressed landowners. Better now. I would get a fairly quick professional opinion on trail format and size. Settle what trail will look like, cost at 10 feet, prepping for future pavement. East road, huge substantial trail, had to be designed to repave. State parks will have standards for designs for wetlands. Might be leftover material from Islands and Oceans.

Dave: Bishops Beach material will be replaced. Possible to get that material.

Lynn: High value wetlands. EPA allows people who get in trouble to mitigate properties through us. We get asked to make sure Larry Herndon isn't into wetlands while ground freezes. Did core samples, they

have this amount, allowed to do now, rest after ground is frozen hard. Better to piggy back on water/sewer project.

Lindianne: how best to interface with those guys?

Lynn: (1) decide what trail will look like. (2) High value wetlands, forested HEA right of way, trail design may change. May be able to use Islands and Oceans stuff in drier parts. (3) Where will you put trail? F&W, F&G, ACoE, describe two paths, right of way and other. Have that together, then go sit with them and talk it over. Once you get your route A and route B, and trail design, have accepted building material laid out. Go to entities that have designed trails, those are the two or three types, high value wetlands, drier, highlands, enough due diligence to ask for their opinion. They may say no input until finalize. Or may be helpful. Give them some ownership of project, input up front.

Dave: How do moose deal with boardwalks?

Lynn: Moose would cross without too much problem. Heavy snow year, moose use path of least resistance. Snomads have gone into high country when wolves were getting into big valleys, pack down snow so moose can get around. Make paths to next valley. Next day, moose are out of there. Don't make it high, slick. They go across the highway.

Beth: Who for trail design?

Lynn: Jack Sinclair, in charge of state parks, Kenai Peninsula. DOT for trail design. They've done big ones and small ones. DNR state parks has one landscape architect who does trails. He will send you to somebody.

Dave Brann: Alaska Trails has design people.

Lynn: Homework: pull up borough base maps, turn on feature that shows trees, aerial imaging, look to your designs, you can come along here and see how much of each type, have that available as it goes across different parcels.

Bumppo: We're fortunate to have Kevin Walker, retired highway engineer, here. Thank you for valuable information.

Lynn: My name is in phone book. Also work part time Jay Brandt.

Bumppo: Pending business: specific strategy.

Kevin Walker: I made trip charts, a have a file, can print out to draw a line on a map. This section is wetland, this section is high, steep slope, etc. for different types of trail.

Wet, dry, steep. Three types.

Looked at 38 page sewer and water project, a little less than half the trail. Go up behind the gear shed and boat yard. Legal easement there. Davis Street. There is room for ditch and trail.

SWPP plan, whole plan, say they will grade, and plant. If they are doing that work, park money, access to, equipment is there already, have them lay down typar and put some gravel on top of it. Erosion resistant. Compact. Make sure water doesn't pick up dirt when it flows across it. Piggyback on their corps of engineers and permits. We'll help you out, put in some volunteers, in-kind contributions.

Bumppo: Advantage of putting this on top of existing, we don't have to excavate. Just add something on top. Disadvantage is drainage issue Lynn was talking about.

Kevin: while they're excavating, do our foundation as a trough in the middle of their excavation.

Bumppo they quit because they're waiting for it to freeze up for those wet areas. They seeded already.

Dave: Get in touch with Larry Herndon.

Bumppo: Larry is for this project.

Beth: Ongoing conversation with his wife. He will be gone two weeks, be back late November.

Lindianne: Move that we get in touch with Larry and invite to our December meeting.

Bumppo: I will get in touch with Larry Herndon.

Beth: Part of trail from base of spit to Arctic Tern. Few landowners there. We need to work on both at same time. People need to see section that has been accomplished. I am talking with landowners. One landowner said, you should go across my land. He will try to talk to other landowners. This is doable.

Bumppo: To get approved, we need a plan for whole thing.

Beth: Would like to go into detail for first mile and a quarter. 35 landowners from end of sewer and water to east end road. Middle starts at Arctic Tern. Limited number of landowners, most of whom are businessmen, they will get behind it.

Dave Brann: Progress with water and sewer now. I agree, work on that section. Get Larry in here. Specifics. Later in agenda, letter: has to go to commission and then to landowners.

Beth: Talked to Julie Engebretsen, City Planning, I mentioned the letter. She said the letter is not to you. That's the job of the staff to do.

Dave Clemens: Lynn laid this out perfectly. Width, design, materials. What it is. Do our due diligence. Specifics: City trail manual.

Kevin: East End road is ten foot. Major trail. Paving equipment doesn't go less than ten feet. Can pave eight feet. Dig trench and fill, that will be flat, eight feet with no slope.

Bumppo: Put down decent base, could pave later. Just figure out width and surface. Wetland area, would need special work to pave later.

Dave Brann: Safe Level 3 trail description. Page 16. Informal trail, semi-urban to rural, through neighborhoods. Use primarily pedestrian and include bike and horse. No shoulders beyond surface top. Surface material type is 4 inches not frost acceptable gravel over geo textile, boardwalk, or other reinforced type materials. Steps discouraged, less than 8% grade. Their preferred design manual for trails. If necessary include boardwalks, same width.

Dave Clemens: Page 32, non-motorized transport plan, description. Multiuse path, ten feet. Like East End path.

Bumppo: Kevin have seen material dredging out of harbor? Lots of it above Pier One Theatre. Wonder if that material would do? Sandy gravel.

Beth: Jeff Middleton said, it's mostly sand, would use as fill, would have to put gravel that impacts on top to be useful for bicyclists.

Kevin: Has to be combination of sizes, sharp compactable particles, crushed. 30% or more have to have sharp fracture faces. Mix dredging with crushed gravel. Presents

Bumppo: Ten foot wide, geotextile under it, level three. Boardwalk included in some areas. Tough enough to have snow removed. Small snowblower. Maintenance cost.

Detailed plan:

From base of Spit, flat on trail, until pull off place at base of hill. Fill along side of road, leave grass strip. Or boardwalk along that section. \$100/foot for boardwalk. (Need permission from Jocelyn Baloon for working in that right of way. Boardwalk in right of way, but would need permission from ACOE and F&W for pilings.) Grass strip or barrier separates bike path from road. This takes us to base of hill.

From parking lot at base of hill, trail goes off through patches of trees, drops down to wet area, next patch of trees, over hill, to wet area, then hugs the edge of the grass, mostly at toe of the bluff. Ten foot wide gravel path. Until airport access road. Multi-use recreation bicycle-pedestrian trail. Plan includes signage out on road for bicyclists who continue to commute on the road: Do not pass bicyclists on this hill!

There is (1) option to go up the long slope to the west end of long term parking, rejoining huge road shoulder; would require bridging culvert; (2) easiest option is to go up airport access road. Consult with experts, Dave Brann suggests asking Kevin Myers about this section. Dave Brann will call Kevin Myers. This is our only problem area.

Up access road to long term lot across from airport. Research Reserve building, all part of Aviation Leasing. Follow yellow line, sewer line, to Bay Club Driveway, filled trail along that section. Little bit of leveling, geotextile, and gravel, will take care of that section. Cross the road right there, to right of airport manager's office. Cross there because few driveways for bicyclists to cross, good line of sight. Now you're on northside of K Drive, follow blue line, along road to end of boat yard. A few puddles in there, mostly dry, gets wet last lot of four. Power line easements go along there. From end of boat yard, follow power line easement across Lampert Lake to Caravan Trucking Company, section there is somewhat wet. Use plastic boardwalk, or Geoblock. Geoblock maintains trail structure, but nature works through it. Plants, water, gravel can all get through. Could help us get across muskeg/wetland. Stay close to road, but separate. Stay under power line on Moose, Inc. property. To Arctic Tern.

At Arctic Tern, powerline continues parallel to road. Stay with it. From Arctic Tern to curve, terrain is dryish, muskegish, dry enough for typar and 4 to 6 inches of fill. At curve, just before house, cross the drive way, stay close to the road, then cut back to powerline easement. Terrain is relatively dry, use Geoblock, gravel, or turnpike style. Old mill, go in front of mill, stay on powerline. Right across from Morris Avenue. Property owner says OK to go in front of his house. Cut back to road in front of Rob Zolo's property. Join water and sewer.

On water and sewer line, fabric and gravel construction. Easiest for maintenance of water and sewer line. Until high value wetlands. Short high value wetland sections. Where spruce trees grow, fabric and fill gravel OK; where terrain is grassy, will need Geoblock. All the way to boat yard. May be able to use plastic boardwalk from Beluga Lake trail. Boatyard, use water sewer line. To Davis right of way. Big ditch is there, put in large cement culvert, lay in ditch, cover with non-frost fill.

Bumppo: Agenda: Close meeting. 8:30.

Beth: Carey Meyer said we need formal letter, requesting plastic material to be removed from Bunnell Street. Beth will write the letter, Bumppo will sign. Beth moved, Dave seconded.

Bumppo: meeting dates: City Council is 28 November. Go to City Council and ask for resolution back. Do not need vote on it. Need ten days to advertise. Our next meeting will be Tuesday the 22nd of November, 5:30 here, Parks and Rec is 17th of November.

Bumppo will call Larry Herndon and see if we get him to come to that meeting.

Dave: Will keep chain sawing logs out of Mud Bay path.

Bumppo: Adjourns meeting at 8:30.

Minutes for Kachemak Drive Path Committee, Thursday, October 6, 2011

Bumppo Bremicker, chair, Beth Cummings, Dave Clemens, Lynn Burt, Dave Brann, Lindianne Sarno recording

Bumppo called the meeting to order, 5:30 p.m..

Motion to approve agenda by Dave Brann, seconded by Lynn Burt.

Agenda approved.

Beth Cummings moved to approve minutes, seconded by Dave Brann.

Minutes from last meeting approved.

No public comments.

Visitor: Regina Mauras, Finance Director, City of Homer. Regarding HART policy, spoke with Walt Wrede, for better clarification. Walt reiterated that any trails built need to be adjacent to city roads, city property. State road wouldn't be considered. Suggests that path committee come up with exactly what you want, run it through the attorney's office and see if it meets the policy. This is what we intend, is it within what City intended? Funds are available for non-motorized trails. \$110,000/year available for trails. Actually: \$226,222 at end of 2010. \$305,000 will be available at end of 2012. Trail does not have to be along the road, then it's considered diverse, and can be within meaning of HART. Sidewalk has to be along city road, trail not necessarily along city road.

ADA compliance necessary. Dave Clemens suggests we get full wording of Resolution 7-82 because this is referenced by ADA requirement.

Once you get full approval resolution, then ask council if we can access these funds for this trail. Get an Ordinance for however many dollars you think it will be. Can request loan from city from City funds. Look through project section of budget, funds can come from different sources. HART, general fund.

Bumppo: say it costs \$1,000,000, that would be ten years of HART funds, could city access loan using HART funds to pay it off? Regina: there are precedents.

Dave Brann: Once city approves use of those funds, how do you access just the amount you need. Regina: Look at project, e.g. \$5,000. Ask for whole amount. Once it's been approved you have three year window to complete project. Spend it as you use it.

Dave Brann: If this committee were in charge of building this trail. Go to SBS to get materials, they bill city? Regina: Through contractor, we would get bill from contractor, with costs of labor and materials. If you were building, it would go through the city.

Regina: Will get copy of Resolution. (Delivers copies of Resolution 7-82)

Beth: Over water utility easement, one segment or phase from East End road, second segment from base of Spit going as far as Artic Tern Road, third section in between there. Three phases. Regina: Put that in a resolution to get blessing of council. Ordinance would be once we have a dollar amount.

Pending Business:

Dave Brann: Met with HEA and got response: no problem with use of HEA right of way for bike path. Beth questions Dave on water sewer line plan, some electric lines seem to be underground. Discussion: Dave says wouldn't want us to raise with so much fill that maintenance would be difficult under lines.

Dave Clemens: In Anchorage, trails run over easements. Dig up if needed, then patch again. Beth: get letter from Anchorage? Dave Clemens will get a letter from Anchorage. Gas, water, sewer, electric, easements are the best way to go for bike paths.

Dave Brann: Right of ways are reserved for utilities. Bumppo: State right of way? Dave: It varies, 30 to 50 feet. When road was rebuilt, state didn't have money to have consistent right of way. Beth: State claims land along Kachemak Drive that goes across ditches to be able to cut willow. Prescriptive right of way. Dave: advantage not having path on DOT right of way.

Discussion of possible funds source. HART program. Page 15 of packet: Bumppo: we meet most of these criteria. Need to discuss property owner participation.

Beth: Reiber Trail is not wheelchair accessible. ADA applies?

Bumppo: could be done at Kachemak Drive.

Dave Clemens: Re staying off DOT: city council may try to push this off on state, keep in mind. Bumppo: City has been saying for ten years let state build it. I don't see this ever being number one on CIP list. Maybe we can get some partial funding from the State. Dave Brann: State DOT resists building bike trails. This will cost way less than most people imagine. Much can be done by volunteers. I'm concerned about permission from landowners. Once we decide where line on map is, potential costs known, we can possibly look for other sources. But start with HART funds. Bumppo: Use ten years of HART funds: \$1.3 million available. Matching funds from state. Grants. If you have pool of money to start, it attracts other money. City Council is concerned about money. Come up with solid plan.

Beth: I talked to four council members. One said, go for it. Second person said, you need to simplify, direct and be more specific in purposes. Two other people said, I don't want any unforeseen commitments financially for the city. (But I didn't mention HART funds.) We don't even know if there are trails that have higher priorities. Trail between Senior Citizens and Pioneer possible.

Bumppo: I testified at City Council and brought up HART funds. I made it clear that the HART trail funds are a separate fund and have to be used for trails. This won't take any money from road building funds. I made that clear.

Beth: Regarding Reiber Trail. It was not related to roads. It connected a road and another road. Purely connecting two roads and paid for by HART trail funds.

Lynn: Reiber Trail: Is it through private property? Dave Brann: Within piece of property that was donated to city by a homesteader.

Bumppo: At some point we should make recommendation to city. If offroad trail is our intention, we should move to do that and get this buttoned down.

Dave: Any other encumbrances on HART funds? Bumppo: none that I know of. Regina said this is what's left after encumbrances, at end of 2012.

C. Discussion and Recommendations on Defining Purpose and Goals of this Committee

Beth: Mission statement should stick to one path paralleling Kachemak Drive for safety of non-motorized traffic. Dave Clemens: We have five or six different notions. Beth: would like to get back to having a separated path, separated from K Drive, emphasizing safety of bicyclists and pedestrians. Include reference to need for smooth flow of traffic. Would like to give one choice to go to Parks and Rec and City Council.

Bumppo: change available funding, contingent on "available HART trail funds" and give specific numbers. On page 25, new resolution: path paralleling K Drive.

Dave C: goal of this committee: trail separated from road generally, or will this committee also obtain easements? Is goal to get blessing or is it to get specifics?

Bumppo: Blessing of City Council has been accomplished. Our goal is to make this happen. The more specifics, the better their reaction will be.

Dave: Line it out, this is what we want to do. We should plan out the trail that we want, that makes sense, is workable, is a good trail. Then look at funding. Design a good piece of infrastructure, then go for it.

Bumppo: I agree. Put in clear funding sources. In resolution: this is our plan, here is how to pay for it.

Beth: Would like to provide exact wording for new resolution. Old resolution on page 25. Concept and construction of a non motorized pathway to increase safety for motorized and non motorized users along Kachemak Drive located within city limits. And to encourage the smooth flow of traffic for both commercial and noncommercial users along Kachemak Drive.

Line 7, "these safety and traffic flow concerns, construct separated non-motorized path"
Line 15 leave out "and"

Take out line 16 through 20.

Put in a whereas: before line 22, be it resolved City of Homer Alaska supports concept and construction of a separated non-motorized pathway along Kachemak Drive.

Add: WHEREAS lanes are mainly only 12 feet wide with little shoulder space between road and ditch and mainly prescriptive right of ways,

WHEREAS, this project is dependent on funding, HART funds are available for this project.

Line 29, be it further resolved that the city of Homer, etc ... as well as supports action to encourage smooth flow of commercial and non-commercial traffic,

Line 31 omit

Keep line 32, a separated path paralleling Kachemak Drive utility easements

Bumppo: discussion?

Dave Brann: smooth traffic flow for commercial vehicles. I don't think we have to worry about traffic on road. Separated path is our job, creating smooth traffic flow is not in our purview.

Bumppo: Wider reasons for this path, it doesn't hurt to have them in there.

Dave Clemens: This is a significant change. We're concentrating on a path. This is good. Moving forward. A gravel based trail will not remove all cyclists from the road. Cyclists will say, "gravel path? I'm on the road." Would still like to see wider shoulders and signs on the road because bikers will be on the road. Relieving traffic flow: there will be bikers on the road.

Bumppo: Lindianne looked on internet about surfacing trails. City has amount charged to homeowners. Paving costs half as much as gravel base. Asking Kevin: cost of paving and subsurface?

Kevin Walker, retired state highway engineer. I worked in Nome and Kotzebue where it cost hundreds of dollars a yard to get materials. I know state had a program to pave everything black because maintenance coat is lower. Once it's paved, don't have to grade. Have to tear up once it falls apart. City charges less for paving than for gravel?

Bumppo: page 16, number 6 seems to say gravel costs more than paving.

Kevin: passed two people riding on east end road tonight. Several types of bicyclists. Several kinds of roads. Can ride a bike 30 mph on compacted highly crushed gravel. Pavement great as long as it's maintained. Gravel starts at 30/foot and then the paving goes on it. Crushed high quality gravel surface. Base is expensive.

Dave C: City council meeting, idea of bikes off the road? Bikes have a legal right to road. Bikes won't disappear. Public needs to know it's OK for bikes to ride on Kachemak Drive. Get bikes off the road is wrong message to send. It's not the law.

Beth: whereas all of the north side of K drive is commercial and routinely used by commercial sized vehicles, add that whereas to resolution.
Do you feel there should be mutual courtesy for bicyclists to ride down middle of lane?
Saw two bicyclists with dog on rope with dog wandering all over. If

Dave C: I'm in middle of lane, don't pass me. It's not safe. Trucks go slowly, I couldn't pass truck. If cars stack up behind me I pull over and let them pass. I try not to slow cars for more than 12 seconds on West Hill Road.

Lynn: Everybody is responsible for safety of all. Good citizen.

B: I wait until it's safe to pass cyclists on K Drive.

B: Comment: Not all commercial on North side of Kachemak Drive; it's zoned rural residential where I live. Want to continue this? Hammer out resolution now?

Lynn: great to hear what Beth had to say. Would be good if we had Ingrid here.
Vote to wait on that.

Dave B: wait. We need to be able to answer city councils questions. We don't have the line drawn. Will it all be on water and sewer line easements? We talked about work party concentrating maps and where we want the trail to go, so we have clear picture

Bumppo: Table this discussion. We're starting to get it. We need to get more specific.
Thank you, Kevin, for your input.

Kevin: Plans, specifications and estimate. Take care of easements. Huge job. I'd be happy to work with you guys and try to put the line on the map. From here to here, this kind of construction, each section of trail has typical section. Might have three or fifteen types of sections. We need a sketch, a readable document, assume can go across airport leasing, cross south to north side at some place, I volunteer to help get it started. Pencil and paper sketch will result.

B: Work session, you'd be valuable.

K Can work with Dave or whoever, here's where we have to go through trees, make it so when we come to work session we're not starting from scratch.

Bumppo: schedule this: we're now on new business. Leave time for preliminary work.

Dave C: Have connectivity plan all the way from spit to East End,

Kevin: From Artic Tern Road to East End Road it's all laid out. Good to go.

Beth: 35 property owners from Artic Tern to water sewer line. Have started calling. Listened to him for half an hour on Eminent domain. Anne Whelan wildly enthusiastic. No one else was happy. Planning commission voted to put it on the STIP list. Dave: STIP list does not mean right of way. State Transportation Improvement Plan.

Dave: Getting back to worksession, plan work session, get it on paper first. Until we draw lines on paper here, then we are spinning our wheels.

Kevin: I will download data from boro, doing each 100 feet on paper. Need project plans from sewer. Will be a stack of paperwork. Will line out the entire path.

B: Our next scheduled meeting is: Monday November 7th. Kevin will attend. 5:30 in here. That will be the worksession. Two items: Resolution and the segmented plan.

Parks and Rec Commission meets on 20th of October.

Beth: wants to talk about Mud Bay Trail. Simplest part of whole trail, from base of spit to Artic Tern Lane. Five segments. Want to mention segment B. No shoulder. Have faith in Dave Brann being able to do this: Enormous quantities of fill. Two terraced trails, one for eastbound and one for westbound. Only difficult part in whole trail.

Bumppo: Table this discussion til next time.

Comments of committee:

Dave C: good that we can get refocused. Move forward! I'll be leaving committee due to time constraints.

Dave B: envision committee as ongoing, changes in members. Some kind of trail from one end to the other. I like the idea of stuff on the roadway, won't happen in short term until road is rebuilt. We can make real progress if we look at maps and landowners. Put the line on the ground. Drafting letter to landowners. Many do support the trail. Find out who doesn't support the trail. Options range from do nothing to donating easement. Selling easement. Making conservation easement. Tax incentives. Find out their individual concerns. If can't cross, go around. Slow, frustrating but we're getting there.

Lynn: B: I agree, trail will happen. Thanks for Mud Bay trail. Beth and I marked high tide with new stakes. 23.1 tide is second highest this year. Dave has mowed and its wonderful.

Beth: I e-mailed thank you to you Dave. Glad Dave brought up possibility of perk for people who agree. Checking with land trust to see if they have ideas. Nature conservancy. Will call them. See if they have an idea that didn't occur to us. We're ready to be more specific.

Bumppo: Lindianne and I drafted a letter to landowners. Not that many owners. Maybe 100. Get together with Dave Brann and compare notes.

Beth: Can I keep calling people?

Bumppo: That's fine.

Keep plugging away.

Thank you all.

Bumppo adjourns about 8:30 p.m.

Kachemak Drive Path Committee
491 E. Pioneer Avenue
Homer, Alaska

October 6, 2011

To: Property Owners Along Kachemak Drive

The Kachemak Drive Path Committee is working to build a pedestrian and bicycle path along Kachemak Drive. The City of Homer plans to build the bike and pedestrian path along the utility (water and sewer) easement or under HEA power lines. Both forms of easement cross private property such as yours. Therefore the City of Homer would need your permission to build the portion of the path where the easement crosses your land.

Alaskan state law provides that where established trails cross private land, property owners are not liable for any injuries that occur on their lands on those trails.

Do you give permission for the Kachemak Drive bicycle/pedestrian path to cross your property?

Yes.

No. I have further concerns. Concerns: _____

Signed,

Name _____

Date _____

Address _____

Kachemak Drive Path Committee
Minutes September 8, 2011

Meeting called to order 5:37 P. M.

Members present: Beth Cummings, Dave Brann, Lynn Burt, Dave Clemens, Bumpo Bremicker

Moved and seconded to approve agenda

Moved and seconded to approve minutes

Public Comments:

Kevin Walker supports developing a path on top of the water / sewer easements. He will help the committee any way he can.

Lindianne Sarno: Expressed concerns about a gravel pathway as it is difficult to ride on. She volunteered to research various path surface types.

Val McLay: He supports a separated pathway but opposes narrowing the travel lanes. He sees narrowing the lanes as creating a bigger safety issue. He thinks bicyclists should ride in the road as allowed (not try to squeeze in to a two foot lane which is not safe for kids) or push for the separated path.

Beth Cumming read a letter from Trina Fellows into the record: "I think turning an industrial road (Kachemak Bay Drive) into three lanes so bikers can have a lane is a very dangerous idea. From February to October oversized boats & gear are being towed from all the boat yards to the Spit road. Large trucks also use this road. This would be an accident waiting to happen. Thank you, Trina B Fellows"

Jeff Middleton, applauded the effort to create a bike trail but is opposed to narrowing the travel lane. He feels that would be very unsafe.

Pending Business:

Bumpo and Dave plan to attend the city council meeting on Monday. The committee didn't establish a schedule of who would attend city council meetings in the future.

Beth mentioned she had contacted city council members about a month ago and may do so again.

No new information on the use of fill at this point although it was noted that dredging is once again going on and that it would be cheaper to haul it once from the source rather than having to store it then haul it again. Beth talked with Bryan Hawkins and the gravel is being advertised to be sold.

McLay did provide some information on how the harbor enterprise fund works and there would still be some expense in getting the dredge materials. He said the city contracts to have the material hauled away. He also provided some history and precedence of using dredge materials off the spit.

Dave has not contacted HEA yet as he is waiting for more information from a couple of sources before making the contact. He stated he is working on it.

No changes, progress or info related to the idea of narrowing the travel lanes. Beth did have emails from businesses along Kachemak Drive that are opposed to narrowing of the travel lanes. She was going to pass them on to the city council.

It was moved and seconded to pass two recommendations on to the Parks and Recreation Commission, the Homer Transportation Committee, the City Manager and the Homer City Council related to signage on Kachemak Drive.

1. It is recommended that two yellow and black "Share the Road" or "Bicyclists / Pedestrians on the Road" signs be added to existing advisory signs along Kachemak Drive. Locations to be: A. Between the Spit Road / Trail and the airport hill, B. After the Boat Yard going towards the spit.

2. It is recommended that two "driver feedback" (digital radar) signs be added to existing advisory signs on Kachemak Drive. Radar signs would flash driver's speed if over the established speed limit.

Homer Non-Motorized Trail Plan (Section 5.0, pg. 56) states:

"Creating safer bicycling conditions on Homer's most traveled bicycle routes is a high priority. The routes include Kachemak Drive."

"To further the continuity of this route, (East Road Bike Path) Kachemak Drive must become safer for non-motorized transportation."

"A rehabilitation project for Kachemak Drive that includes a separated multi-use pathway has been identified as an essential need of the City's Capital Improvement Program."

Section 5.0, page 55-56 Near Term Improvements states:

"Near term improvements for Homer's non-motorized transportation infrastructure should address immediate safety concerns...and the creation of safer bicycle conditions.

"For the purposes of this report, Homer Non-Motorized Transportation Plan, June 28, 2004, a near term improvement is one that will be realized in the next two years."

*Signs could be purchased with a combination of private, city and grant funds. Signs could be installed and maintained by volunteers.

Approx. costs, two yellow advisory signs \$50.00 each
Two radar signs \$3000 each: Total \$6100.00 + or -

The Committee added discussion of the Hart Funds to the agenda.

Lynn gave a brief report on the monies available and noted that Regina would be willing to come to a meeting to explain more about the funds. Currently \$234,719.00 available.

Bumppo suggested that we ask the city to build the trail and use the HART funds to pay it off. There is an annual increase of \$40-\$50,000 based on a percentage of the sales tax collected within the city.

There was discussion of using the water and sewer easements for the trail. There was some question on how best to approach it. Dave B. mentioned that Carey and Walt said that when the city obtained the easements for water and sewer some landowners specifically stated they didn't want a trail on the easement. It was felt that the committee needs to find out what the landowners concerns / objection are in order to explore possible solutions. No action taken or suggested.

NEW BUSINESS:

It was moved and seconded to name the portion of the trail from the Spit Trail to the Airport Beach access road the "MUD BAY TRAIL". Motion passed. A brief discussion on the historic name of Coal Bay as opposed to Mud Bay.

It was moved and seconded to place routed signs along the trail. Beth though 12-15 signs with directional arrows would be needed. Motion passed.

Discussion on signage related to illegal camping along the Mud Bay Trail and other city properties. Beth felt signs designating the Mud Bay Trail would be enough to discourage campers in the future. She supports a kinder-gentler approach. Dave B. stated he didn't have the patience that Beth did and was frustrated by the abandoned campsites, accumulated camping debris, and waste disposal (or lack of). As an individual (not as a committee member) he will ask the police about impoundment of abandoned tents, etc.

Bumppo brought up the concept of a free camping area to accommodate those in need. Will add it to a later agenda.

Lynn said she did complain to the city police about the illegal camping in the early summer but that no action was taken. She was referred to Angie Otteson but wasn't able to make contact. Someone mentioned that Angie was willing to check out the sites but wanted someone to go with her. It doesn't seem like that happened.

Discussion on Portion of Proposed Pathway from East End Road to the West End of the Kachemak Drive Water / Sewer Phase II Project.

Beth felt that a turnpike type of trail construction would be inexpensive and a good way to cross some of the wetland areas along the sewer / water easements. Dave B. concurred that the turnpike construction, fabric, retaining logs along the edges, sand fill with a cap of gravel or other surface material was a standard trail construction technique in wet areas. It was noted that anyone could recommend this type of construction to the city council but the committee felt it was too early to present construction techniques when permission to utilize the easements hadn't been obtained. Discussion continued on how "how to make this happen". Dave B. said Walt was going to look into the easement language to see what could or couldn't be done, it was suggested that the committee continue to research what the concerns / objections to having a trail on the easement might be. Bumppo felt the city council should decide on how to make the trail happen.

Discussion on the Proposed Implementation for the Segment of Trail from the End of the Spit Bike Trail to Arctic Tern Road. Beth presented ideas on a segment of trail along the edge of the parking areas near the top of the bluff. It would require permission from Airport Leasing. She stated the cars parked there pay nothing for the use of the land and the parking could be adjusted to accommodate a trail. She talked with Paul Seaton's office about the possibility of a waiver from ?? in order to accommodate a trail in that area. Val McLay provided a little history on that section of land noting that it was a dump and trees have grown up on it holding it in place, he was concerned about cutting any trees that might compromise the bluff. Beth felt no trees would need to be cut.

Dave said that with permission of landowners he would be able to use a trail mower to mow the power-line easement from the Airport Managers office to Arctic Tern with only a few wet areas or ditches to be addressed. It wouldn't create a trail but provide a visual representation of the possibility.

Discussion and Recommendation on the Mission Statement of the Committee was postponed until the next meeting.

Comments from the audience: Val supports the turnpike style of construction with the use of D-1 gravel as a surfacing material. He suggested the committee look into flashing signs that could be rider (bicycle) activated as they used the road. The signs are used on various highways and in road tunnels when bicycles are utilizing them. Don't remove any trees from the top of the bluff.

He doesn't support the idea of free camping as a way to accommodate current illegal campers.

Comments from the Committee:

Dave C. wants to discuss the Committee goals, feels we have become a little scattershot in our approach.

Dave B. Some recent improvements to the Mud Bay Trail, check it out.

Lynn will write a letter to the editor about illegal campers.

Beth said no trees would be cut with her proposal and that she did contact many business owners along Kachemak Drive.

Bumppo said we would look at the mission statement of the committee and that "We're not done."

Meeting adjourned at approx. 7:30

Next Meeting is October 6, 2011 at 5:30 p.m. in Council Chambers at City Hall

Minutes submitted by Dave Brann

NEW BUSINESS

RESOLUTION(S)

CITY OF HOMER
HOMER, ALASKA

City Manager/Finance

RESOLUTION 12-039

A RESOLUTION OF THE CITY COUNCIL OF THE CITY OF HOMER, ALASKA, ACCEPTING THE 2011 COMPREHENSIVE ANNUAL FINANCIAL REPORT WITH AUDIT AND FINANCIAL STATEMENTS AND ACKNOWLEDGING THE MANAGEMENT LETTER SUBMITTED BY THE CITY'S INDEPENDENT AUDITOR, MIKUNDA, COTTRELL, & CO. AND AUTHORIZING THE CITY MANAGER TO EXECUTE FINANCIAL REPORT.

WHEREAS, Mikunda, Cottrell, & Co. conducted the annual audit, submitted the 2011 Comprehensive Annual Financial Report, with the audit with financial statements, and the management letter was delivered for review and distributed to the Mayor and City Council May 2012; and

WHEREAS, Mikunda, Cottrell, & Co. made a public presentation during the Committee of the Whole and the Regular Meeting of May 14, 2012.

NOW, THEREFORE, BE IT RESOLVED by the City Council of Homer, Alaska, that the 2011 Comprehensive Annual Financial Report with audit and financial statements is accepted and that the management letter is acknowledged as submitted by Mikunda, Cottrell, Co. the City's independent auditor.

PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

ATTEST:

JO JOHNSON, CMC, CITY CLERK

Fiscal information not required.

CITY OF HOMER
HOMER, ALASKA

City Clerk

RESOLUTION 12-040

A RESOLUTION OF THE HOMER CITY COUNCIL
EXTENDING THE CITY OF HOMER PRISONER MEAL
SERVICE CONTRACT FOR ONE YEAR TO THE FIRM OF
CARIBOU FAMILY RESTAURANT OF HOMER, ALASKA,
AND AUTHORIZING THE CITY MANAGER TO EXECUTE
THE APPROPRIATE DOCUMENTS.

WHEREAS, Caribou Family Restaurant submitted a request for renewal of the meal services contract to the Chief of Police on April 27, 2012; and

WHEREAS, The prisoner meal contract was awarded to the Caribou Family Restaurant in 2011 for one year with options for two one-year renewals; and

WHEREAS, The Chief of Police recommends renewal for an additional year; and

WHEREAS, The current contract amount will remain in effect to include: Breakfast \$6.50, Lunch \$7.00, and Dinner \$10.00 Per Day/Per Inmate and Delivery Fee of \$5.00 Per Trip; and

WHEREAS, This first extension will extend through June 30, 2013 and is not final until written notification is received by Caribou Family Restaurant from the City of Homer.

NOW, THEREFORE, BE IT RESOLVED that the Homer City Council extends the City of Homer Prisoner Meal Service Contract for the term of one year with the firm of Caribou Family Restaurant of Homer, Alaska, and authorizes the City Manager to execute the appropriate documents.

PASSED AND ADOPTED by the Homer City Council on this 14th day of May, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

ATTEST:

JO JOHNSON, CMC, CITY CLERK

Fiscal Note: Continuing monthly expenditures to account 100.164.5206 based on meals provided.



CITY OF HOMER

POLICE DEPARTMENT

4060 HEATH STREET HOMER, AK 99603-7609

EMERGENCY 911
TELEPHONE (907) 235-3150
TELECOPIER (907) 235-3151

MEMORANDUM 12-075

DATE: April 30, 2012
TO: Walt Wrede, City Manager *W. Wrede*
FROM: Mark Robl, Chief of Police *M. Robl*
SUBJECT: Jail Meal Contract

The jail meal contract went to bid last year and was awarded to the Caribou Family Restaurant. The contract is let for a one year period with the option of two, one year renewal extensions for a total of three years. The Caribou Family Restaurant has submitted the attached letter requesting to exercise the renewal option and extend the contract for another one year period. We have received good service and quality food from this provider; I recommend that we obtain council authorization to extend the contract for another year.

Caribou Family Restaurant

672 East End Road, Unit B

Homer Alaska 99603

Phone (907)235-5148

Fax (907)235-5149

April 27, 2012

Attention: Mark Robl
Homer Chief of Police
4060 Heath Street
Homer AK 99603

Ref: Letter of Intent

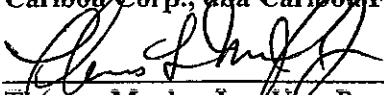
To: Mark Robl:

It is our intention to extend the contract: Prisoner Meals Contract dated June 9, 2011.
Extending this contract for an additional twelve 12 months starting on July 1, 2012 and
expiring on June 30, 2013.

Under the current contract Meals prices will remain the same.

All the terms and conditions of the contract remain the same and in full force and effect,
Current business license, Certificate of Insurance, and permits will be provided within 30
days of contract mutual agreement has be executed by both parties.

Execution Approval for renewal of Contract:
Caribou Corp., d/b/a Caribou Family Restaurant


_____,
Thomas Moxley Jr., Vice-President

Date: 07/27/12

City of Homer

Date: _____

Print Name , City Manager

Handwritten text at the top of the page, possibly a title or header, which is mostly illegible due to fading and bleed-through.

1 CITY OF HOMER
2 HOMER, ALASKA

Wythe

3
4 RESOLUTION 12-041
5

6 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
7 ALASKA, DESIGNATING COMMUNITY ECONOMIC
8 DEVELOPMENT STRATEGY (CEDs) PRIORITIES AND
9 REQUESTING THAT THE CITY ECONOMIC
10 DEVELOPMENT ADVISORY COMMISSION REVIEW THESE
11 PRIORITIES AND PROVIDE RECOMMENDATIONS
12 REGARDING IMPLEMENTATION STRATEGIES
13 INCLUDING TIMETABLES, RESPONSIBLE PARTIES, AND
14 FUNDING.
15

16 WHEREAS, The Homer City Council recently adopted a Community Economic
17 Development Strategy (CEDs); and
18

19 WHEREAS, Economic development and job creation is a Council priority and it would
20 like to move forward with implementation of goals and objectives that are feasible and prudent at
21 this time; and
22

23 WHEREAS, The Economic Development Advisory Commission's (EDC) work plan for
24 this year includes reviewing the CEDs and making recommendations to the Council regarding
25 implementation; and
26

27 WHEREAS, The City Council determined that it would be beneficial to review the
28 document again itself and provide the EDC with a list of priorities that it would like the
29 Commission to focus on; and
30

31 WHEREAS, Council members reviewed the CEDs and offered suggestions regarding
32 priorities at a workshop on April 27, 2012.
33

34 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council hereby
35 designates the following as CEDs priorities:
36

- 37 • Affordable Housing
- 38 • Voc-Tec or Marine Tech training and education
- 39 • Market Homer for High Tech Businesses
- 40 • Downtown Vitalization

- 41 • Expand Water and Sewer Distribution Systems and the Number of Customers
- 42 • Expand Shoulder Season Sports
- 43 • Promote Homer as an Agricultural Center

44
45 BE IT FURTHER RESOLVED that the Council requests that the EDC review these
46 priorities and provide recommendation regarding implementation strategies including timetables,
47 responsible parties, and funding sources.

48
49 PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

50
51 CITY OF HOMER
52
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54 _____
55 JAMES C. HORNADAY, MAYOR

56 ATTEST:
57 _____
58 JO JOHNSON, CMC, CITY CLERK

59
60 Fiscal Note: N/A
61

40 WHEREAS, It would be appropriate for the Borough to transfer its share of Fisheries
41 Business Tax Proceeds that are collected in the City of Homer to the City of Homer so that it can
42 reinvest the funds into Port and Harbor infrastructure that benefits the entire Borough.

43
44 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council hereby
45 respectfully requests that the Kenai Peninsula Borough transfer or “pass through” its share of
46 Fisheries Business Tax proceeds that are collected in the City of Homer to the City of Homer to
47 be deposited into the Port and Harbor Enterprise Fund and reinvested into maintaining, repairing,
48 and replacing vital infrastructure.

49
50 BE IT FURTHER RESOLVED that the funds be transferred on an annual basis as a
51 “pass through” with no grant agreement or reporting requirements since no such requirements
52 are included in the statutes governing this program or imposed upon the Borough.

53
54 PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

55
56 CITY OF HOMER
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59 _____
JAMES C. HORNADAY, MAYOR

60 ATTEST:
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62 _____
63 JO JOHNSON, CMC, CITY CLERK

64
65 Fiscal Note: Estimated revenue \$80,000.
66

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**CITY OF HOMER
HOMER, ALASKA**

Howard, Lewis, Wythe

RESOLUTION 12-043

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA, ACCEPTING AND APPROVING RECOMMENDATIONS SUBMITTED BY THE PORT AND HARBOR IMPROVEMENT COMMITTEE REGARDING CAPITAL IMPROVEMENTS IN THE HARBOR AND THE FUNDING THEREOF AND AUTHORIZING THE CITY MANAGER TO PREPARE THE DOCUMENTS NECESSARY FOR GRANT FUNDING, A REVENUE BOND SALE, AND THE FEE ADJUSTMENTS NECESSARY TO SERVICE THE BONDS.

WHEREAS, The Homer City Council created the Port and Harbor Improvement Committee through adoption of Resolution 11-60; and

WHEREAS, The Committee scope of work included identifying priority port and harbor capital projects that could be financed through the sale of revenue bonds and to prepare a “revenue bonding plan”; and

WHEREAS, The Committee has held meetings on a regular basis since August of 2011, reviewed and analyzed relevant information and facts, prepared a proposal as a basis for public discussion, conducted public outreach activities, and prepared a final recommendation to the Council; and

WHEREAS, The Committee recommends that the City prepare grant applications and sell bonds for the following projects at a total cost of \$9,094,000 (includes Denali Grant contribution of \$330,000); the City would fund one half of the amount (\$4,547,000 plus \$110,000 (City’s 25% match requirement of the Denali Grant of \$440,000) for a total of \$4,657,000 through the sale of revenue bonds:

- Ramp 3 Gangway and Approach
- Harbor Float Replacement
- System 5 Upgrades
- Load and Launch Ramp; and”

41 WHEREAS, The Committee recommends that the Council bond for a full \$5 Million so
42 that unforeseen contingencies and increases in the costs of construction and materials can be
43 addressed; and

44
45 WHEREAS, The Committee recommends that the following fees be increased in order to
46 generate the revenue needed to service the bonds and increase contributions to the depreciation
47 reserves:

- 48 • Fuel Wharfage
- 49 • Moorage
- 50 • Ice
- 51 • Dockage

52
53 WHEREAS, Detailed information on the proposed projects, fee increases, and bond
54 payments is included in the attached Memorandum 12-076 from the Committee.

55
56 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council hereby accepts
57 and approves the recommendation provided by the Port and Harbor Improvement Committee as
58 described in Memorandum 12-076, a copy of which is attached and incorporated herein; and

59
60 BE IT FURTHER RESOLVED that the Council authorizes the City Manager to begin
61 implementing these recommendations by preparing the documents necessary for a grant
62 application, bond sale application, and the necessary resolutions to adjust the fee schedule and
63 Port and Harbor Tariff.

64
65 PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

66
67 CITY OF HOMER

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69
70
71 _____
72 JAMES C. HORNADAY, MAYOR

73 ATTEST:

74
75
76 _____
77 JO JOHNSON, CMC, CITY CLERK

78
79 Fiscal Note: Bond debt: \$4,274,500.00.

MEMORANDUM 12-076

TO: Mayor Hornaday and Homer City Council

FROM: Port and Harbor Improvement Committee / Barbara Howard, Chair

DATE: May 14, 2012

SUBJECT: Port and Harbor Improvements / Recommended Revenue Bonding Plan

Introduction

The Port and Harbor Improvement Committee was established by the City Council through the adoption of Resolution 11-060 during the summer of 2011. The Committee was formed “to establish a port and harbor revenue bonding plan.” The Council formed the Committee because it recognized that there are significant maintenance, repair, and replacement issues at the port and harbor which are accumulating and not being addressed adequately. The port and harbor is a vitally important public asset and economic engine and it must be maintained properly. It was understood that the City could not continue to wait until someone else provided the necessary funding. The Committee held its first meeting on August 18, 2011 and it has conducted a total of 12 meetings since. The Committee provided its first report and recommendations to the Council in November as required by the enabling resolution.

The Work Plan

The Committee addressed the scope of work in a thorough and methodical manner. It worked hard to make sure it understood all relevant variables in the decision making matrix. This included the need and justification for the proposed projects, the finances of the enterprise fund and its capacity to incur debt, and the details of potential funding sources.

During the course of its work, the Committee considered the following information:

- Several presentations by the Finance Director which focused on the fiscal health of the enterprise fund, the most recent audit of the fund and recommendations by the auditors, and the status of depreciation reserves.
- A presentation by the City Attorney on the bond approval process; specifically actions that would be required of the City Council.
- A presentation by the Director of the Alaska Municipal Bond Bank regarding the bond application process and specifically, what types of financial information and fiscal capacity would be required in order for a bond sale to be approved.
- A presentation by the Port and Harbor Director regarding the need and justification for priority projects including those contained in the enabling resolution and others either on or proposed for the CIP List.
- An analysis by the Finance Director regarding the fee increases that would be required in order to support different levels of bond payments and transfers to the depreciation fund.

- Updated and more precise project cost estimates provided by engineers using funds appropriated by the Council.
- A rate and fee schedule comparison between Homer and other regional ports and harbors provided by the Port and Harbor Director.
- A fee by fee analysis and discussion regarding which fees could reasonably be raised and their nexus to users that would benefit by the improvements.
- Detailed information on the Municipal Harbor Matching Grant Program; the proposed source of funds to be leveraged by revenue bonds.
- Information regarding alternative funding sources and project descriptions for the harbor Entrance Erosion Control and Harbormaster Building projects.

Using the information referenced above, the Committee selected a list of projects with the help of an objective set of criteria which included, but was not limited to, health and safety, the economic and/or revenue impacts of the project, and whether the project was eligible for matching funds. The Committee also established an upper ceiling on how much debt the enterprise fund could reasonably assume and proposed fee increases accordingly.

Public Outreach and Education

The Committee recognized early on that incurring debt and raising fees was going to be a tough sell and that it was imperative to get as much information out to the public as possible. It took a draft list of projects and proposed fee increases to the public and solicited input. To that end, the Committee undertook a number of steps which included:

- Power point presentations for the Council and for other boards and commissions
- Staff and Committee member attendance and participation at Port and Harbor and Economic Development Commission meetings.
- Presentations to affected user groups
- Development of a informational web page
- Media interviews
- Development of a Q and A informational brochure
- A Coffee Table radio discussion devoted to the topic.
- Public hearings including a hearing conducted by the Port and Harbor Commission.

Fine Tuning the Proposal

After receiving public input and reviewing the motions adopted by the Port and Harbor Commission, the Committee fine tuned its proposal and recommendation to the Council. In summary, the Committee amended the proposal as follows:

- It deleted two projects (Harbormaster Building and Harbor Entrance Erosion Control) that are not eligible for a state match and would be funded entirely by bond sale proceeds. This reduces

the amount to be borrowed. The Committee urges the Council to identify alternative funding sources for those projects.

- It eliminated the proposed imposition of a \$2.00 "head tax" on commercial passenger vessels
- It voted to raise fees in stages beginning this year in order to reduce impacts to harbor users, increase deposits into reserve accounts, and demonstrate to the Bond Bank and the State of Alaska that the City has the financial capacity and the revenue stream in place to make its bond payments and provide a 50% match.
- It added the Load and Launch Ramp to the list of projects to be partially funded by the Municipal Harbor Matching Grant Program in the event that a local 25% match for federal dollars was needed.
- It voted to recommend bonding for \$5 Million which is a little more than the estimated cost of the remaining projects. It did so in anticipation of possible increases in construction costs and other unforeseen circumstances.
- It voted to keep proposed fee increases in place (with the exception of the "head tax"), even though two projects were dropped, so that any excess in revenues over and above what is needed for bond payments could be deposited into the depreciation reserves as recommended by the City auditors. The proposed 10% increase in ice fees are to be used for repair and replacement at the ice plant.

The Proposal

The Committee recommends that the Council sell revenue bonds for three projects. The proceeds of the bond sale would provide the required 50% local match for grant funding through the State Municipal Harbors Matching Grant Program. The projects are Ramp 3 Gangway and Approach, Harbor Float Replacement, and System 5 Upgrades. The total cost of these three projects is \$8,549,000.00. The City would be responsible for half of that amount or \$4,274,500.00. As noted above, the Committee recommends bonding for \$5 Million. If the amortization schedule was twenty years, the annual bond payment would be \$250,000 plus interest.

The Committee recommends that the revenue to make the bond payments be obtained by raising the fees for fuel wharfage, moorage, ice, and dockage. A more detailed description of the fee increases and project funding is attached.

RECOMMENDATION

The Committee recommends that the City Council adopt this proposal and course of action and authorize the City Manager to:

- Prepare an application for funding for these projects through the Municipal Harbors Matching Grant Program
- Prepare a bond sale application
- Prepare resolutions amending the Fee Schedule and Port and Harbor Tariff for adoption in June.

Office of the City Clerk

Jo Johnson, CMC, City Clerk
Melissa Jacobsen, CMC, Deputy City Clerk II
Renee Krause, CMC, Deputy City Clerk I



491 E. Pioneer Avenue
Homer, Alaska 99603
(907) 235-3130
(907) 235-8121
ext: 2224, 2226, or 2227
Fax: (907) 235-3143
Email: clerk@ci.homer.ak.us

MEMORANDUM 12-079

TO: Mayor Hornaday and Homer City Council

FROM: Melissa Jacobsen, CMC, Deputy City Clerk *WJ*

DATE: May 8, 2012

SUBJECT: Port and Harbor Advisory Commission Recommendation Regarding the Port and Harbor Improvement Committee Harbor Improvement Bond Project List

The Port and Harbor Advisory Commission took the following action at their April 25, 2012 regular meeting:

ZIMMERMAN/WEDIN MOVED TO RECOMMEND TO COUNCIL TO HAVE THE HARBOR IMPROVEMENT COMMITTEE CHANGE THE CAPITAL IMPROVEMENT BOND INCLUDE ONLY SYSTEM 5 UPGRADE, RAMP 3 GANGWAY, AND PORTIONS OF THE FLOAT REPLACEMENT TO A MAXIMUM BOND OF \$4 MILLION.

There was discussion that the necessary work on the floats could be done now, fixing pilings and replacing A float to replace ramp 3, and the rest of the float work out spread to be done as reserve funding allows. It is a good compromise. Concern was expressed that relying on reserve funds to grow and be able to spend as you go could take about 5 years based on the proposed tariff increases. By that time inflation will have increased the cost but if we can bond and set the cost in today's dollars and pay with future income is the more economical way to get the work done. 5 years from now there will be more problems that need to be addressed. Bonding rates are really low right. It looks good on paper to put it off and maybe we can get by without someone getting hurt or a float failing, but taking care of these things presently will help us see our way clear.

Other comments included recommending the harbor office be address separately through a general obligation bond that would be paid back by the city as a whole since there is a significant amount of sales tax generated by the harbor that goes in to the general fund.

VOTE: YES: STOCKBURGER, ULMER, WEDIN, ZIMMERMAN
NO: CARROLL, HARTLEY, HOWARD

Motion carried.



Port & Harbor Improvement Committee

* Facts & Information *

May 8, 2012

Committee Q&A

❖ **What was the reason for establishing the Harbor Improvement Committee?**

Homer City Council is very aware of the Port and Harbor's importance to this community and that as an enterprise we cannot continue to allow the facilities to fall into a further state of disrepair. All of the harbor facilities have an expected life span and we are falling further and further behind every year that we don't act. The Improvement Committee was formed to address this issue. The goals were to prioritize the projects from the Capital Improvement Plan (CIP) list and then work to find the best options possible for funding. The Improvements Committee was directed to work collaboratively with the Port and Harbor Advisory Commission and bring recommendations to City Council.

❖ **How were the projects selected?**

The committee and commission selected projects from the Port and Harbor CIP list. The goal was to select projects that benefit as wide a range of the harbor users as possible and not focus on just one user group or the other.

❖ **How do matching funds tie in?**

Most grants, either State or Federal, will not fund projects 100%. They most often call for 25% or even 50% in matching funds. In some cases cities are able to use State grant money to match Federal grant money or vice versa. This of course helps to lower the local contributions needed to fund the projects.

❖ **What does revenue bonding mean?**

Revenue bonding means the act of securing a loan to do improvement work to capital facilities that add real value to the Enterprise assets. Revenue bonds are secured by the value of the asset and a cash flow (income) that assures repayment of the borrowed money including interest. The Harbor Enterprise will dedicate funds in its budget to make the bond loan payments.

❖ **What would the money be used for?**

Bond money will be used to pay the local share of any grant agreements that partially fund a project, and can be used to pay for projects where grant funds are not available. The bond funds will not be used for day to day maintenance and operations of the harbor.

A Work in Progress

The Improvement Committee and staff will continue searching for funding sources that will help to lower the costs of these projects. Likely there will be changes to the plan as we move ahead. These changes will be done with full transparency and vetted through the public process. However, it is important that the Committee have the flexibility to search out all funding avenues in order to get the biggest bang for the Enterprise buck.

Taking a Proactive Stance

The Harbor Enterprise has a simple mission: provide safe port and harbor for all customers, collect fees for services to pay operating costs, and to fund reserves for maintenance and/or future replacement of all port and harbor facilities. The Federal and State government does not own Homer Harbor, we do. The message coming down loud and clear from the government is that we must be proactive in our maintenance programs. In the grant applications we are required to show our commitment to maintaining the facilities now and when replacement becomes necessary, and that we have the funds in our reserves to cover those costs.

What is a Sustainable Rate?

What is the value of existing infrastructure, the life expectancy, and its maintenance costs? What will it cost to replace these facilities when they are simply worn out? The challenge for the Enterprise is to set rates that can be applied fairly and equitably to all Port and Harbor users. The rates need to be competitive yet still meet our mission of operating and maintaining all the Port and Harbor facilities. *Easy right!*

Asset Values Compared to Reserve Funds

The estimated asset value of the Enterprise infrastructure and facilities on the Homer Spit is \$44 million. You can see by the chart below that the projected ending balance of 2012 is \$1,285,027. Independent auditors recommend that our reserves balance should be at approximately \$10 million; and our annual transfer to our reserves from our operating budget should be about \$1.4 million instead of \$440,000.

CITY OF HOMER 2012 OPERATING BUDGET					
Port & Harbor Reserves 456 - 380					
Acct #		2009 Actual	2010 Actual	2011 Budget	2012 Budget
	Beginning Balance	1,219,265	1,383,836	1,652,816	1,675,027
4992	Annual Transfer	324,530	324,530	470,000	440,000
4801	Interest Income	11,050	35,752		
4610	Plans & Specs	320			
	Loan Repayment for Energy Projects 11-02(S)(A)				See Note *
5990	Energy Fund Transfer (Fund 620)		(48,620)	(131,335)	
5990	Bond Reserve Fund (456-382) Budget				(500,000)
	Expenditures	(171,329)	(42,682)	(186,311)	
	Subtotal	1,383,836	1,652,816	1,805,170	1,615,027
	Encumbered			(130,143)	(330,000)
	Ending Balance	1,383,836	1,652,816	1,675,027	1,285,027

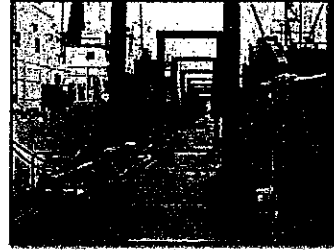
Port & Harbor Projects on the CIP List

A capital improvement plan (CIP) is a long-term guide for capital project expenditures. The CIP includes a list of capital projects, the community envisions for the future, and a plan that integrates timing of expenditures with the City's annual budget. A capital improvement project is one that warrants special attention in the municipal budget. Normally, public funds are not expended if the project is not listed in the CIP. Projects specifically related to the Port and Harbor, plus how long they have been on the CIP list, are listed below:

- Barge Mooring Facility 2011
- Deep Water/Cruise Ship Dock Expansion, Ph. I 1989
- East Boat Harbor 2004
- End of the Road Wayside, Ph. I 2008
- Fish Dock Restrooms 2001
- Fishing Lagoon improvements 2009
- Harbor Entrance Erosion Control 1998
- Harbor Float Replacement 2003
- Harbor Sheet Pile Loading Dock 2011
- HH Float Replacements 2011
- Homer Spit Dredged Material Beneficial Use 2010
- Ice Plat Upgrade 2011
- Load and Launch Ramp 2012



- Marine Ways Large Vessel Haulout Facility 2011
- Mariner Park Restrooms 2004
- Port and Harbor Building 1985
- Ramp 3 Gangway and Approach 2003
- Truck Loading Facility Upgrades at Fish Dock 2012
- Upgrade System 5 – Vessel Shore Power 2010



Selected Projects for Bond Funding

These projects were selected by the Improvement Committee and Port and Harbor Commission. After receiving public testimony the Harbor Improvements Committee revised the list of projects selected for funding by removing the Harbor Office and the Harbor Entrance Erosion Control projects. The Load and Launch Ramp project was added back to the list as we will be seeking State Municipal Harbors grant monies for our required local match to the federally funded Load and Launch Ramp Renovation project.

Item No.	Description	Total Cost
1	Ramp 3 Gangway & Approach	\$795,000
2	Harbor Float Replacement	\$6,783,000
3	Upgrade System 5 – Vessel Shore Power	\$971,000
4	Load and Launch Ramp	\$875,000
Homer Harbor Capital Improvements Total		\$9,424,000

Grants & Matching Funds Needed

The following graph shows a breakdown of the project costs, the grant funding, and the local matching requirements as to date. Staff and the Improvement Committee are striving to find additional funding sources that will lower the cost to the Enterprise.

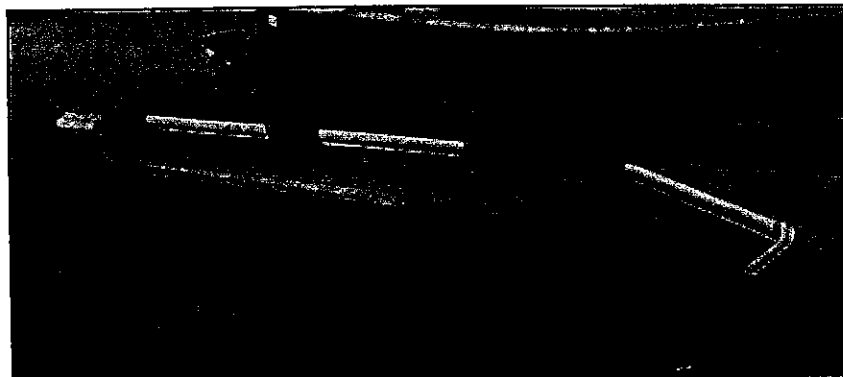
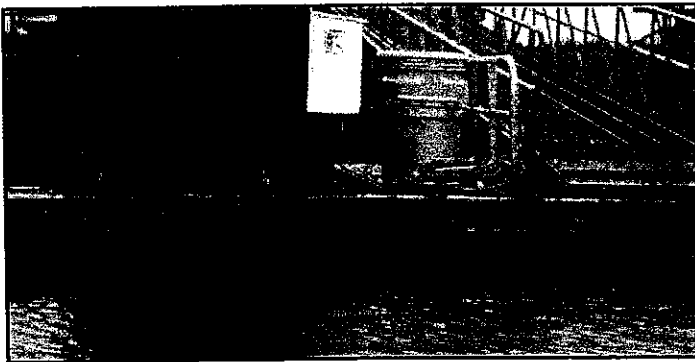
Project Description	Grant Funding	Local Funding
Ramp 3 Gangway & Approach	\$795,000	
Harbor Float Replacement	\$6,783,000	
Upgrade System 5 – Vessel Shore Power	\$971,000	
Load & Launch Ramp (local match covered by grant funding)	\$875,000	
	\$9,424,000	
Denial Grant Monies (25% matching required)	-\$330,000	\$110,000
	\$9,094,000	
50/50 Match (half of total)		\$4,547,000
Total amount needed		\$4,547,000
Annual Bond Payment (\$70,361 per million)		\$327,672

Proposed Port Tariff Rate Increases

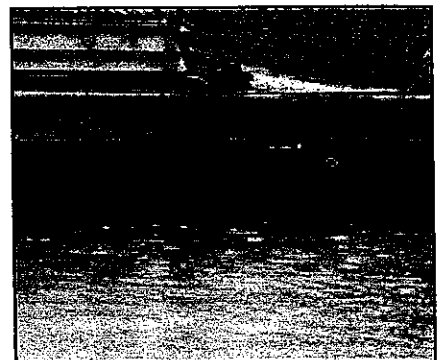
2012 & 2013 Proposed Homer Port & Harbor Rate Increases	2011 Actual Revenue	2012 Increase	2013 Increase
*All rates implemented over a two year period.			
Fuel Wharriage: Increase from \$0.0103 per gallon to \$0.02 per gallon.	\$141,176	\$66,477	\$132,953
Moorage: Increase 15% from current rate of \$35.22 per linear foot per year.	\$1,589,214	\$119,191	\$238,382
Ice: Increase 10% from current rate of \$119.00 per ton. (Monies gained from ice rate increase will go towards Fish Dock & Ice Plant maintenance, NOT for bond payments.)	\$372,722	\$18,636	\$37,272
Dockage: Change docking rate from per linear foot rate to a graduated rate schedule, 100% similar to Anchorage's	\$157,848	\$48,422	\$87,983
Total 2012 Increases from 2011		\$252,726	
Total 2013 Increases from 2011			\$496,590

Facility Conditions throughout the Harbor

Harbor maintenance is like painting a boat: if you start at the bow and work your way all around, by the time you get to where you began it'll be time to start all over again. The facilities on the Spit deal with salt water, extreme tides, extreme weather, and heavy use. This means we have to work hard to stay ahead of the game and we ask: is the Homer Harbor keeping up with the wear and use after all these years?

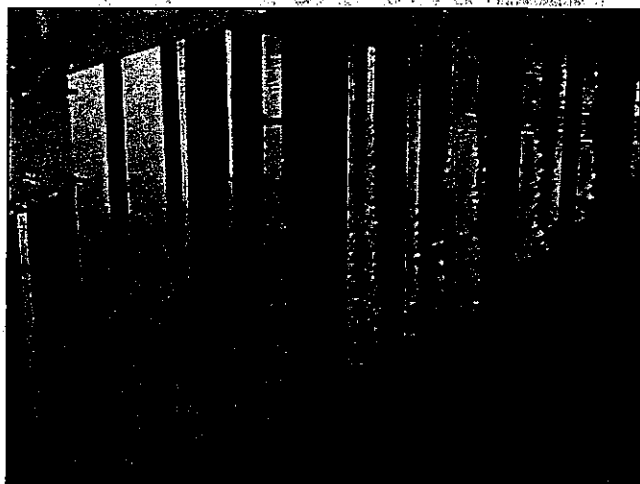


Deteriorating Floats through Harbor

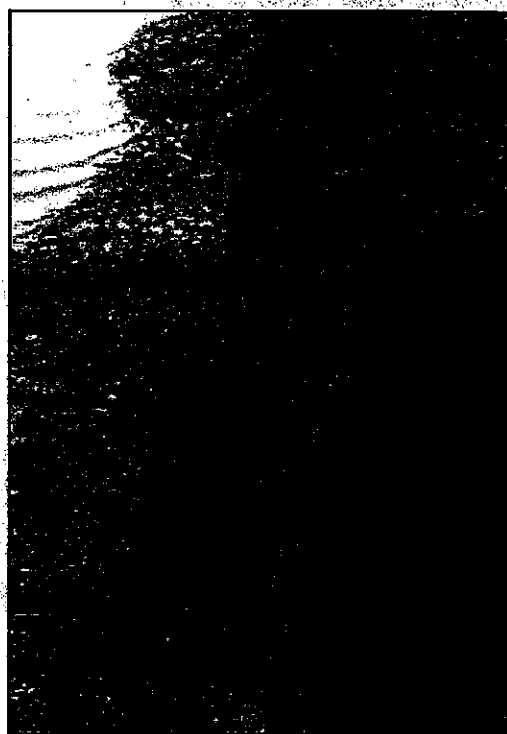
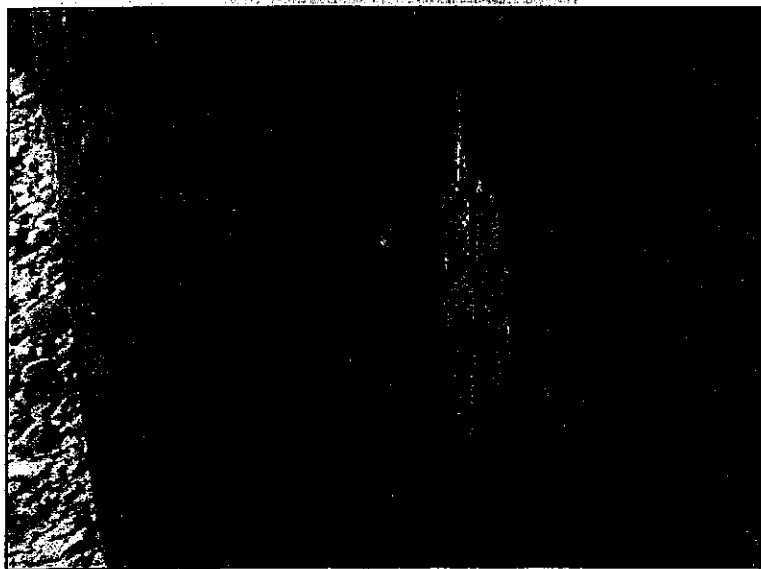




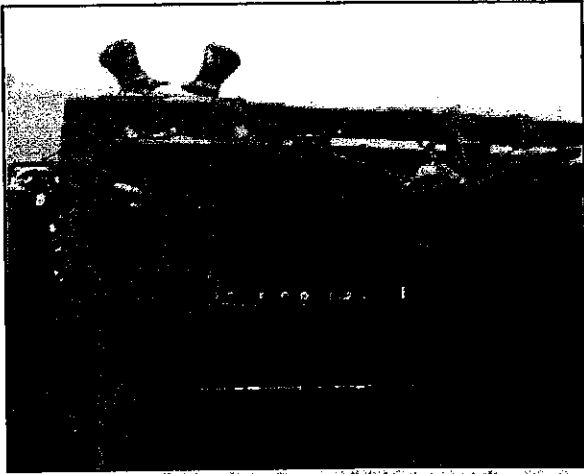
↑ Harbor Entrance Erosion ↑



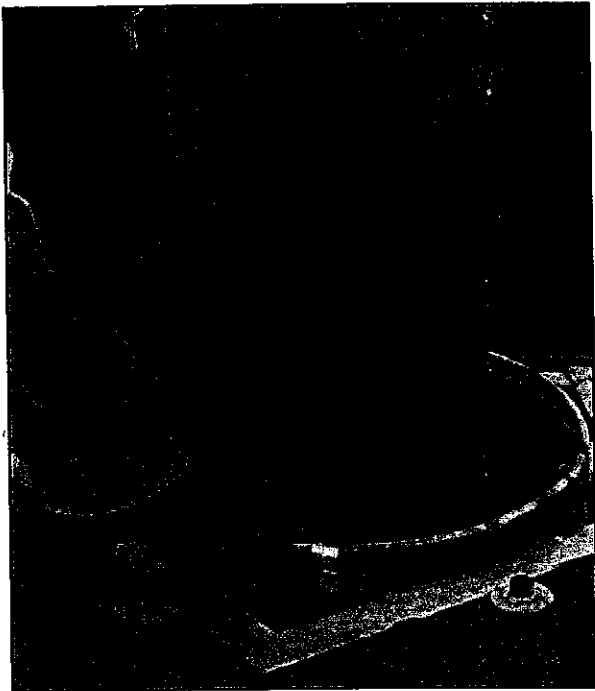
↑ Corrosion on Pioneer Dock and Fish Dock Pilings ↑



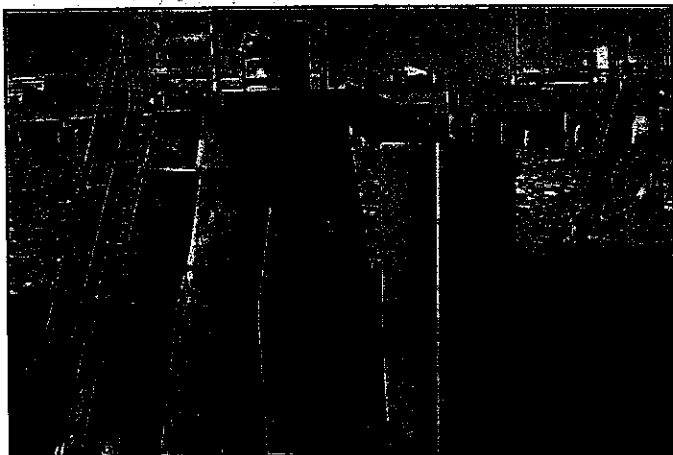
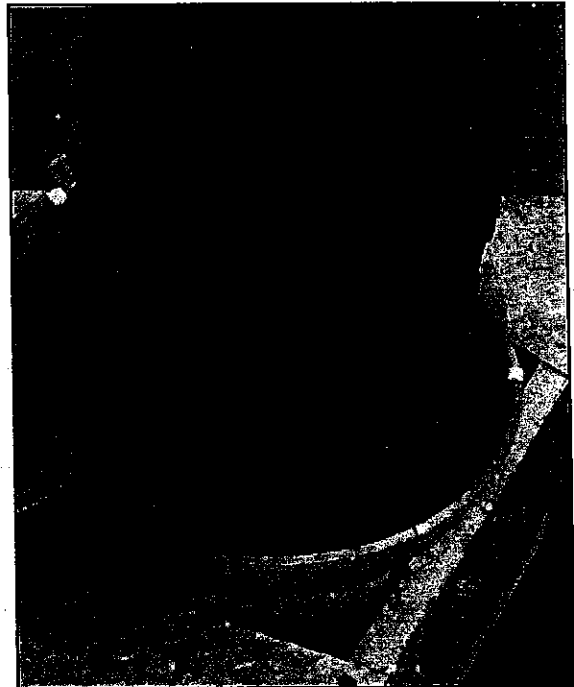
Wood Grid Piling Deterioration & Retaining Wall Sinkholes ↑ →



↑ Deep Water Dock – Missing fender & fender being held on by chains ↑

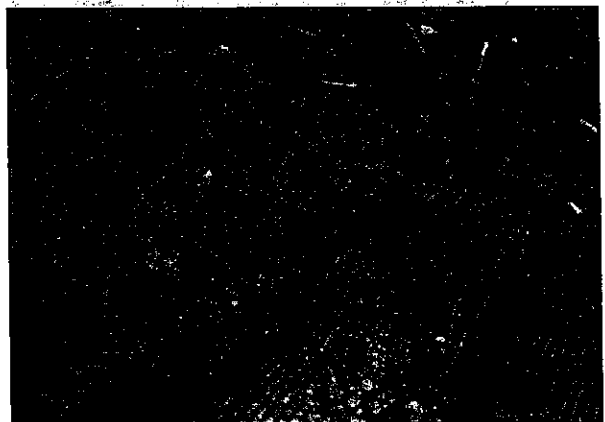


Harbor
Float
Pilings



Pioneer Dock Damaged Dolphin ↑

Load & Launch Ramp Deterioration ↓





Harbor Improvement Revenue Bond Projects

PROJECT DESCRIPTION & BENEFIT: This project will utilize municipal revenue bonds along with state and federal grant funds to accomplish six significant harbor improvement projects:

- Port & Harbor Building—will replace the existing Port & Harbor building (Harbormaster’s Office) constructed in 1983. The building is substandard with electrical, lighting, and heating deficiencies, and does not meet codes and standards for occupancy as an office building. The new building is being planned as overslope development. Cost: \$2,875,000.
- Harbor Entrance Erosion Control—will construct a rock revetment to replace the badly damaged and disintegrating log cribbing that was installed as a temporary erosion control measure in 1995. Rip-rap revetment would extend 935 feet from the jetty entrance of the harbor to the existing revetment near the Ferry Terminal, providing critical shore/infrastructure protection. Cost: \$600,000.
- Ramp 3 Gangway and Approach—will replace the existing Ramp 3 which dates back to the mid-1960s. This ramp is the steepest ramp in the harbor and difficult to use during very low tides. At 100 feet long, the new ramp will be less steep and therefore ADA compliant. It will be constructed of aluminum and covered by an awning to keep it snow and ice-free for year-round access. The existing Ramp 3 approach, a long narrow wooden structure in poor condition, will also be replaced. Cost: \$1,700,000.
- Harbor Float Replacement—will replace some of the oldest and most badly damaged floats in the harbor. These floats are plagued by worn and irregular walking surfaces, bull rails in need of replacement, ice damage to pilings, and broken sidewalks with exposed flotation. A total of 1,706 linear feet will be replaced: A Float, connecting E-J; J Float, R Float, and S Float. Cost: \$3,500,000.
- Upgrade System 5 – Vessel Shore Power and Water—will provide System 5, the large vessel float system in the Homer Harbor, with additional power pedestals and a year-round fresh water supply to meet the needs of the large vessel fleet and attract new vessels to be home-ported in Homer. Cost: \$530,000.
- Load and Launch Ramp—will reconstruct the entire Load and Launch Ramp facility, replacing all five lanes along with the floats and piles. The existing ramp is suffering from age-related wear and tear and has been judged by the Alaska Dept. of Fish and Game to be in need of replacement. The facility serves small boaters from all over south-central Alaska. Cost: \$3,500,000.

PLANS & PROGRESS: 80% of design and engineering funds have been procured through a Denali Commission Waterfront Development grant for three of the projects: Ramp 3 Gangway and Approach, Harbor Float Replacement, and System 5 Upgrade. The same three projects are also eligible for an Alaska Municipal Harbor Facility grant, which would cover 50% of construction costs. Local revenue bonds can be used to meet the 50% match requirement for this grant program. Load and Launch Ramp reconstruction may be accomplished with a Federal Aid in Sport Fish Restoration Act grant together with state matching funds (provided through the Dept. of Fish and Game).

Total Project Cost: \$12,705,000

Amount covered by existing or projected grant funds = \$6,530,000 as follows:

\$440,000 (Denali Commission grant – already secured)

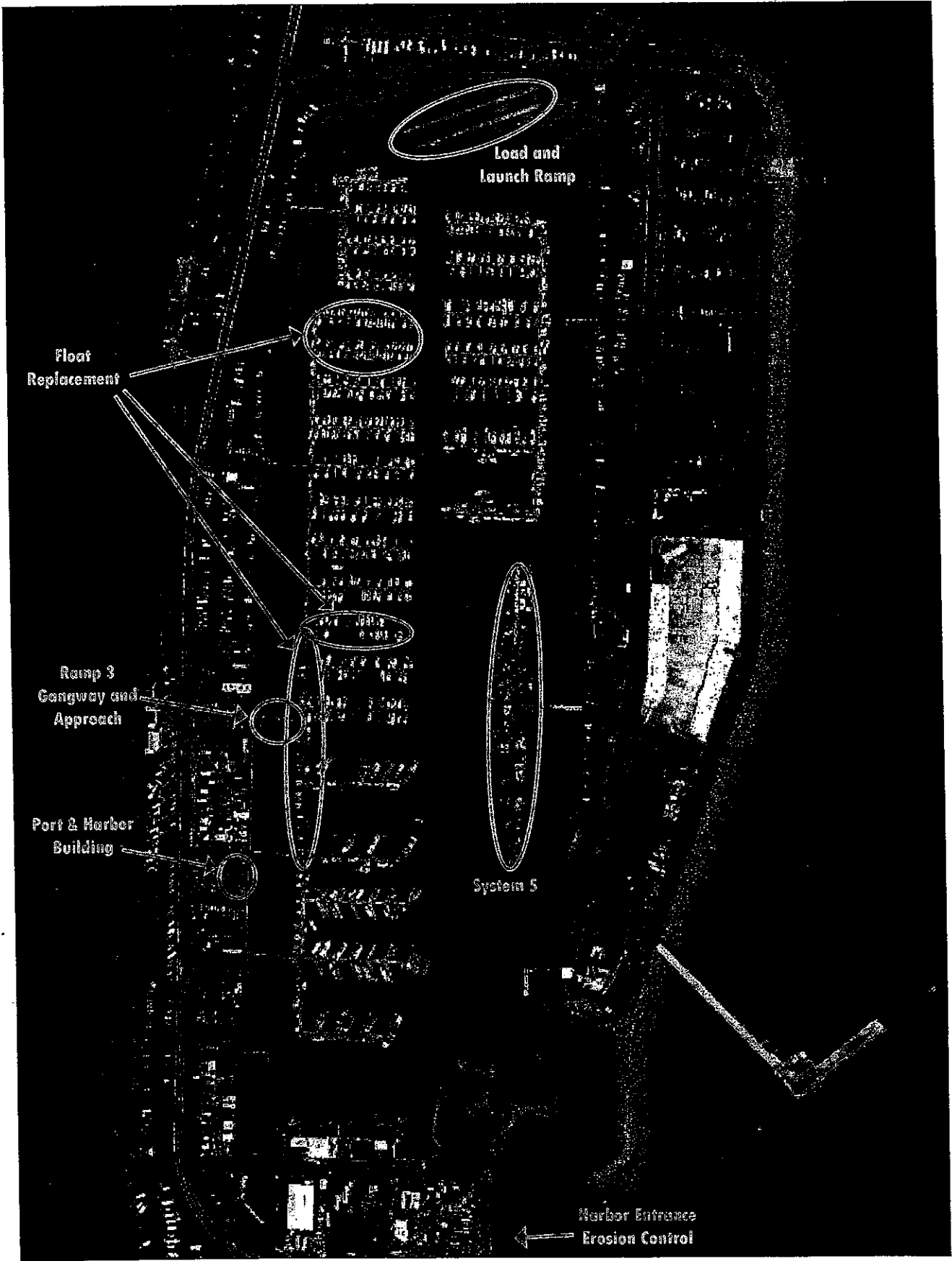
\$3,377,500 (Municipal Harbor Facility grant – to be requested after revenue bond sale is approved)

\$2,712,500 (Federal Aid in Sport Fish Restoration Act funds plus state Fish and Game funds)

Remaining (local responsibility) = \$6,175,000

Schedule: 2012-2014

Priority Level: 1



1 CITY OF HOMER
2 HOMER, ALASKA

Wythe

3
4 RESOLUTION 11-060
5

6 A RESOLUTION OF THE CITY COUNCIL OF HOMER,
7 ALASKA, ESTABLISHING A COMMITTEE TO DEVELOP A
8 PORT AND HARBOR IMPROVEMENT REVENUE BONDING
9 PLAN AND PROVIDE COMMITTEE REVIEW AND
10 OVERSIGHT THROUGHOUT THE IMPLEMENTATION AND
11 COMPLETION OF ANY APPROVED PLAN.
12

13 WHEREAS, The City of Homer Capital Improvement Plan (CIP) contains a variety of
14 repair and improvement items for the Port & Harbor district, many of which have been
15 languishing on the CIP list for a number of years with the continued hope of receiving funding
16 from State and Federal agencies; and
17

18 WHEREAS, The Homer City Council recognizes the Port and Harbor Enterprise as a
19 vital economic contributor to the community and as such desires to maintain and improve the
20 facilities to ensure continued economic vitality and support economic development within the
21 community; and
22

23 WHEREAS, State and Federal funding of these projects becomes increasingly unlikely
24 due to economic shortfalls at all levels of government; and
25

26 WHEREAS, The Port and Harbor is an Enterprise Fund which generates revenues that
27 can be allocated to the repayment of financed projects, which qualifies the Enterprise Fund for
28 Revenue Bonding consideration; and
29

30 WHEREAS, The Port and Harbor Enterprise Fund currently has no indebtedness; and
31

32 WHEREAS, The current Port and Harbor Enterprise Fund depreciation reserve account
33 has a balance of \$1,653,816 as of December 31, 2010.
34

35 NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska,
36 supports the establishment of a committee to develop a plan for the implementation of a Port and
37 Harbor Improvement Revenue Bonding Project; and
38

39 BE IT FURTHER RESOLVED that the committee will consist of six members, three
40 representatives from the Homer City Council (to be appointed by the Mayor and approved by the
41 Council), the Harbormaster, and two members from the Port and Harbor Advisory Commission
42 (to be selected by the Port and Harbor Advisory Commission). Other staff members will provide
43 administrative and consultative support as requested by the committee or directed by the City
44 Manager.
45

46 BE IT FURTHER RESOLVED that the committee will make a recommendation to the
47 Council not later than November 1, 2011, including information on the process for revenue bond
48 funding, a proposed timeline for project completion in not longer than three years with a
49 beginning date of no later than June 1, 2012, and including the following items from the Capital
50 Improvement Projects list:

- 51
- | | | | |
|----|----|---|-------------|
| 52 | a. | Harbormaster's Office (to be developed as a model over-slope development | |
| 53 | | project and include public restroom facilities with a separated entrance from the | |
| 54 | | building to allow 24-hour access) | \$2,875,000 |
| 55 | | | |
| 56 | b. | Harbor Entrance Erosion Control | 600,000 |
| 57 | | | |
| 58 | c. | Ramp 3 Gangway replacement (to include disability access) | 1,700,000 |
| 59 | | | |
| 60 | d. | Harbor Float Replacement | 3,500,000 |
| 61 | | | |
| 62 | e. | Upgrade System 5 – Vessel Shore Power & Water | 530,000 |
| 63 | | | |
| 64 | f. | Fishdock Restrooms | 400,000 |
| 65 | | | |

66 PASSED AND ADOPTED by the Homer City Council this 13th day of June, 2011.

67

68 CITY OF HOMER

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72 JAMES C. HORNADAY, MAYOR

73 ATTEST:

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76 JO JOHNSON, CMC, CITY CLERK

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79 Fiscal Note: \$6,720, Acct. No. 165-375 (HART Trails Reserve)

80

81

- 41 • Increasing the number of gas customers will result in the \$1.00 per Mcf tariff increase
- 42 being removed more quickly.
- 43 • Environmental benefits such as cleaner air, less green house emissions, and fewer fuel
- 44 spills will occur more rapidly.
- 45 • Costs to residents associated with extending mains would be more fair and equitable.

46
47 WHEREAS, There is an urgent need to begin a detailed review, public dialog, and
48 decision making process as soon as possible so that if the Council decides to move ahead with
49 public financing, everything could be in place by the fall or early winter of this year; and

50
51 WHEREAS, The advantages to having the financing mechanisms, including LID
52 boundaries and loan agreements or a bond sale in place by this fall include:

- 53
54 • Enstar would know exactly what it would be constructing next summer and it would have
- 55 adequate time to order materials and let contracts.
- 56 • The community could lock in construction cost estimates (good for 2 years) and the
- 57 current low interest rates.

58
59 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council finds that it is in
60 the public interest to quickly begin a focused and detailed review and public dialog regarding the
61 merits of providing public financing for construction of the natural gas distribution system within
62 the City.

63
64 BE IT FURTHER RESOLVED that the Council establishes June 4, 2012 as a workshop
65 date to begin the process and requests that the Administration provide relevant information and a
66 list of options regarding financing mechanisms and invite interested stakeholders to participate
67 including, but not limited to, representatives from Kachemak City, Enstar Natural Gas Company,
68 and the Borough.

69
70 PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

71
72 CITY OF HOMER

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75 _____
76 JAMES C. HORNADAY, MAYOR

77 ATTEST:

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79 _____
80 JO JOHNSON, CMC, CITY CLERK

81 Fiscal Note: N/A

Office of the City Clerk

Jo Johnson, CMC, City Clerk
Melissa Jacobsen, CMC, Deputy City Clerk II
Renee Krause, CMC, Deputy City Clerk I



491 E. Pioneer Avenue
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(907) 235-8121
ext: 2224, 2226, or 2227
Fax: (907) 235-3143
Email: clerk@ci.homer.ak.us

MEMORANDUM 12-080

TO: Mayor Hornaday and Homer City Council

FROM: Melissa Jacobsen, CMC, Deputy City Clerk *WJ*

DATE: May 8, 2012

SUBJECT: Economic Development Advisory Commission Recommendation
Regarding the Build Out of the Natural Gas Line Distribution System

At the April 10, 2012 meeting of the Economic Development Advisory Commission, City Manager Wrede updated the Commission on developments of the natural gas line progress and gave an overview of possible options for the distribution line.

The Commission made the following recommendation:

FAULKNER/WAGNER MOVED THAT THE EDC RECOMMENDS TO THE COUNCIL OF THE THREE OPTIONS THE CITY MANAGER LISTED THAT WE RECOMMEND THEY BOND TO BUILD OUT THE ENTIRE TOWN AT ONCE.

There was discussion that the build out would be for anything within the city limits and would include the main lines in the streets and there will likely be some adjustments as it is designed and engineered. The sooner we get gas to Homer the better off we will all be, the carbon footprint will be lower, we will save money on heat, and doing it as quickly as possible brings in more users.

Question was raised whether there are draw backs to doing it this way and City Manager Wrede explained that it would mean all properties would be included in one Local Improvement District. An advantage to that is that everyone has a reasonable expectation they will get natural gas soon and everyone would pay the same. The farther you get away from the downtown core the more expensive you get because density decreases. There is a lot of discussion that still needs to happen.

VOTE: NON OBJECTION: UNANIMOUS CONSENT

Motion carried.

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**CITY OF HOMER
HOMER, ALASKA**

City Clerk/Public Works Director

RESOLUTION 12-045

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA, AWARDED THE WASTEWATER TREATMENT PLANT BOILER REPLACEMENT PROJECT TO THE FIRM OF ORCA SPECIALTIES, INC. OF HOMER, ALASKA, IN THE AMOUNT OF \$17,437.50 AND AUTHORIZING THE CITY MANAGER TO EXECUTE THE APPROPRIATE DOCUMENTS.

WHEREAS, The 2012 City operating budget includes \$10,000 for the replacement of the boiler at the wastewater treatment plant; and

WHEREAS, Ordinance 12-02(A) appropriated a total of \$149,288 for the replacement of polymer injection equipment at the wastewater treatment plant (which included provisions for modifications to the hot water supply at the plant). Replacement of the boiler, as part of this overall project, will reduce energy usage and provide hot water for the polymer injection equipment (see Memorandum 12-081 from Public Works); and

WHEREAS, Quotes for the boiler replacement work were competitively obtained (consistent with the City Procurement Policy); four quotes were received; and the lowest quote was from Orca Specialties, Inc. in the amount of \$17,437.50; and

WHEREAS, This award is not final until notice is received by Orca Specialties, Inc. of Homer, Alaska, from the City of Homer.

NOW, THEREFORE, BE IT RESOLVED that the City Council of Homer, Alaska, hereby awards the contract for the Wastewater Treatment Plant Boiler Replacement Project to the firm of Orca Specialties, Inc. of Homer, Alaska, in the Amount of \$17,437.50 and the City Manager is hereby authorized to execute the appropriate documents to complete the work.

PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

CITY OF HOMER

JAMES C. HORNADAY, MAYOR

47 ATTEST:

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51 JO JOHNSON, CMC, CITY CLERK

52

53 Fiscal Note: Acct. No. 256-379 (polymer equipment replacement, per Ordinance 12-02(A)) -
54 \$7,437.50, and 2012 water/sewer fund capital budget (boiler replacement) - \$10,000.

55



CITY OF HOMER
PUBLIC WORKS
3575 HEATH STREET HOMER, AK 99603

TELEPHONE (907)235-3170
FACSIMILE (907)235-3145

MEMORANDUM 12-081

TO: Walt Wrede, City Manager

FROM: Carey Meyer, Public Works Director

DATE: May 8, 2012

RE: **Construction Contract Award Recommendations**
Wastewater Treatment Plant Boiler Replacement Project

The 2012 City operating budget includes \$10,000 for the replacement of the boiler at the wastewater treatment plant. Ordinance 12-02(A) appropriated a total of \$149,288 in funding for the replacement of polymer injection equipment at the wastewater treatment plant (which included provisions for modifications to the hot water supply at the plant).

During the planning and design of the polymer equipment and boiler replacement projects, Public Works realized that the existing boiler that was installed in the original plant was quite a bit larger than currently needed. Plans were revised to down size the boiler, eliminate the electric hot water heater (used to provide hot water to the polymer equipment), and utilize hot water from the boiler to mix polymer. Treatment plant staff will complete some of the demolition/installation efforts to conserve the budget.

Completing the project in this manner (significant boiler down-sizing and elimination of electric hot water heater) will accomplish the original purpose of the project, but will now also dramatically reduce energy consumption at the plant. All work can be completed within the established budget.

Quotes for the boiler replacement work were competitively obtained; seven quotes were solicited and four quotes were received. The lowest quote was received from Orca Specialties, Inc. in the amount of \$17,437.50.

Orca Specialties, Inc.	\$17,437.50
Eayrs Plumbing and Heating	\$18,470
All Season's Plumbing and Heating	\$22,171
Glen's Mechanical	\$34,300

Recommendations: The City Council pass a resolution awarding a contract for the Wastewater Treatment Plant Boiler Replacement project in the amount of \$17,437.50 to Orca Specialties, Inc., of Homer, Alaska and authorize the City Manager to execute all appropriate documents necessary to complete the project.

CITY OF HOMER
HOMER, ALASKA

City Manager/
Public Works Director

ORDINANCE 12-02(A)

AN ORDINANCE OF THE CITY COUNCIL OF HOMER,
ALASKA, AMENDING THE FY 2012 OPERATING BUDGET
BY APPROPRIATING \$114,288 FROM THE SEWER
RESERVE ACCOUNT FOR THE POLYMER EQUIPMENT
REPLACEMENT PROJECT.

WHEREAS, Feeding polymer into the wastewater treatment process is critical to protecting Kachemak Bay from pollution. The original polymer feed equipment at the wastewater treatment plant (installed when the plant was constructed in the 1980's) needs to be replaced; and

WHEREAS, The 2011 capital budget contained \$35,000 for the design/replacement of this equipment; and

WHEREAS, The cost to design, purchase and install the equipment is estimated to be \$149,288 (as reflected in Memorandum 12-003, prepared by the Public Works Director).

NOW, THEREFORE, THE CITY OF HOMER ORDAINS:

Section 1. The FY 2012 Operating Budget is hereby amended by appropriating \$114,288 from the Sewer Reserve Account for the replacement of polymer feed equipment at the wastewater treatment plant, as follows:

Expenditure:

<u>Account No.</u>	<u>Description</u>	<u>Amount</u>
256-379	Polymer Equipment Replacement	\$114,288


Section 2. This is a budget amendment ordinance is not permanent in nature, and shall not be codified.

ENACTED BY THE CITY COUNCIL OF HOMER, ALASKA, this 23rd day of June 2012.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

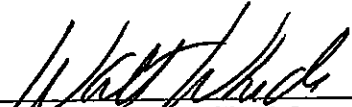
47 ATTEST:

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49 _____
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51 JO JOHNSON, CMC, CITY CLERK

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55 YES: 6
56 NO: 0
57 ABSTAIN: 0
58 ABSENT: 0

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61 First Reading: 1/09/12
62 Public Hearing: 1/23/12
63 Second Reading: 1/23/12
64 Effective Date: 1/24/12
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67 Reviewed and approved as to form:

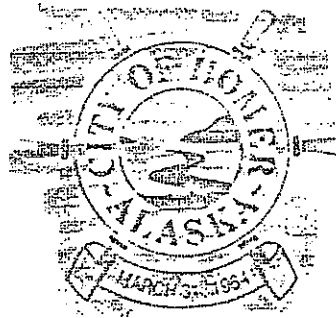
68
69 
70 _____
71 Walt E. Wrede, City Manager



Thomas F. Klinkner, City Attorney

72
73
74 Date: 1/25/12
75 _____

Date: 1-27-12



40 WHEREAS, The Homer Boys and Girls Club has requested that they be able to remain in
41 the facility at least until September 1, 2012 and has offered to contribute \$750 per month to
42 offset the cost of utilities; and

43
44 WHEREAS, The City will have to keep the building heated minimally even if it is
45 unoccupied because of the flat roof and other considerations.

46
47 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council finds that it is in
48 the best interest of the City to enter into another short term lease with the Homer Boys and Girls
49 Club and to work cooperatively with the Homer Boys and Girls Club Task Force and provide it
50 with more time to complete its work.

51
52 BE IT FURTHER RESOLVED that the Council authorizes the City Manager to enter
53 into a short term lease with the Homer Boys and Girls Club and to extend its occupancy of the
54 lower level of the HERC building. The term of the lease shall expire on September 1, 2012 and
55 the rent shall be \$750 per month.

56
57 PASSED AND ADOPTED by the Homer City Council this 14th day of May, 2012.

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59 CITY OF HOMER
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62 _____
63 JAMES C. HORNADAY, MAYOR

64 ATTEST:
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66 _____
67 JO JOHNSON, CMC, CITY CLERK

68 Fiscal Note: Utility fees to be determined.

CITY OF HOMER
HOMER, ALASKA

Lewis

RESOLUTION 11-078(A)

A RESOLUTION OF THE CITY COUNCIL OF HOMER, ALASKA,
AUTHORIZING THE CITY MANAGER TO ENTER INTO A
SHORT TERM LEASE WITH THE BOYS AND GIRLS CLUB TO
EXTEND ITS OCCUPANCY OF THE LOWER LEVEL OF THE
OLD INTERMEDIATE SCHOOL UNTIL DECEMBER 31, 2011.

WHEREAS, The City of Homer recently solicited proposals to lease all or a portion of the old Homer Intermediate School and a proposal was submitted by the Boys and Girls Club; and

WHEREAS, The Lease Committee found that the proposal was incomplete and requested that the City Manager work with the Boys and Girls Club to address some of the issues and obtain missing information, an effort that is currently in progress; and

WHEREAS, The Boys and Girls Club has been looking for alternative locations knowing that its current agreement with the City would lapse on May 31, 2011 and that the City was looking for new tenants or uses for the facility; and

WHEREAS, No other suitable locations have been found to date and the local chapter of the Boys and Girls Club will likely close this fall if a venue cannot be found; and

WHEREAS, The City currently has no prospective tenants for the facility until at least September 2012 and parts of the City administration are temporarily housed there, thus requiring that the building be maintained and heated to more than "warm" status; and

WHEREAS, There are a number of scenarios and ideas circulating about the future use of the building that incorporate a continued presence by the Boys and Girls Club.

NOW, THEREFORE, BE IT RESOLVED, That the Homer City Council hereby authorizes the City Manager to enter into a short term lease with the Boys and Girls Club and extend its occupancy of the lower level of the old intermediate school until December 31, 2011.

PASSED AND ADOPTED by the Homer City Council this 25th day of July, 2011.

CITY OF HOMER


JAMES C. HORNADAY, MAYOR

ATTEST:


JO JOHNSON, CMC, CITY CLERK

Fiscal Note: Utility fees to be determined

40 WHEREAS, The Homer Boys and Girls Club has requested that they be able to remain in
41 the facility at least until the end of the current school year and has offered to contribute \$750 per
42 month to offset the cost of utilities; and

43
44 WHEREAS, The City will have to keep the building heated minimally even if it is
45 unoccupied because of the flat roof and other considerations.

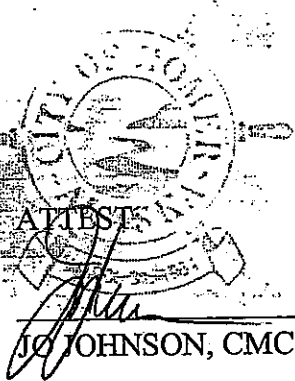
46
47 NOW, THEREFORE, BE IT RESOLVED that the Homer City Council finds that it is in
48 the best interest of the City to enter into another short term lease with the Homer Boys and Girls
49 Club and to work cooperatively with the Homer Boys and Girls Club Task Force and provide it
50 with more time to complete its work.

51
52 BE IT FURTHER RESOLVED that the Council authorizes the City Manager to enter
53 into a short term lease with the Homer Boys and Girls Club and to extend its occupancy of the
54 lower level of the HERC building. The term of the lease shall expire on May 31, 2012 and the
55 rent shall be \$750 per month.

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57 PASSED AND ADOPTED by the Homer City Council this 28th day of November, 2011.

58
59 CITY OF HOMER

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61 
62 MARY E. WYTHE, MAYOR PRO TEMPORE



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66 JO JOHNSON, CMC, CITY CLERK

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68 Fiscal Note: \$14,000 in additional expenditures for utility costs to be paid for by new revenues
69 and a fund transfer: \$3,750 in rent and a \$10,250 transfer from GF Reserves (Fund Balance).

COMMENTS OF THE AUDIENCE
COMMENTS OF THE CITY ATTORNEY
COMMENTS OF THE CITY CLERK
COMMENTS OF THE CITY MANAGER
COMMENTS OF THE MAYOR
COMMENTS OF THE CITY COUNCIL
ADJOURNMENT

